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AGENDA FOR

PLANNING CONTROL COMMITTEE



Contact: Michael Cunliffe
Direct Line: 0161 253 5399
E-mail: m.cunliffe@bury.gov.uk
Web Site: www.bury.gov.uk

To: All Members of Planning Control Committee

Councillors: G McGill (Chair), S Arif, C Boles, D Duncalfe, D Green, J Harris, M Hayes, D Quinn, S Thorpe, D Vernon and M Walsh

Dear Member/Colleague

Planning Control Committee

You are invited to attend a meeting of the Planning Control Committee which will be held as follows:-

Date:	Tuesday, 13 December 2022
Place:	Council Chamber, Bury Town Hall
Time:	7.00 pm
Briefing Facilities:	If Opposition Members and Co-opted Members require briefing on any particular item on the Agenda, the appropriate Director/Senior Officer originating the related report should be contacted.
Notes:	https://councilstream.com/burycouncil

AGENDA

1 APOLOGIES FOR ABSENCE

2 DECLARATIONS OF INTEREST

Members of the Planning Control Committee are asked to consider whether they have an interest in any of the matters on the Agenda and, if so, to formally declare that interest.

3 MINUTES OF THE MEETING HELD ON THE 8TH NOVEMBER 2022 *(Pages 3 - 6)*

Minutes of the meeting held on Tuesday the 8th November 2022 are attached.

4 PLANNING APPLICATIONS *(Pages 7 - 212)*

Reports attached.

5 DELEGATED DECISIONS *(Pages 213 - 228)*

A report from the Head of Development Management on all delegated planning decisions since the last meeting of the Planning Control Committee is attached.

6 PLANNING APPEALS *(Pages 229 - 242)*

A report from the Head of Development Management on all Planning appeal decisions since the last meeting of the Planning Control Committee is attached.

7 URGENT BUSINESS

Any other business which by reason of special circumstances the Chair agrees may be considered as a matter of urgency.

Minutes of: PLANNING CONTROL COMMITTEE

Date of Meeting: 8 November 2022

Present: Councillor G McGill (in the Chair)
Councillors S Arif, C Boles, D Duncalfe, J Harris, M Hayes,
D Quinn, S Thorpe, D Vernon and M Walsh

Also in attendance: Councillors K Hussain and J Lancaster

Public Attendance: 13 members of the public were present at the meeting.

Apologies for Absence: Councillor D Green

PCC.1 APOLOGIES FOR ABSENCE

Apologies for absence were submitted by Councillor D Green.

PCC.2 DECLARATIONS OF INTEREST

Councillor D Duncalfe declared an interest in planning application 68595, Wesley Methodist Primary School, Forth Road, Radcliffe, as a family member attended the school and another relative was a member of the Parent and Teachers Association for the educational establishment.

PCC.3 MINUTES OF THE MEETING HELD ON THE 11TH OCTOBER 2022

Delegated decision:

That the Minutes of the meeting held on the 11th October 2022 be approved as a correct record and signed by the Chair.

PCC.4 PLANNING APPLICATIONS

A report from the Head of Development Management was submitted in relation to applications for planning permission.

There was supplementary information to add in respect of application numbers 68542, 68608, 68633 and 68661.

The Committee heard representations from applicants, objectors and Ward Councillors in respect of applications submitted. This was limited to three minutes for the speaker.

Delegated decisions:

1. That the Committee **Requested a site visit** for the following application to further consider issues of vehicle parking: -

79 Bury New Road, Whitefield, Manchester, M45 7EG

Change of use from 6 bed HMO (Class C4) to 8 bed HMO (Sui Generis)

2. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and subject to the conditions included: -

Ainsworth Nursing Home, Knowsley Road, Ainsworth, Bolton, BL2 5PT

Erection of a dwelling and associated access, parking and landscaping, provision of landscape buffer, and change of use and refurbishment of Gate House within the curtilage of the former Ainsworth Nursing Home

3. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and subject to the conditions included: -

Wesley Methodist Primary School, Forth Road, Radcliffe, Manchester, M26 4PX

Erection of 2.4m high fencing and 2no. Access gates

4. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and subject to the conditions included: -

Land adjacent to 51 Humber Drive, Bury, BL9 6SJ

Erection of 2 no. semi detached dwellings

5. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and subject to the conditions included: -

Land adjacent to 391 Whalley Road, Shuttleworth, Ramsbottom, Bury, BL0 0ER

Demolition of existing garages and Erection of 2 no. semi-detached dwellings with associated parking; Additional off street parking for residents of Whalley Road

PCC.5 DELEGATED DECISIONS

A report from the Head of Development Management was submitted listing all recent planning application decisions made by Officers using delegated powers since the last meeting of the Planning Control Committee.

Delegated decision:

That the report and appendices be noted.

PCC.6 PLANNING APPEALS

A report from the Head of Development Management was submitted listing all recent planning and enforcement appeal decisions since the last meeting of the Planning Control Committee.

Delegated decision:

That the report and appendices be noted.

PCC.7 URGENT BUSINESS

No urgent business was reported.

COUNCILLOR G MCGILL
Chair

(Note: The meeting started at 7.00pm and ended at 8.32pm)

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Title	Planning Applications
To:	Planning Control Committee
On:	13 December 2022
By:	Development Manager
Status:	For Publication

Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

This report has the following implications

Township Forum/ Ward: Identified in each case.

Policy: Identified in each case.

Resources: Not generally applicable.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:
The elimination of discrimination, harassment and victimisation;
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

Human Rights: All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

Development Manager

Background Documents

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

- | | | |
|----------|---|--|
| 1 | Township Forum - Ward: Prestwich - Sedgley

Location: 24 Brookfield, Prestwich, Manchester, M25 1EL
Proposal: Demolition of existing link detached dwelling and erection of 2 no. semi-detached dwellings
Recommendation: Approve with Conditions | App No. 67953

Site Visit: N |
| <hr/> | | |
| 2 | Township Forum - Ward: Radcliffe - East

Location: Land at York Street/Bury Road, Radcliffe, Manchester, M26 2GL
Proposal: Full planning permission for 211 dwellings, together with associated car parking, landscaping, public open space, drainage, the laying out of roads and footways and other associated works including engineering operations to create flood defences and the development platform
Recommendation: Minded to Approve | App No. 68368

Site Visit: N |
| <hr/> | | |
| 3 | Township Forum - Ward: Radcliffe - West

Location: Stand United Reformed Church, Stand Lane, Radcliffe, Manchester, M26 1JE
Proposal: Proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access
Recommendation: Approve with Conditions | App No. 68548

Site Visit: N |
| <hr/> | | |
| 4 | Township Forum - Ward: Radcliffe - West

Location: Stand United Reformed Church, Stand Lane, Radcliffe, Manchester, M26 1JE
Proposal: Listed building consent for proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access
Recommendation: Approve with Conditions | App No. 68549

Site Visit: N |
| <hr/> | | |
| 5 | Township Forum - Ward: North Manor

Location: Hollymount Farm, Hollymount Lane, Tottington, Bury, BL8 4HP
Proposal: Demolition works and redevelopment for 4no. new dwellings (3 new build, 1 barn conversion) with car ports and two storey extension at side of existing farmhouse; associated landscaping; access works.
Recommendation: Approve with Conditions | App No. 68505

Site Visit: Y |
| <hr/> | | |
| 6 | Township Forum - Ward: Whitefield + Unsworth - Pilkington Park

Location: Philips High School, Higher Lane, Whitefield, Manchester, M45 7PH
Proposal: Increase existing car park from 802 m2, 43 bays to provide additional 1.354 m2 to create a further 42 car parking bays plus 2 disabled bays and 2 electric car charging stations; New automated gate 2m high to the entrance road to create a two way system; LED lighting | App No. 68615 |

Recommendation: Approve with Conditions

Site Visit: N

7 **Township Forum - Ward:** Radcliffe - North and Ainsworth **App No.** 68778

Location: Land to rear of 104 Bury New Road, Radcliffe, Bolton, BL2 6QB

Proposal: Removal of existing outbuildings; formation of hardstanding; construction of stable block with tack room / feed store; waste storage container and erection of fencing, gates and stile

Recommendation: Approve with Conditions

Site Visit: N

8 **Township Forum - Ward:** Whitefield + Unsworth - Pilkington Park **App No.** 68542

Location: 79 Bury New Road, Whitefield, Manchester, M45 7EG

Proposal: Change of use from 6 bed HMO (Class C4) to 8 bed HMO (Sui Generis)

Recommendation: Approve with Conditions

Site Visit: Y

Ward: Prestwich - Sedgley

Item 1

Applicant: Mr Ahmed Al Jundy

Location: 24 Brookfield, Prestwich, Manchester, M25 1EL

Proposal: Demolition of existing link detached dwelling and erection of 2 no. semi-detached dwellings

Application Ref: 67953/Full

Target Date: 19/07/2022

Recommendation: Approve with Conditions

Description

The proposal relates to the plot of a traditional detached suburban dwelling positioned at the head of a cul-de-sac and flanked by the substantially vegetated embankment of the Metrolink line at the immediate north-east (the line is actually around 30m away). The plot is further flanked by a vehicle vallet, tyre and cafe business beyond the rear garden to the east and neighbouring dwellings to the south. The plot widens from the front to rear and there are sporadic mature trees within the plot towards the rear boundary and a mature hedgerow forms the south-western boundary with the neighbouring dwelling at no. 22. There is a driveway that can accommodate a single vehicle. The street has a mixed residential character, but is largely made up of bay fronted terraces (in a buff coloured brick on the east side and red brick of the west).

The proposal is for the erection of 2 no. four bedroom semi-detached dwellings following demolition of the existing dwelling. The original submission was for a development of 3 no. dwellings. The proposed dwellings would provide three storey accommodation internally (with the second floor being accommodated within the roof space, but expressed as three storey to the rear elevation). They would have integral single garages and a single off-street parking space each. External materials would comprise light cream brick to the majority of elevations with dark brown panels around windows and grey slate tiles and grey GRP roofing with new timber fencing to boundaries. Hard landscaping would include permeable paving and soft landscaping would include tree planting.

Relevant Planning History

N/A

Publicity

The proposal has been advertised by direct neighbour notification. 11 representations have been received in total objecting to the proposal on the following grounds:

Original scheme (9 objections)

- Parking requirements for 3 dwellings would have unacceptable and hazardous impacts in a street that is already overcrowded and has issues relating to waste collection, parking and congestion, particularly at the end of the cul-de-sac, which serves as a turning area. The parking for 8 vehicles would be unworkable.
- Additional traffic would increase traffic noise and disturbance.
- The development would have unacceptable impacts on neighbouring amenity by reason of overlooking, loss of privacy, impact on light and due to construction activity
- The design and appearance would be completely out of character with the established character of the street and would be overdevelopment of the plot with considerable loss of garden land and unsightly bin storage.
- Lack of detail of impact on adjacent trees on the Metrolink embankment.
- There has been inadequate notification/consultation and the neighbouring and opposite

properties are empty.

Amended scheme (4 objections (2 from previous source))

- All previous comments pertain.
- It doesn't appear possible for the dwelling on the left to bring bins out for collection. The bin storage area to the right hand dwelling could not function properly.
- The 3 parking spaces at the end of the street would become unusable (noting that this is a turning area and they block an existing drive at the site).
- The rooms in the proposed dwellings appear small.
- Worried about structural damage to neighbouring dwellings.

Statutory/Non-Statutory Consultations

Traffic Section: No objection, subject to conditions requiring a Construction Traffic Management Plan and agreement and implementation of parking and driveway alterations and off-site highway improvements.

Waste Management: No comments received.

Borough Engineer - Drainage Section: No comments received.

United Utilities (Water and waste): No objection, subject to conditions relating to drainage.

Greater Manchester Ecology Unit: No objection, subject to conditions relating to nesting birds, protection of bats and landscape and biodiversity enhancements.

Environmental Health - Contaminated Land: No comments received.

Transport for Greater Manchester: No objection, subject to a condition concerning tree protection.

Pre-start Conditions - Applicant has agreed with pre-start conditions

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/2	Townscape and Built Design
EN5/1	New Development and Flood Risk
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

ASSESSMENT

Policy

Policy H1/2: Further Housing Development - requires the council to have regard for the suitability of proposals for housing development on sites not identified on the Proposals Map.

Policy H2/1: The Form of New Residential Development - requires all new residential development to make a positive contribution to the form and quality of the surrounding area. This is further supported by SPD 16 - Design and Layout of New Development in Bury.

Policy H2/2: The Layout of New Residential Development - seeks to ensure that new residential development will be acceptable in terms of layout in order to provide a good quality residential environment.

Policy EN1/2: Townscape and Built Design - seeks to ensure that any proposals would not have an adverse impact on the particular character and townscape of an area.

Policy EN6/3: The effect of land use changes on existing features of ecological or wildlife value will be considered and proposals should seek retain features and incorporate them into new development.

Policy HT2/4: Car Parking and New Development - requires all applications for development to make adequate provision for their car parking and servicing requirements. This is further supported by SPD 11 - Parking Standards in Bury.

Policy HT6/1: Pedestrian and Cyclist Movement - seeks to ensure that pedestrians and cyclists are able to move safely and conveniently.

Principle (Residential)

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The proposal is to replace an existing dwelling located in a suburban residential area with a further residential development. The site is located within the urban area, surrounded by established residential areas and close to routes well served by public transport and to facilities, goods and services. Thus, the principle of the development is established. The acceptability of the proposal thereafter is subject to consideration of the subsequent matters.

Layout and Design

Objections refer to the design of the proposed development, as noted above.

The site forms the last residential plot on the street, with the most notable flanking feature being the wooded embankment of the Metrolink line. On the same (east) side of the street, the residential form includes bay fronted terraced dwellings (buff brick), merging into two pairs of part rendered hipped roof semi-detached dwellings before arriving at the only detached dwelling on the street, which occupies the application site. The dwelling at the site is of a passively routine suburban character and is of little architectural merit.

The proposed replacement pair of semi-detached dwellings, exhibiting recessed parking bays, contrasting brickwork and large vertically proportioned first floor windows, would be of a contemporary simple and minimalist vernacular, aligned with the established building line. The eaves would align with the neighbouring dwellings, but the roof would be around 1.3m higher (at 10.0m) with gable walls, as opposed to hipped as existing. From the street, the

pair of dwellings would span the full width of the frontage, aside from a small gap to the dwelling to the south-west.

As noted, the site forms the last plot, and it is considered that this therefore offers an opportunity to provide a development of a bespoke design and character (the existing detached dwelling is anomalous within the street scene). On the same side of the street, dwellings largely span the full width of the associated plot and roof heights and eaves do not uniformly align and contrasting materials feature at juxtaposed dwellings. The roof, although forming a larger proportion of the front elevation, would not be out of scale or imbalance weighting between the facade and roof or the overall appearance of the principal elevation.

The rear elevation would be a full three storeys, thus the second floor fenestration would be above the level of the neighbouring eaves. This elevation would be unconventional in character and appearance, but fenestration and materials treatment would continue the contemporary minimalist vernacular of the principal elevation. Pertaining to the rear, it would not be perceptible in the street scene and would only be visible from private rear gardens of a limited number of neighbouring dwellings.

Given the above, it is considered that the proposed development would represent an enhancement to visual amenity and to the appearance and character of the street scene and would therefore be compliant with the above UDP Policies.

Residential Amenity

Objections refer to impact on residential amenity, as noted above.

The front elevation of the proposed dwellings would align with the dwelling to be demolished. The full 3 storey rear elevation would project beyond the rear elevation of the neighbouring dwelling by 1.5m and the single storey element by approximately 5.1m. However, the single storey would be set 1m lower and would have a lean to roof. The existing first floor gable window at the neighbouring dwelling appears to serve a non-habitable room and presently opposes the blank gable of the existing dwelling. The present dwelling and other dwellings (which are all two storey) have unobstructed views into neighbouring private rear gardens.

The proposed dwelling would exceed the minimum gross internal floor area requirements and associated minimum bedroom dimensions of the 'Technical housing standards – nationally described space standard'.

Rear gardens would be at least 15m long and 5m wide.

Given the above, the proposal would not have unacceptable impacts on neighbouring or occupiers amenity

Access and highway safety

Objections largely relate to existing traffic issues and consider that the proposal would exacerbate these.

The Highway Officer does not object to the proposal following the amendment to the scheme, which would provide a single garage space and driveway space for each dwelling, access from the turning head.

Supplementary Planning Document 11 - Parking Standards for Bury, sets a maximum parking standard of 3 spaces for four bedroom dwellings. However, the site is located in a high access area and within 1km of Prestwich Metrolink Station and town centre and bus stops on Bury New Road. Very few dwellings on Brookfield have dedicated off-street parking and the present dwelling has only one space. Given these circumstances, the parking provision proposed would be acceptable.

Therefore, subject to the conditions recommended by the Highway Officer, the proposal would not have any unacceptable impacts on highway safety and would therefore be compliant with the above Local Plan policies and the NPPF.

Ecology and trees

The Bat Emergence and Re-entry Survey found that there are no bat roosts present in the existing dwelling (to be demolished). It does however recommend that a precautionary working method is implemented.

The Arboricultural Impact Assessment and Tree Protection Plan indicate that through retention of existing hardstanding within the site and erection of protective fencing, the trees within the site and on the immediately adjacent Metrolink embankment would not be impacted upon by the proposed development.

The GMEU and TfGM are satisfied that the proposal would be acceptable, subject to the conditions as noted above. In such circumstances, the proposal would be compliant with the above Local Plan policies and the NPPF.

Other matters

Waste storage and collection - Concerning the specific objection concerning bin storage and access to collection, side doors into the integral garage would enable access for frontage collection and there would be ample room around the dwelling for alternative storage.

Conditions - The removal of permitted development rights is considered to be necessary and reasonable given the position of the proposed dwellings in relation to juxtaposition of the proposed dwellings to the neighbouring dwelling to the south-west.

The GMEU recommend that conditions are attached to protect nesting birds, bats and landscape and biodiversity enhancements.

The site would have two off-street parking spaces, so it would be reasonable to require the installation of an electric vehicle charging point at each.

The notification of the application was conducted in accordance with The Town and Country Planning (Development Management Procedure) (England) Order 2015.

Conclusion

Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that if regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts the determination must be in accordance with the plan unless material considerations indicate otherwise.

The proposal would deliver much needed residential development in a sustainable location and without detriment to the character and appearance of the area, highway safety or amenity, ecology or trees. As such, the proposal would comply with the above policies of the Local Plan and the NPPF. Therefore, in accordance with Section 38(6) of the Planning and Compulsory Purchase Act, the proposal merits approval.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental

conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings - Site Location Plan (Dwg no. 00-000)
Proposed Site Layout Plan (Dwg no. 01_000_B Rev. A)
Proposed Site Plan (Dwg no. 01_001_B Rev. A)
Proposed Ground Floor (Dwg no. 01_002_B Rev. B)
Proposed First Floor Plan (Dwg no. 01_003_B Rev. B)
Proposed Second Floor Plan (Dwg no. 01_004_B Rev. B)
Proposed Elevations (Dwg no. 01_006_B Rev. B)
Proposed Elevations (Dwg no. 01_007_B Rev. B)
Proposed Section (Dwg no. 01_008_B Rev. B)
Arboricultural Impact Assessment (Dwg No. Arbtech AIA 01)
Tree Protection Plan (Dwg No. Arbtech TPP 01)
and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. The development hereby approved shall be carried out in accordance with a surface water drainage scheme which shall be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include provision of potential SuDS options for surface water drainage, which shall be drained on a separate system to foul sewage. The drainage scheme shall be implemented prior to first occupation of any dwelling and thereafter maintained.
Reason. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and Section 14 - Meeting the challenge of climate change, flooding and coastal change of the National Planning Policy Framework.

6. The external materials used in the construction of the development hereby approved shall be as detailed on drawing 'Proposed Elevations' (Drawing No. 01_006_B).
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Townscape and Built Design.

7. No development shall commence, including demolition, unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
 - Photographic dilapidation survey of the footways and carriageways abutting the site in the event that subsequent remedial works are required following demolition works, construction of the development/boundary treatment and as a result of any statutory undertakers connections to the new dwellings;
 - Access routes for demolition/construction traffic from the adopted highway;
 - Site hoardings (if proposed) clear of the adopted highway;
 - Hours of operation, confirmation of demolition, delivery and construction vehicle sizes that can be accommodated on the residential estate roads that serve the site and number of vehicle movements;
 - A scheme of appropriate warning/construction traffic signage in the vicinity of the site and its access;
 - Parking on site (or on land under the applicant's control) of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials, including any requisite phasing of the development to accommodate this;
 - Measures to ensure that all mud and other loose materials are not spread onto the adjacent adopted highways as a result of the groundworks operations or carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the demolition and construction periods and the measures shall be retained and facilities used for the intended purpose for the duration of both periods. The areas identified shall not be used for any other purposes other than the parking of vehicles and storage of demolition/construction materials. All highway remedial works identified as a result of the dilapidation survey shall be implemented to the written satisfaction of the Local Planning Authority prior to the development hereby approved being occupied.

Reason. - To maintain the integrity of the adopted highway, mitigate the impact of the construction traffic generated by the proposed development on the adjacent adopted highways, ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and ensure that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety, pursuant to Policy EN1/2 of the Bury Unitary Development Plan.

8. The development hereby approved shall not be first occupied unless and until the access, car parking and driveway alterations and bin storage arrangements shown indicatively on approved plan reference 01_001_B Revision A, incorporating the reconstruction of the footway abutting the site to a scope and specification to be agreed, repositioned footway crossing to serve the shared access arrangements, provision of edgings at the back of the footway to demarcate the limits of the adopted highway, new driveway in a porous/permeable material and/or measures to prevent the discharge of surface water onto the adopted highway and all associated highway remedial works required to reinstate the highway to its former condition prior to commencement of the development, have been implemented in full accordance with the approved details.
Reason. - To ensure good highway design and maintain the integrity of the adopted highway in the interests of highway safety, pursuant to Policy EN1/2 of the Bury Unitary Development Plan.

9. The garage to both dwellings shall be fitted with a roller shutter door or a similar approved type which does not project outwards at any time during or after operation and shall thereafter be maintained. Furthermore, the garage at both dwellings shall be used as a private garage and/or garden store only and shall not at any time be used to provide living accommodation or for any purpose which would preclude its use for the parking of a motor vehicle.
Reason. To enable a vehicle to stand clear of the highway whilst the garage doors are opened and to allow adequate space to maintain a vehicle clear of the highway and to enable two off-street parking spaces in the interests of road safety, pursuant to Policy EN1/2 of the Bury Unitary Development Plan.

10. Each dwelling hereby approved shall be provided with 1 electric vehicle (EV) charge point (minimum 7kW*) prior to its occupation.
 *Mode 3, 7kW (32A) single phase, or 22kW (32A) three phase, and for 50kW Mode 4 rapid charging may be required. British Standard BS EN 61851-1:2019 to be used.
Reason. To encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

11. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

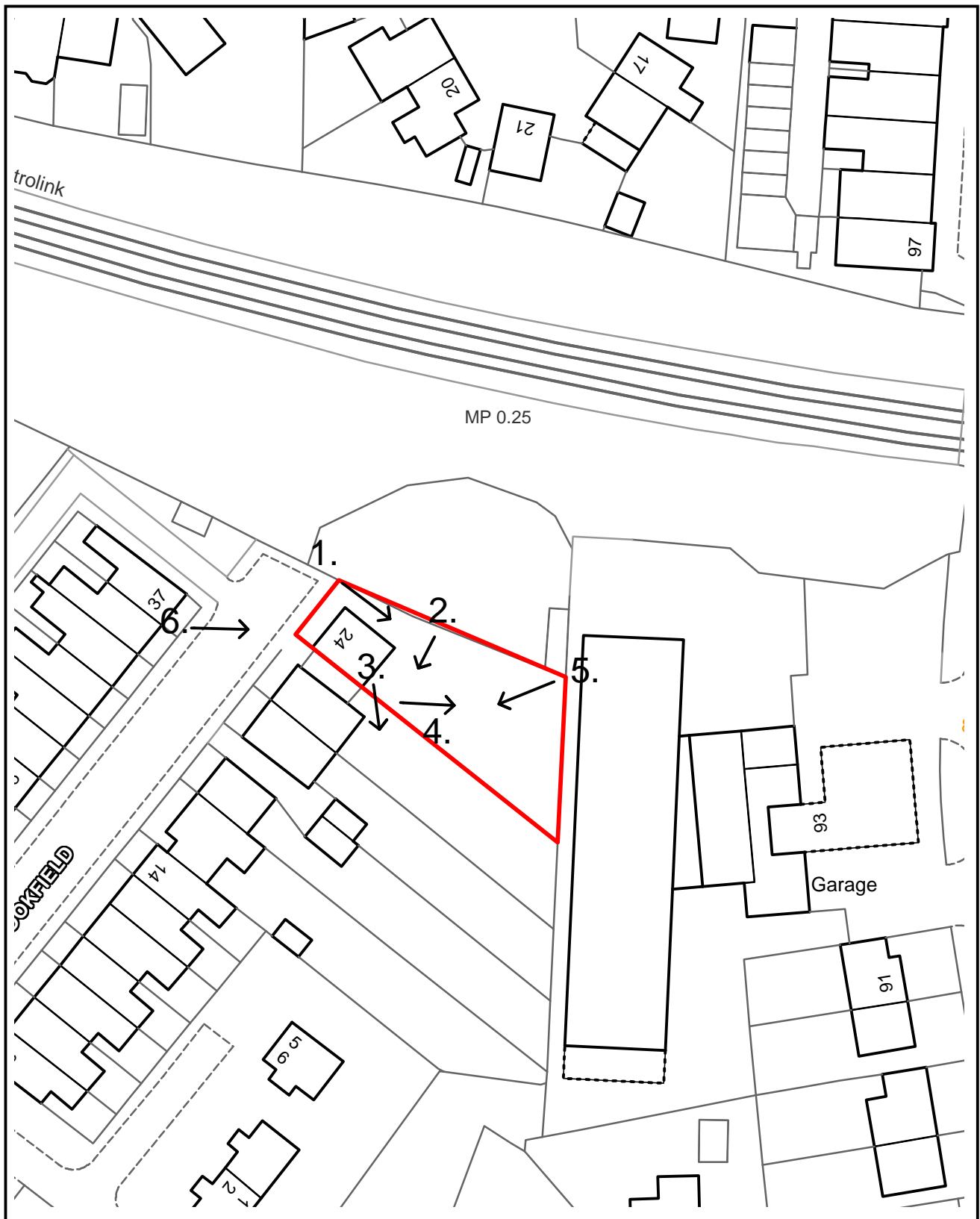
12. A landscaping scheme shall be submitted to, and approved by the Local Planning Authority prior to the commencement of the development. The contents of the plan should include native tree and shrub planting and the provision of bat bricks/tubes within the new development and bat and bird boxes. The approved scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree planting season,; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and

Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

13. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
14. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking or re-enacting that Order with or without modification), no development within Class A, AA and B of Part 1 of Schedule 2 shall be carried out unless permission is granted by the Local Planning Authority.
Reason - In order to protect the amenity of neighbouring properties and the appearance of the area in general, pursuant to Policy EN1/2 of the Bury Unitary Development Plan.

For further information on the application please contact **Dean Clapworthy** on **0161 253 5317**

67953 - Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 67953

ADDRESS: 24 Brookfield Prestwich



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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Item 01 – 67953

Photo 1



Photo 2



Photo 3



Photo 4

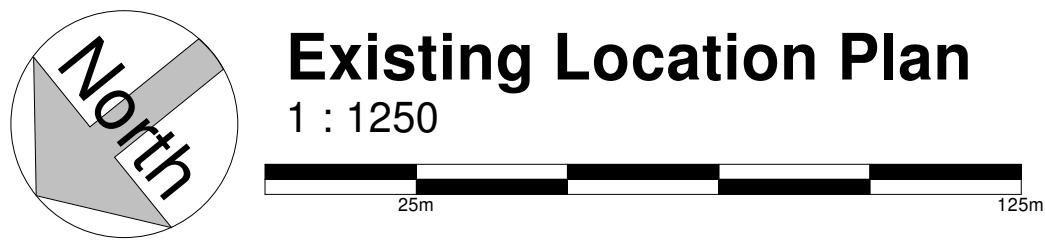


Photo 5



Photo 6





SEED
Architects

Parsonage Chambers
3 Parsonage
Manchester
M3 2HW

● Tel: 0161 832 5750 ● E: office@seedarchitects.co.uk ●

status
For Planning

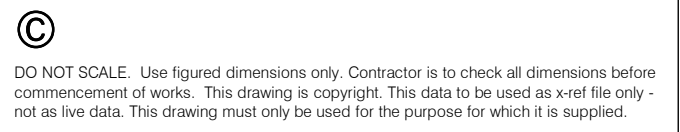
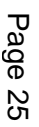
Job title
24 Brookfield

Location

Site Location Plan

author LP	date 09/12/2021	
checker RH	date 09/12/2021	
scale 1 : 1250		paper size A1

Project no. 3268	Type	Seed drawing no. 00-000	Rev.
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SEED
Architects.

Parsonage Chambers
3 Parsonage
Manchester
M3 2HW

● Tel: 0161 832 5750 ● E: office@seedarchitects.co.uk ●

job title
24 Brookfield | Prestwich

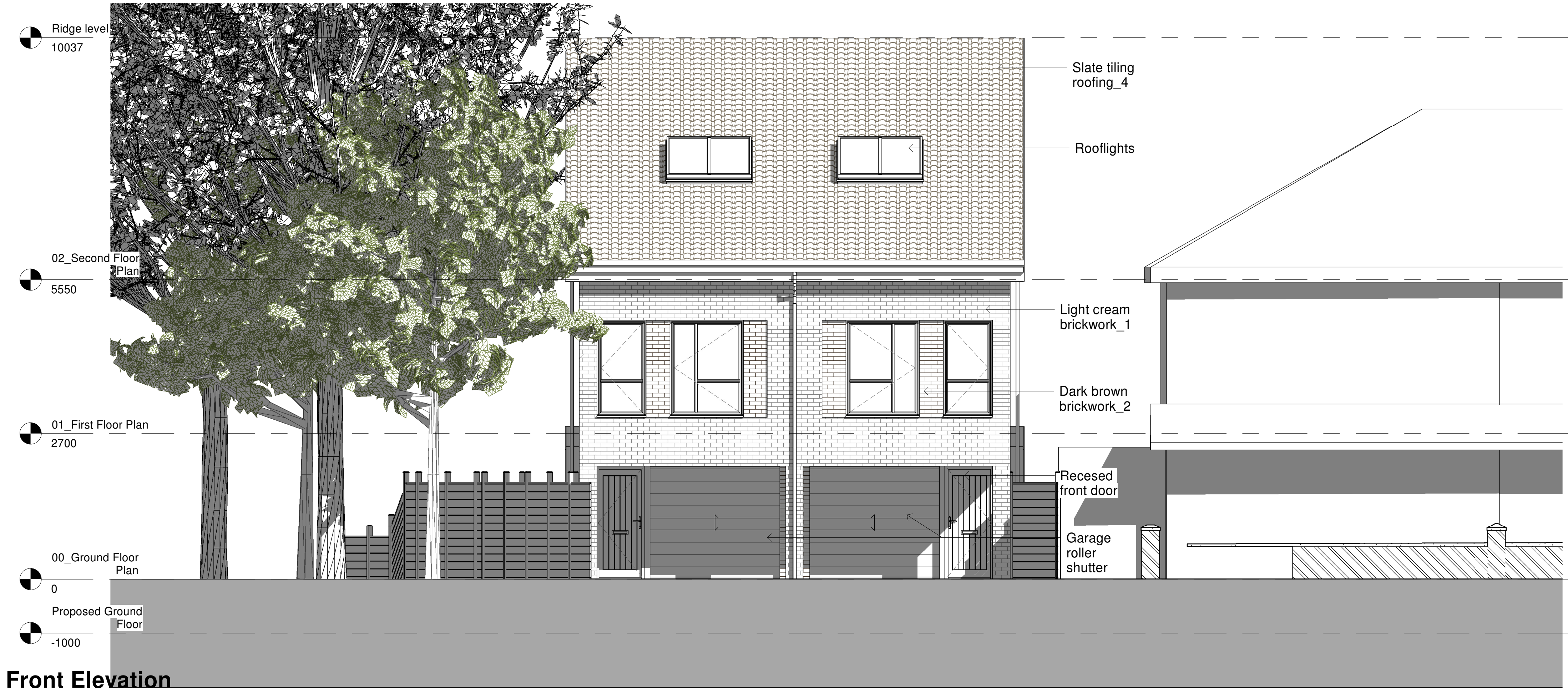
Location

drawing title

Proposed Site Plan

author LP	date 08/02/2022
checker RH	date 08/02/2022
scale 1 : 100	paper size A1

Project no. 3268	Type	Seed drawing no. 01_001_B	Rev. A
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Front Elevation
1 : 50



Rear Elevation
1 : 50



Light cream brick_1



Dark brown brick_2



GRP roofing_3



Slate tiling roof_4



Timber fencing_5



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NOTES:

Revision Number	Date	Description of Revision	Chkd	Appr
B	16/08/22	Amendments following planner comments		
A	15/08/22	Amendments following planner comments		

SEED Architects!
Parsonage Chambers
3 Parsonage
Manchester
M3 2HW
● Tel: 0161 832 5750 ● E: office@seedarchitects.co.uk ●

status
Planning Permisson

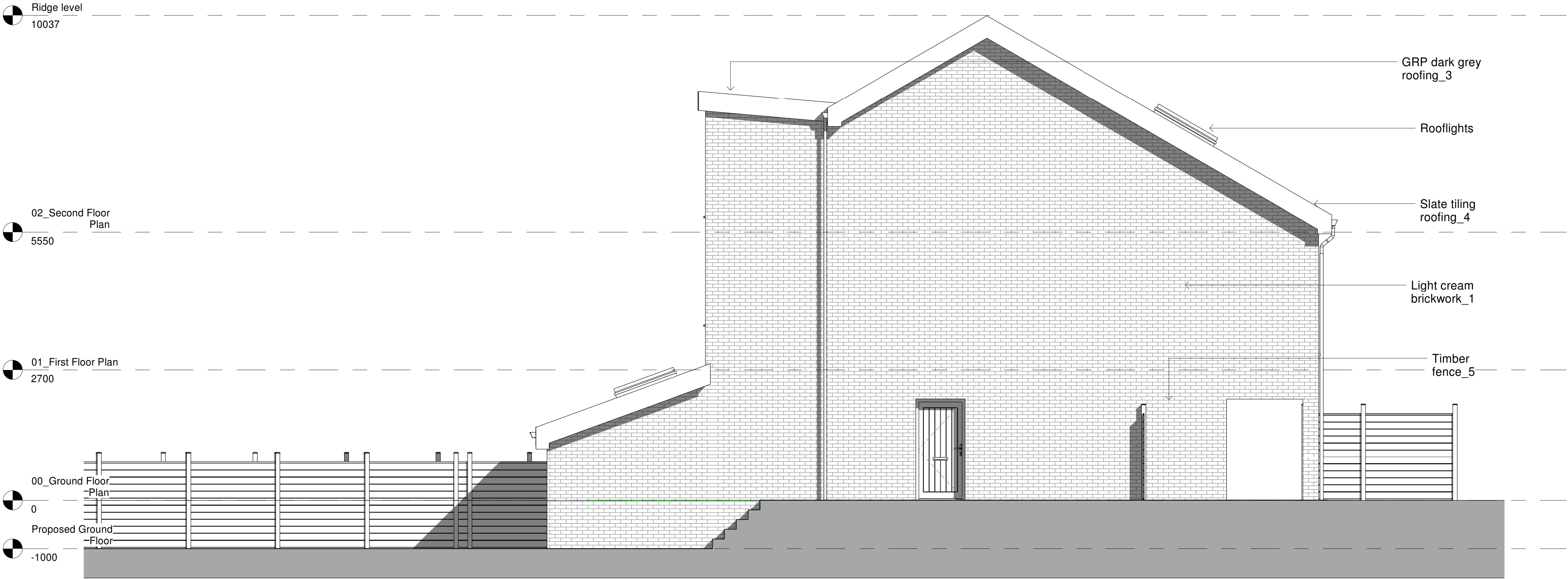
job title
24 Brookfield | Prestwich

Location

drawing title
Proposed Elevations

author DR	date 08/02/2022
checker RH	date 08/02/2022
scale As indicated	paper size A1

Project no. 3268	Type	Seed drawing no. 01_006_B	Rev. B
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North Side Elevation
1 : 50



South Side Elevation
1 : 50



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NOTES:

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SEED Architects!

Parsonage Chambers
3 Parsonage
Manchester
M3 2HW

● Tel: 0161 832 5750 ● E: office@seedarchitects.co.uk ●

status
Planning Permisson

job title
24 Brookfield | Prestwich

Location

drawing title
Proposed Elevations

author DR	date 08/02/2022
checker RH	date 08/02/2022
scale 1 : 50	paper size A1

Project no. 3268	Type	Seed drawing no. 01_007_B	Rev. B
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Ward: Radcliffe - East

Item 2

Applicant: Kellen Homes Limited, Seaham Estates and Cantt Pak Limited

Location: Land at York Street/Bury Road, Radcliffe, Manchester, M26 2GL

Proposal: Full planning permission for 211 dwellings, together with associated car parking, landscaping, public open space, drainage, the laying out of roads and footways and other associated works including engineering operations to create flood defences and the development platform

Application Ref: 68368/Full

Target Date: 23/08/2022

Recommendation: Minded to Approve

It is recommended that this application is Minded to Approve subject to the signing and completion of a Section 106 agreement for recreation provision and public access thereto in accordance with Policy RT2/2 of the adopted Unitary Development Plan and SPD1 and for affordable housing commuted sum in accordance with Policy H4/1 of the UDP and SPD5. If the agreement is not signed within a reasonable timeframe, then delegated authority is sought by the Development Manager to determine the application.

Description

The site relates to 6.26 hectares of land which falls into two main parcels to the immediate west of the River Irwell. The southern parcel comprises land formerly occupied by Unifi Dyed Yarns Ltd. situated to the south of an existing logistics and distribution centre (currently occupied by Morrison's) and is accessed via York Street. This part of the site has now been cleared and a raised platform constructed under extant planning application (ref: 60418). The northern parcel comprises open land to the north of the existing logistics and distribution centre and is accessed via Bury Road. Under the above extant planning permission, this area could be developed with industrial/employment units.

To the west of the site is Bealy's Goit and Swan Lodge and York Street to the south comprises mainly residential properties. The River Irwell forms the easterly and northerly boundary of the site and further to the north across the river is a primary school and an extensive residential area.

The application seeks to redevelop the site for residential development for 211 no. dwellings with associated car parking, landscaping, public open green space, drainage, the laying out of roads and footways and other associated works including engineering operations to create flood defences and the development platform.

Dwellings would be located in the southern parcel, with public open space provided in the northern parcel.

Two vehicular accesses are proposed from York Street to serve the proposed dwellings. An emergency access road (which is part implemented) is/would be routed adjacent to the western boundary of the site, which would connect the residential development to the private access serving logistics and distribution centre, around 120m east of the junction with Bury Road. The existing public right of way (PROW (ref: 3SM)) would remain, partially routed along the emergency access route. Four house types are proposed in a mix of two, three and four bedrooms dwellings.

All properties would have driveway parking for two cars. Rear gardens would be bounded by 1.8m fences. Acoustic fencing is proposed to the north of the residential development that would comprise two full length rows installed north of Road 4. The northernmost row of acoustic fence, which would be 4.435m, would be positioned at the bottom of the slope adjacent to the boundary with the distribution centre and a second row positioned at the top of the slope, comprising a 5m high fence upon a 1.1m bund. Further 2.4m acoustic fencing would be installed to garden boundaries at the east of the site (plots 109-126) and plots 82-83 and 50-51. 1.2m high timber ranch fencing and 0.5m high timber knee rails would be installed around site perimeters.

House types would comprise a mix of semi-detached and detached dwellings featuring hipped and gable roofs and would be finished with red brick elevations with contrasting brick detailing, window head and cills, and grey and red roof tiles. The most northerly road (Road 4) would be flanked on the south side by a continuous terrace comprising 33 no. dwellings.

Dwellings would have either in-curtilage parking with two spaces to the front of the property or a tandem double length drive to the side. The terrace block at Road 4 would be provided with either in-curtilage parking or parking bays on the north side of the road.

The proposed development includes raising ground levels to provide flood defences to York Street and the existing residential area to the south. The proposed ground levels would be set at 70.57m above ordnance datum (AOD), above the 1 in 100 years plus climate change level. The development platform would connect to the EA flood defence scheme approved under application ref: 68541.

Extensive landscaping would be introduced throughout the scheme in addition to retained trees. The palisade fencing to the boundary with York Street would be removed. Residential gardens would be landscaped with lawn and ornamental planting southern parcel, whilst other areas within the southern parcel would have wildflowers, native lawn mix and shrubs. The northern parcel would perform a dual function as public open space and functional flood plain. Existing trees, vegetation and grassland would be retained and there would be additional mixed native shrub planting. Hard landscaping would include the provision of a bound gravel footpath with timber bench and waste bin. A 20m riparian strip would be retained throughout the scheme, which would be subject to enhanced shrub planting.

The application has been amended since the original submission to provide noise mitigation from the adjacent site, which is occupied by the logistics and distribution unit to the north. This included reconfiguring the road layout and acoustic fence and introducing a terraced block at the north of the residential parcel. Further adjustments were also made to the house type mix and the extent of the riparian strip on the west bank of the River Irwell.

All dwellings would be for private rent. No affordable housing is proposed.

Relevant Planning History

Residential site

68994: Non-material amendment following approval of planning permission ref no. 60418 - Increase the width of the emergency access road. Approved 15/11/2022.

68031: Non-material amendment following approval of planning permission 56744- Raising of the level of the emergency access road to 70.89m A.O.D to align with the current EA flood modelling work. Approved 16/03/2022.

65847: Full planning permission for 201 dwellings, together with associated car parking, landscaping, public open green space, drainage, the laying out of roads and footways and other associated works including engineering operations to create flood defences and the development platform. Undetermined.

63361: Reserved matters approval for the layout, appearance and landscaping relating to

60418 (Matters of scale and access have already been approved). Approved 15/03/2019.

60691: Variation of condition 22 of 56744 for amendment to access route adjacent to plots 35 & 36/Wincanton site to provide alternative routes. Withdrawn 05/06/2017.

60418: Variation of conditions 20,27,28,29 & 35 of approved planning application 56744 relating to: 20&35- surface water drainage scheme, 27- samples of materials, 28- lifetime homes, 29- footpath and cycle route to vary the timing of submission for implementation. Approved 13/12/2018. This consent has been commenced and is therefore extant.

56744: Full planning permission for the erection of 153 no. dwellings together with associated works including the laying out of public open space, and the undertaking of engineering operations to remediate the site, raise the levels, construct an emergency access and development platform for future commercial development; Outline planning application for erection of a Class B1/B2 B8 development of 7435 m2. Approved 09/10/2015.

53645: Erection of 200 dwellings; new warehouse (7435 square metres) and land remediation and raising of land; raised emergency access to Bury Road; open space and car parking. Approved 20/04/2011.

Flood mitigation works

65256: Non-material amendment following grant of planning permission 64788: Localised modifications to the defence wall alignment. Specifically, this includes proposals to reposition the defence further away from the north corner of the existing brick building to minimise disturbance of the existing river walls, improve clearance and reduce risk to the existing building. In addition, the section downstream of Bealey's Weir connecting to the Warth Road Bridge is proposed to be repositioned to now align with the front of the existing river wall. Approved 31/03/2020

64790: A series of sheet piled walls and concrete walls across 4 sites at land to west of Metrolink line at Warth Road; Land to west of Bury Road; Land to east of Whitefield Road; Land to west of Hardy's Gate Bridge and Land to north of York Street, Bury. Approved 22/01/2020.

64789: Erection of walls between 1.5m & 3.5m in height to form a flood defence on the land south of Central Avenue & Keswick Drive, Bury. Approved 18/12/2019.

64788: Erection of retaining walls and sheet piled walls between 1m & 3m in height to form a flood defence between the Metrolink bridge and Warth Bridge. Approved 17/12/2019.

Publicity

In relation to the original scheme

4 representations were received raising matters that can be summarised as:

- Invasive plants grow on the proposed area of recreation land at the north that could be potentially harmful and there is noise pollution that has been ongoing for a number of years emanating from the existing warehouse on a 24 hour basis. Why can't fast growing evergreen trees be planted on the north side to try and eradicate some of the noise. Despite what is presented in the application, (occupants of) any new dwellings will hear the noise.
- Why is the Hogweed and Japanese Knotweed on the River Irwell banking not being treated? On-going construction activity at the site (under extant planning permission) are spreading the invasive plants and vehicles are not being washed down to stop the spread. Invasive plants should be treated on a year on year basis considering that it can take 15/20 years to completely eradicate them. Invasive plant spread could affect the ability to secure mortgages.
- What about road run-off (into the River Irwell), such as from residents washing cars.

The Environment Agency are aware of on-going issues in relation to existing businesses washing vehicles.

- The drainage in the York Street area is very poor. Local drains have been blocked by mud and debris from the site. Will the development further affect drains?
- Houses on Ripon Close are affected by water running into their gardens from the goits. Has this been considered?
- The flood defences built along York Street is very good, but it stops where the proposed land starts and where the houses are to be built (hence the flooding in 2015). The bund is insufficient.
- Most houses own two vehicles, so this totals 422 vehicles using York Street. There are large trucks from the neighbouring chemical works. The amount of traffic is going to be very heavy & we have cars speeding. The emergency vehicle access that was constructed got ripped up at the bottom part, so what is happening with this right of way.
- Large trucks using York Street/Bealey Avenue have caused damage to the road and footways.

Peacock & Smith (agent)

- The operators of the DHL (warehouse/distribution) site, which adjoins the application site to the north, and which is within a designated employment area. The site has had considerable investment since 2019 and there are plans to invest further. The occupants operate unrestricted from the premises 24 hours a day daily, year round and employ 350 people at the site.
- The occupant is mindful of the extant planning permission for 153 dwellings. However, the current proposal would bring dwellings in much closer proximity to the DHL site. As the proposal would increase in the number of dwellings and bring them closer a professional review of the submitted noise impact assessment (NIA) has been undertaken.
- The review has raised significant concerns over the methodology adopted in the supporting NIA and the conclusions reached. These are based on the reviewer having undertaken two 18-hour continuous noise surveys at two positions along the northern boundary.
- NPPF paragraphs 185 and 187 are pertinent. The applicant has failed to adequately assess the impact of noise from the DHL site and specifically in relation to the 24 hour nature of the operation. There are very serious concerns about the ability of the established use to operate unrestricted and this therefore puts into jeopardy the long-term future of the facility and any further investment in the site.
- National Planning Policy is very clear that new development should be integrated effectively with existing businesses, and that existing businesses should not have unreasonable restrictions placed on them as a result of development permitted after they were established - as potentially is the case now.

This representation includes a review by Sharps Redmore of the submitted NIA, which contends that it is inadequate/incomplete and in comparison with the SR noise survey data is considered likely to misrepresent the existing noise climate, particularly at the critical northern site boundary. Further, the review finds the use of BS 8233 (Sound Insulation and Noise Reduction for Buildings) as a primary method of assessment to be inadequate, stating that BS 4142 (Methods for rating and assessing industrial and commercial sound) is the more appropriate method. Thus, it is considered that the NIA fails to adequately assess the impact of noise from the distribution centre on the proposed residential development to the north and would potentially cause unreasonable restrictions on the continued 24 hours operation there.

In relation to the amended scheme

One further representation was received from Sharps Redmore, raising the following:

- In terms of noise the revised site layout plan offers a considerable improvement on the previous site layout plans; this is by virtue of the proposed continuous terrace row of properties along the northern part of the site, that will provide a noise barrier to the properties further into the site to the south.
- Despite the revised layout plan, there is continued concern about the high potential for

future noise complaints from existing lawful 24 hour Morrison distribution centre activities, particularly should windows in the properties closest to the northern site boundary be of openable type.

- Version 5 of the E3P noise assessment report continues to contain a number of serious omissions and flaws, presented in earlier noise assessments, that have not been addressed. These are explained within this technical note.
- The reliance on the noise assessment report for the consented residential scheme is worrying, since it too is fundamentally flawed. Whilst it is understood that the extant residential planning permission could be built out, the accompanying REC noise assessment (prepared by the same author as the latest E3P report), proposed noise mitigation measures and the level of scrutiny of that noise assessment by Bury Council Officers fell well short of acceptable standards, all to the detriment of the operator of the distribution centre.
- Despite the improvements to the proposed site layout from a noise perspective, the latest E3P report still appears to contain a number of fundamental errors that means the true noise impact remains unquantified. It is of significant concern that Bury Council have indicated that they do not have the necessary in-house expertise to adequately determine this planning application.

Statutory/Non-Statutory Consultations

Traffic Section: No objection, subject to conditions.

Borough Engineer - Drainage Section: No comments received

Public Rights of Way Officer: No objection, subject to conditions.

Environment Agency: No objection, subject to conditions relating to the raising of river bank ground levels, landscape and ecological management, flood risk, surface water infiltration, contamination and remediation and piling.

Greater Manchester Police - designforsecurity: No objection. Having looked at the proposed development, we would support the application subject to the layout issues within Section 3.3 being addressed and recommend that the physical security measures within Section 4 of the Crime Impact Statement are conditioned.

United Utilities (Water and waste): No objection, subject to conditions relating to surface water and sustainable drainage management.

Greater Manchester Ecology Unit (GMEU): No objection, subject to conditions relating to nesting birds, invasive species management, protection of the River Irwell, Otter protection, management of the riparian strip, enhancements for bats and compliance with the landscape management plan.

Electricity North West Ltd: No comments received

Environmental Health - Contaminated Land: No objection subject to conditions relating to contaminated land investigation, risk assessment, remediation and verification, the installation of electric vehicle chargers and a scheme for the minimisation of dust emissions.

Environmental Health - Pollution Control: The proposal should be subject to specialist review in relation to noise impacts.

G.L.Hearn (Independent Review for noise impacts): The latest design includes some elements of good acoustic design, such as those referenced in Planning Practice Guidance (PPG). These include a double barrier system along the north boundary with the site at the north, a buffer area with no sensitive uses between the north boundary and the first row of

buildings and a 'sealed' facade for the first row of buildings, with improved glazing, roof insulation and, where relevant, a specific ventilation strategy.

All the dwellings are provided with private gardens and the proposed development also includes an open space north of the adjacent distribution centre, which may partially off-set the reported noise impacts.

In relation to noise and vibration policy (NPPF paragraphs 174 and 185):

- The proposed development prevents unacceptable levels of noise pollution due to the fact that significant adverse effects are predicted to be limited to daytime periods and the noise surveys by e3p and Sharps Redmore indicate that the occurrences of these highest noise levels during daytime are rare.
- The BS 4142 assessment identifies the potential presence of significant adverse effects before the context is taken into account. Significant adverse effects should be avoided as per NPPF paragraph 185. Nevertheless, once the context is taken into account (such as the incorporation of design measures that secure good internal and outdoor acoustic conditions – BS 4142 11.3) the proposed development appears to be in accordance with NPPF 185, that is, the proposed development mitigates and minimise any adverse effects although significant and non-significant adverse effects will remain. The provision of an open space, classified as an ecological corridor, to the north of the Morrisons site but connected to the residential site, might provide a relatively tranquil open space within walking distance of the development, which is also in line with NPPF paragraph 185 and PPG-Noise.
- The Noise Policy Statement for England, which is referred to in NPPF, states that there is a need to integrate consideration of the economic and social benefit of the activity or policy under examination with proper consideration of the adverse environmental effects, including the impact of noise on health and quality of life. This should avoid noise being treated in isolation in any particular situation, i.e. not focusing solely on the noise impact without taking into account other related factors.

In relation to the agent of change principle (NPPF paragraph 187) the assessment identifies significant adverse effects under the assessment criteria in BS 4142. When compared to the noise levels measured during the noise surveys, the use of the same noise emission values for daytime and night-time might result in the assessment of a worst-case scenario rather than a typical scenario. The applicant has identified the mitigation measures to address these potential significant effects based on a worst-case scenario. There is a risk that the application is not strictly in line with NPPF paragraph 187 based on this worst-case scenario. However, considering the actual daytime noise levels measured during the surveys this risk is reduced.

Paragraph 009 of the PPG states that 'it can be helpful for developers to provide information to prospective occupants about mitigation measures that have been put in place, to raise awareness and reduce the risk of occupancy complaints.

Waste Management: No objection, subject to the roads being adopted and being able to withstand a 26 tonne refuse vehicle.

The Coal Authority: No objection, subject to conditions requiring past coal mining investigation, remediation and verification.

Greater Manchester Fire & Rescue Service (GMFRS): No objection. The emergency access road should meet the access requirements of Approved Document B, Volume 1 and be a minimum width of 3.7m and capable of carrying 12.5 tonnes.

Transport for Greater Manchester (TfGM): No objection, subject to a condition requiring a full Residential Travel Plan. A financial contribution should be made towards the upgrade and revalidation of MOVA (Microprocessor Optimised Vehicle. Actuation) at the junctions of Manchester Road/Dumers Lane and Dumers lane/York Street to improve traffic light operation and efficiency at junctions.

This would help mitigate the increased delay as a result of the increased traffic flows and changes in traffic patterns. This would ensure that the network operates as efficiently as possible to ensure that congestion is minimised. This mitigation measure is considered necessary to support and accommodate the development on the network.

Pre-start Conditions - The agent has agreed with pre-start conditions

Unitary Development Plan and Policies

EC1	Employment Land Provision
EC1/1	Land for Business (B1) (B2) (B8)
EC2	Existing Industrial Areas and Premises
EC2/1	Employment Generating Areas
EC6/1	New Business, Industrial and Commercial
H1	Housing Land Provision
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/5	Crime Prevention
EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN6/4	Wildlife Links and Corridors
EN8/2	Woodland and Tree Planting
EN6/1	Sites of Nature Conservation Interest SSSI's NNR's
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
EN7/3	Water Pollution
EN9	Landscape
OL5/2	Development in River Valleys
OL5/3	Riverside and Canalside Development in Urban Areas
RT1/1	Protection of Recreation Provision in the Urban Area
RT2	New Provision for Recreation in the Urban Area
RT2/1	Provision of New Recreation Sites
RT2/2	Recreation Provision in New Housing Development
HT5/1	Access For Those with Special Needs
SPD1	DC Policy Guidance Note 1: Recreation Provision
SPD2	DC Policy Guidance Note 2: Wildlife Links & Corridors
SPD3	DC Policy Guidance Note 3: Planning Out Crime
SPD4	DC Policy Guidance Note 4: Percent for Art
SPD5	DC Policy Guidance Note 5: Affordable Housing
SPD16	Design and Layout of New Development in Bury
SPD14	Employment Land and Premises
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

ASSESSMENT

Principle

The southern section of the application site falls within the Dumers Lane Employment Generating Area (EGA) and is subject to Unitary Development Plan (UDP) policies EC2/1 and EC2/1/9. These policies state that within the Dumers Lane EGA, the Council will only allow development for Business (B1 - now Class E), General Industrial (B2) and Warehousing (B8) uses. Other uses will only be allowed where they constitute limited development or do not substantially detract from the area's value for generating employment.

The northern section of the application site that would host a 3.29 hectare area of public open space is located in the River Valley, Wildlife Corridor and upon an allocated recreational area and is subject to UDP policies OL5/2, EN6/4 and RT1/1.

As noted above at 'Relevant Planning History', the site is subject to an extant planning permission (60418) for the erection of 153 dwellings. Given the commencement of the residential element, this development could be completed in perpetuity. The reserved matters permission 63361 for the erection of a Class B1/B2, B8 development lapsed and therefore the Employment Contribution of £514,800 was triggered. Therefore, the principle of residential development at the site is established.

In addition, the National Planning Policy Framework (NPPF) should be treated as a significant material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. The Framework maintains the emphasis on identifying a rolling five year supply of deliverable housing land.

Bury's Strategic Housing Land Availability Assessment sets out the latest housing supply position, which is made up of sites that have an extant planning permission and sites that have potential to obtain planning permission in the future. This shows that there are a number of sites within the Borough with the potential to deliver a significant amount of housing. However, not all of these sites will contribute to the five year supply calculations as many sites will take longer than five years to come forward and be fully developed (e.g. some large sites could take up to ten years to be completed). As such, latest monitoring indicates that the Council is unable to demonstrate a five year supply of deliverable housing land and this needs to be treated as a material factor when determining applications for residential developments.

The National Planning Policy Framework also sets out the Housing Delivery Test, which is an assessment of net additional dwellings provided over the previous three years against the homes required. Where the test indicates that the delivery of housing was substantially below (less than 75%) of the housing requirement over the previous years, this needs to be taken into account in the decision-taking process. The latest results published by the Government show that Bury has a housing delivery test result of less than 75%, and therefore this needs to be treated as a material factor when determining applications for residential development.

Therefore, in relation to the proposed dwelling, paragraph 11d) of the National Planning Policy Framework states that where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, planning permission should be granted unless:

- i. The application of policies in the Framework that protect areas, or assets of particular importance, provides a clear reason for refusing the development proposed; or
- ii. Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework, taken as a whole.

Therefore, in this case the 'titled balance' would apply and planning permission should be granted unless the above points Para 11(d) i or ii apply.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area, adjacent to established residential areas and close to routes well served by public transport and to facilities, goods and services.

Given the above factors, the principle of the development is acceptable, subject to consideration of the subsequent matters.

Loss of employment land

The Section 106 agreement attached to extant planning permission 60418 required the payment of an Employment Contribution of £514,800, pursuant to UDP Policy EC1/1/15 – Dumers Lane Employment Generating Area. Its purpose was to compensate for economic harm arising from the loss of the land, which is designated as protected employment land (Employment Generating Area). This related to the northern parcel the Employment Contribution was to be offset by any employment development delivered on the site, with the definition clause in the S106 setting the contribution as £514,800 minus any sq.m of floor space built on site multiplied by £69.24. Thus, the payment of the contribution was designed as a fallback in the event that the outline employment part of the consent did not come forward.

Further to the grant of planning permission 60418 and the severe flood events in the winter of 2015/16, the Environment Agency (EA) entered into discussions with the landowner in relation to potential flood defence works. As a result, the proposed implementation of the planning permission was put on hold while flood defence avenues were explored and a revised scheme considered. Discussions around the revised scheme have been on-going for some considerable time, with the EA and Council supportive of not delivering the employment land. The eventual conclusion was that the EA's preference for the betterment of the Greater Radcliffe area is to increase the residential platform in area (thus contributing to flood defence mitigation) and the previously approved and extant employment platform area remain undeveloped and used as open space. This would therefore prevent the landowner from delivering the consented employment use and thus being able to mitigate paying the compensatory Employment Contribution of £514,800.

The EA has confirmed that it has no objection, subject to conditions relating to the raising of river bank ground levels and tie in to the Radcliffe and Redvales flood risk management scheme. Therefore, in the scheme now proposed, the residential platform (southern parcel) and the northern parcel would provide future flood mitigation, enhanced open space and ecology/biodiversity, and would deliver significant environmental and social amenity benefits as a result.

Affordable housing and viability

UDP Policy H4/1 - Affordable Housing and Supplementary Planning Document 5 - Affordable Housing Provision in New Residential Developments with the First Homes Policy Position Statement, require that the development provides 25% affordable housing (i.e. 53 dwellings), of which 25% should be First Homes. Whilst Build to Rent schemes are exempt from the 10% requirement set-out in the National Planning Policy Framework, the UDP requirement pertains.

The proposal is supported by a Viability Assessment that concludes that the scheme cannot deliver any Section 106 contributions or affordable housing on the site when a scheme of market sale housing or build to rent is considered.

The Council has had this position independently assessed by a specialist third party. This confirms that the open market sales and build to rent (the intended format) appraisals with an on-site fully policy compliant affordable housing provision would be unviable. If additional Section 106 requirements were envisaged (e.g. for open space contributions -

see later section) then this would only increase the pressure on the scheme.

However, the independent appraisal finds that on the full build to rent scheme, with no affordable housing, then the proposal could provide a contribution of £207,299 towards affordable housing and any other additional policy requirements (See later section on Section 106 requirements).

In the first instance, affordable housing is typically expected to be provided on site. In this case, the applicant intends to make a financial contribution for off-site provision, noting that UDP Policy H4/1 states that there will be an 'emphasis towards encouraging' affordable homes as an integral part of large housing developments. They also highlight that Supplementary Planning Document 5 - Affordable Housing Provision in New Residential Developments, states that it is important that affordable housing for rent is made available through a Registered Social Landlord approved by the council. At the level of contribution available, the site could only deliver 2 affordable units on the site, but this would not be in perpetuity (given the limited available contribution). Thus, affordable housing could not be provided on site through a Registered Social Landlord.

Given these circumstances and the flexibility within the UDP policy, it is considered that a financial contribution would be most effective and therefore an acceptable proposition. A financial contribution would not therefore conflict with the stated UDP policy.

Open space and recreational provision

UDP Policy RT2/2 - Recreation Provision in New Housing Development, updated by Supplementary Planning Document 1 - Open Space, Sport and Recreation Provision in New Housing Development requires 2.4 hectares of Public Open Space (POS) per 1,000 population. On-site provision will only be considered in exceptional circumstances.

Applying an average occupation of 2.3 persons per dwelling, the proposal would create a new population of 485.3 persons, generating a requirement for 1.16 hectares of new POS. The proposal contains 3.29 Ha of open space (circa 2.13 Ha more than the requirement).

The proposed area of on-site open space would perform three functions for the purposes of recreation provision, habitat enhancement and flood risk mitigation. Given this, it is considered that exceptional circumstances pertain to justify on-site provision. Furthermore, the scheme would deliver almost three times the open space area than would be required by the above policy and enhancements to the River Valley and Wildlife Link and Corridor in which the northern parcel falls. The on-going management of the open space should be subject of a management plan secured via a Section 106.

Layout and design

H2/1 - The Layout of New Residential Development and H2/2 - The Layout of New Residential Development, provides the assessment criteria for detailed matters relating to height, appearance, density and character, aspects and finish materials.

The development platform has been implemented under the extant permission. The residential layout would be more extensive than that approved under the extant planning permission (60418), coming closer to the northern boundary with the adjacent logistics and distribution unit and the eastern boundary with the River Irwell.

This would appear to better relate to the physical and natural boundaries to the site, but retaining a riparian strip to the river and significant landscape buffers to York Street and the northern boundary. Rear gardens would be enclosed by timber fencing.

The proposed development would be relatively self-contained, being flanked by the River Irwell, commercial uses and Bealey's Goit to the east, north and west and set back from York Street with an intervening landscape buffer and retained conifers as a significant boundary. Therefore, the proposal would not be viewed in context with surrounding developments and would primarily be viewed at a standalone residential estate.

The layout of streets would be legible and the variety, form, proportions and materials of the house types would form harmonious and attractive streetscapes framed with substantial areas of structural landscaped areas.

The proposal would therefore comply with the above UDP policies and Section 12 of the NPPF.

Amenity

UDP policies H2/1 and EN1/2 and requires consideration of various factors relating to built design, including the relationship of the proposal to the surrounding area and impact on residential amenity.

Neighbouring amenity

As noted above, the proposed development is relatively self-contained. Nearest neighbouring dwellings are limited to those opposing the site on the south side of York Street. The linear 'street' at the southernmost extent of the site, whilst being positioned on a raised platform with principal elevations facing south towards opposing dwellings, would be around 30m distant. Additionally, the landscaping scheme proposes the retention of the existing linear stand of conifers adjacent to the boundary with York Street, aside from selected felling to create a new access into the site (Road 1), although this is not necessary in order to prevent unacceptable impacts on nearest neighbouring amenity.

Given the above, the proposal would not have any unacceptable impacts on existing residential amenity.

Amenity for future occupiers

The house types have been amended to ensure that they would all comply with the minimum gross internal floor area requirements and associated minimum bedroom dimensions of the 'Technical housing standards – nationally described space standard'. All dwellings would have generous private amenity space and the open space on the northern parcel would provide a large amenity area within easy walking distance.

UDP Policy EN7/2 states that the Council will not permit development close to a permanent source of noise.

As detailed above, the proposal is subject to a detailed objection concerning the potential noise impact on future occupiers of the scheme from the operation of the logistics and distribution centre to the immediate north of the residential parcel. In response, the applicant amended the proposed layout and updated the noise impact assessment in accordance with the more appropriate BD 4142 guidelines, which is used to assess the impact of industrial and commercial sound.

The assessment was conducted for the existing commercial and industrial sound sources to the north and south-east (with the adjacent distribution centre being found to be the main source of sound). It finds that with the present noise climate, additional acoustic barriers (a double acoustic fence) and high specification glazing would be required for the plots closest to the northern boundary (i.e. the continuous terrace block). Acoustic fencing is also required to the eastern garden boundaries at plots 109-126. It found that with such noise mitigation measures, that no adverse impacts are predicted day or night at any proposed dwelling across the site. It also notes that the present scheme with the proposed mitigation measures would result in a significant noise improvement over the completion of the extant consent.

The independent review is reported in detail above. This was undertaken in light of the all the submitted information and the objections. It advises that the latest design includes some elements of good acoustic design, such as those referenced in Planning Practice Guidance (PPG), including a double barrier system along the north boundary with the site at the north, a buffer area with no sensitive uses between the north boundary and the first

row of buildings and a 'sealed' facade for the first row of buildings, with improved glazing, roof insulation and, where relevant, a specific ventilation strategy.

It also notes that all the dwellings would be provided with private gardens and the proposed development also includes an open space north of the adjacent distribution centre, which could be considered to partially off-set the reported noise impacts.

There is a risk that the application is not strictly in line with NPPF paragraph 187, but that would be based on this worst-case scenario. When the actual daytime noise levels measured during the surveys are applied, this risk is reduced.

The potential implementation of the extant scheme is also a significant material consideration, as this would introduce a residential layout that would experience unacceptable adverse impacts on future occupier's amenity and additionally would not include significant open space to partially offset these impacts.

Given the above, it is considered that with the proposed mitigation measures, the development complies with the above UDP policy and the NPPF.

Access and highways

UDP policies H2/2 - The Layout of New Residential Development, HT2/4 - Car Parking and New Development, HT6/1 - Pedestrian and Cycle Movement and HT6/2 - Pedestrian/Vehicular Conflict are relevant.

The emergency vehicle access has now been implemented in accordance with the non-material amendment approved under 68994, which complies with the requirements set out in the response from the Greater Manchester Fire & Rescue Service

Transport for Greater Manchester (TfGM) have raised no objection. A full Residential Travel Plan would be required by condition and a financial contribution towards the upgrade and revalidation of MOVA (Microprocessor Optimised Vehicle Actuation) at the junctions of Manchester Road/Dumers Lane and Dumers lane/York Street to improve traffic light operation and efficiency at junctions would be secured by way of a Section 106 legal agreement.

The Highway Officer (with Public Rights of Way) has indicated that the development is acceptable in principle. The responses and any recommended conditions will be reported in the Supplementary Report.

The proposed development contains two parking spaces per dwelling. Supplementary Planning Document 11 recognises that the site is within a high access area and as such, the provisions of two parking spaces per plot minimum would be sufficient to provide adequate parking for the development. Each dwelling would also have a secure bicycle/motorcycle wall anchor installed.

Given the above, the proposal would not have any adverse impact on highway safety and there would be no conflict with the above UDP policies. The response from the Highway Officer and any recommended conditions will be reported in the Supplementary Report.

Ground conditions

The current use of the site splits it into two distinct parcels. The southern parcel was formerly occupied by the Unifi Dyed Yarns works including tanks, reservoirs and electricity sub stations, while the northern area is occupied by a former sports ground. Surrounding land uses include a chemical works, housing and industrial and commercial premises.

Previous historic land uses at the site and surrounding areas are many and varied and include heavy industrial uses. The site also lies within 250 metres of a landfill site known as Bury Road, Radcliffe Landfill which is located to the north west. The River Irwell flows along the eastern boundary of the site, while Bealeys Goit is located adjacent to the western

boundary. The site is underlain by alluvial deposits over Middle and Lower Coal Measures Secondary A aquifer. A fault cuts the site from north west to south east towards the south.

A number of Desk Study and Site Investigation reports have previously been submitted and reviewed. However, further assessment work including post enabling work gas monitoring and the provision of missing information and a remediation strategy is required.

The Council's Environmental Health Officer and the Coal Authority do not object to the proposal, subject to the above noted conditions.

Flooding and drainage

The implemented development platform is in accordance with the extant planning permission, which was thoroughly assessed in relation to flood risk and drainage matters.

The current proposal is supported by an updated Flood Risk Assessment (FRA). This confirms that the site falls within a Flood Zone 3 and that the proposed development passes the exception test as it is provided wider sustainable benefits as the development will provide a flood defence to the existing houses to the south of the site (as was concluded in relation to the extant planning permission). The platform would be set at a minimum of 70.57m above ordnance datum (AOD) on the north and eastern boundary, which would provide defences for the development and surrounding York street area to a level of protection to 1 in 1000 year event. Thereafter, the finished floor levels would be set at 70.72mAOD, which is also above the 1 in 1000 year event.

As noted above, both the Environment Agency and United Utilities are satisfied that the proposal is acceptable, subject to conditions.

Ecology and landscaping

The northern parcel (the proposed open space) falls within a designated Wildlife Corridor under UDP Policy EN6/4 and borders the Swan Lodge Site of Biological Importance (SBI) (EN6/2).

The application has been submitted with ecological assessments including an Extended Phase 1 Habitat Survey and Otter Survey.

The Greater Manchester Ecology Unit (GMEU) note that previous surveys found no evidence of protected species, though it was assumed that otter, kingfisher and bats utilise the River Irwell as a wildlife corridor and that similar surveys by the Environment Agency found no evidence of protected species residing on or adjacent to the site. Updated survey demonstrates that Otter (a European Protected Species (EPS)) are utilising the river adjacent to the development site, but are very unlikely to be straying on to the site and are therefore avoidable. The GMEU are satisfied that subject to a condition requiring agreement of a reasonable avoidance measures method statement and updated riverbank survey that the development would not impact upon EPS.

Japanese knotweed, giant hogweed and himalayan balsam are confirmed as still present but now restricted to the perimeter of the site. The amended Landscape Management Plan sets out the measures and monitoring for invasive species and the GMEU advise that this is acceptable, subject to condition.

The submission also includes details of landscaping, which includes an indication of extensive planting to the northern parcel and of the riparian strip and retention of the majority of the existing conifer trees to the York Street boundary. Due to the land raising that has taken place, the southern parcel contains negligible vegetation. Conditions relating to landscaping and ecological management, as advised by the Environment Agency and the GMEU are proposed.

Other matters

Crime and design

Greater Manchester Police have raised no objection to the proposal. Whilst stating that the proposed residential development would be acceptable in general, the supporting Crime Impact Statement (CIS) raises concerns relating to access to the riverbank to the rear of those dwellings backing onto it.

The proposal includes boundary fencing to the short section of non-domestic area with a boundary to the riverbank and 2.4m high acoustic fencing to those gardens backing onto the riverbank. Such measures could not guarantee against anti-social behaviour, but they go some way to meeting the recommendation made in the CIS.

Waste management

Each dwelling would have adequate areas for waste and recycling storage.

Planning contributions

As noted above, the scheme has been assessed as being able to make a commuted sum contribution of £207,299 in total.

The appraisal has identified a requirement for two contributions for affordable housing and for the upgrade and revalidation of traffic lights to improve efficiency at junctions close to the access into the site. TfGM have advised that the traffic light upgrade would require a fixed contribution of £30,000. This is clearly necessary to achieve the continued efficient operation of the local highway network.

It is therefore considered that it would be appropriate to allocate £30,000 as noted above and the remainder of £177,299 to off-site provision of affordable housing to be secured by a Section 106 Agreement, which should also secure the long term management and maintenance of the open spaces.

Observation on representations received

Many of the points raised in the representations have been addressed in the above analysis and/or recommended conditions.

CONCLUSION

Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that if regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be in accordance with the plan, unless material considerations indicate otherwise.

The principle of the development is established by the extant planning permission which could be completed in perpetuity. The proposal would deliver much needed residential development on previously developed land in a sustainable location. It would provide good quality rented properties in a layout that would create varied and attractive streets and spaces, framed in a wider context of robust and extensive landscaping and open space.

The scheme would also achieve bio-diversity net gain on site and provides flood mitigation not just for the development, but for existing properties on York Street. It would also achieve better noise mitigation than the extant scheme that would not unacceptably impact upon the amenities of future residents.

Other constraints including drainage and flooding, contamination, coal mining risk and air quality impacts have been adequately addressed.

On this basis, the scheme along with the legal agreement to secure the above noted contributions and on-going open space and landscaping maintenance and management, the development would comply with the Unitary Development Plan and the NPPF when considered as a whole.

As such, the proposal would comply with the above policies of the Local Plan and the

NPPF. Therefore, in accordance with Section 38(6) of the Planning and Compulsory Purchase Act, the proposal merits approval.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Minded to Approve

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.

Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. Subject to the requirements of the conditions below, the development hereby permitted shall be carried out in accordance with the following approved drawings and documents:

Drawings

Location Plan (KH-005-LP)
Planning Layout (KH005-PL2 Rev U)
Planning Layout (Composite) (A-100 Rev R)
Waste Management Plan (KH-005_A-700 Rev: C)
Boundary Treatments (KH-005-BT Rev F)
Materials (KH-005-MAT Rev F)
Housetype Pack - Rev B
Garth Up - Elevations & Floor Plans
Access Road Alignments and 3.7m Plan - Designed to accommodate 12.5 tonne gross weight (PIN Property)
Emergency Access Road Sections (2727_201 Rev C)
Emergency Access Road Plan (Dwg No. 2727 200 B)
Street Scenes (KH-YSR-SS-01)
Plots 50 to 82 Street Scene (KH005 A-600)
Indicative Floor Levels (KH-E-001-FFL Rev A)
Works to Southern and Western Sections (KH-E-005-EW-102 Rev B)
Works to Northern and Eastern Boundary (KH-E-005-EW-101 Rev H)
Landscape Layout (sheet 1) and Wetland Area (sheet 2) (ref: P.1639.22.01 Rev E)
Planting Plan - Wetland Area (P.1639.22.01 Rev A)
Site Discharge Locations (005_1001)
Adoptable Drainage Layout (40-02 Rev P6)
Long Sections (RSK - 40-11-01 Rev P6, 40-11-02 Rev P4, 40-11-03 Rev P5)
Highways response plan (KH005 ENG01)
Forward visibility plan (KH005 ENG02)
Swept Path Analysis (3690-SP01 Rev E)
Section_5 (Dwg No. A-104)

Documents

Extended Phase 1 Habitat Survey, Rachel Hacking Ecology, (March 2022)
Otter Survey, Rachel Hacking Ecology (August 2022)

Biodiversity Impact Assessment Rev B and Biodiversity net gain Metric 3.0, Rachel Hacking Ecology (August 2022)
 Arboriculture Impact Assessment, Urban Green (Ref: UG1521)
 Crime Impact Statement (ref: 2016/0037/CIS/02, Version A)
 Noise Impact Assessment, E3P (Ref: 50-586-R1-6)
 Landscape Management Plan (Ref: P.1639.22 Rev C)
 Invasive Weed Management Plan, York Street, Radcliffe, Manchester, M26 2GL, Knotweed Eradication, Reference: IWMP2369A, 7 July 2022.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence unless and until:-
 - Post-mine stabilisation/grouting works gas monitoring results and risk assessment report has been submitted to, and approved in writing by the Local Planning Authority;
 - Environment Agency approval is received by the Local Planning Authority for the updated controlled waters risk assessment and any proposed remedial actions;
 - A detailed Build Phase Remediation Strategy has been submitted to, and approved in writing by, the Local Planning Authority.
 Thereafter, the development shall be implemented in full accordance with details agreed in relation to the above matters.
Reason. - The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

4. Following the provisions of condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out within timescales agreed with the Local Planning Authority; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the first dwellinghouse.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. No development shall commence unless and until a scheme to minimise dust emissions has been submitted to and approved in writing by the Local Planning Authority, as outlined in Table 20 of the Air Quality Assessment report (Redmore Environmental, Reference: 5490r1, 5 April 2022). The scheme shall include details of all dust control measures and the methods to monitor emissions of dust arising from the development. The construction of the development shall thereafter be implemented in accordance with the approved scheme, and the approved dust control measures shall be retained and maintained in a fully functional condition for the duration of the construction of the development hereby approved or to an alternative timescale previously agreed with the Local Planning Authority.
Reason. The information is required prior to the commencement of development in order to reduce the impacts of dust disturbance from development activity at the site on the local environment, pursuant to Policy EN7/1 of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

6. Each dwelling hereby approved shall be provided with 1 electric vehicle (EV)

charge point (minimum 7kW*) prior to its occupation.

*Mode 3, 7kW (32A) single phase, or 22kW (32A) three phase, and for 50kW Mode 4 rapid charging may be required. British Standard BS EN 61851-1:2019 to be used.

Reason. To encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life, pursuant to Section 9 of the National Planning Policy Framework.

7. No infiltration of surface water drainage into the ground, where adverse concentrations of contamination are known or suspected to be present, is permitted, other than with the written approval of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to controlled waters. The development shall be carried out in accordance with the approval details.
Reason. To ensure that the development does not contribute to and, is not put at unacceptable risk from or adversely affected by unacceptable levels of water pollution, pursuant to policies EN7/3 and EN7/4 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.
8. Piling or any other foundation designs using penetrative methods shall not be utilised unless prior written approval is agreed with the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. Thereafter, such methods shall only be carried out in full accordance with the approved details.
Reason. To ensure that the development does not contribute to and, is not put at unacceptable risk from or adversely affected by unacceptable levels of water pollution, pursuant to policies EN7/3 and EN7/4 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.
9. The raising of ground levels adjoining the River Irwell must not be commenced unless and until details (including finished levels) showing the tie-in arrangements to the Radcliffe & Redvales flood risk management scheme defences has been submitted to and approved in writing by the Local Planning Authority.
Reason. To ensure flood defences are continuous and structural integrity is maintained thereby reducing the risk of flooding, pursuant to Policy EN5/1 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.
10. The landscaping of the wetland area and 20m riparian corridor within the northern parcel shall be carried out in accordance with the approved planting plan (P.1639.22.01 Rev. A) prior to any development comprising the erection of external walls of the first dwellinghouse. Thereafter, the maintenance and management of the implemented landscaping scheme shall be carried out in accordance with the recommendations and measures set out within the Landscape Management Plan by Ascerta (Reference no. P.1639.22 Rev. C) submitted with the application.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity and to ensure the protection of wildlife and supporting habitat and to secure opportunities for enhancing the site's nature conservation value, pursuant to policies EN6/4, EN8/2 and EN10/2 of the Bury Unitary Development Plan and Section 15 of the National Planning Policy Framework.
11. A landscape and ecological management plan, including long-term design objectives, management responsibilities and maintenance schedules for all landscaped areas in and around the residential development (except privately owned domestic gardens), and including the riparian corridor, for the southern residential parcel shall be submitted to, and approved in writing by the Local Planning Authority prior to any development comprising the erection of external walls of the first dwellinghouse. The landscape and ecological management plan

shall be carried out only in accordance with the approved details or any subsequent variations that shall be agreed in writing by the Local Planning Authority. The scheme shall include the following elements:

- details of maintenance regimes
- details of any new habitat created on site
- details of treatment of site boundaries and/or buffers around water bodies
- details of management responsibilities

Reason. To secure the satisfactory development of the site and in the interests of visual amenity and to ensure the protection of wildlife and supporting habitat and to secure opportunities for enhancing the site's nature conservation value, pursuant to policies EN6/4, EN8/2 and EN10/2 of the Bury Unitary Development Plan and Section 15 of the National Planning Policy Framework.

12. No development shall commence unless and until a full Arboricultural Method Statement, that takes cognisance of the advice contained within Section 4 of the Arboricultural Impact Assessment (Project No. UG151) by Urban Green, has been submitted to and approved in writing by the Local Planning Authority. Thereafter, the works shall implemented only in accordance with the approved details.

Reason. The information is required prior to the commencement of development in order to preserve trees on the site in the interests of visual amenity and the character of the area, pursuant to Policy EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

13. No works to trees or shrubs shall occur, or earthworks commence, between the 1st March and 31st August in any year, unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance or commencement of earthworks. The bird nest survey shall thereafter be submitted to the Local Planning Authority in tandem with the clearance or earthworks.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

14. The treatment of invasive species shall be carried out in accordance with the recommendations and mitigation measures set out within the Landscape Management Plan by Ascerta (Reference no. P.1639.22 Rev. C) submitted with the application.

Reason. To ensure the removal/check to the spread of species that are listed as invasive non-native species under the Wildlife and Countryside Act 1981, in order to safeguard the biodiversity of the site and its surroundings, pursuant to Policy 6/4 of the Bury Unitary Development Plan and Section 15 of the National Planning Policy Framework.

15. No development, site clearance or earth moving shall take place or material or machinery brought on site until a method statement to protect the River Irwell from accidental spillages, dust and debris has been supplied to and agreed by the Local Planning Authority. Thereafter, the approved measures shall be implemented in full and maintained for the duration of the construction period or for any other period as may subsequently be agreed in writing by the Local Planning Authority.

Reason. This information is required prior to the commencement of development to ensure that the development does not contribute to, and is not put at unacceptable risk from or adversely affected by unacceptable levels of water pollution, pursuant to policies EN7/3 and EN7/4 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.

16. Prior to any earthworks, development or landscaping, a reasonable avoidance

measures method statement for otter, including an updated survey of the riverbank immediately adjacent to the development for otter holts, will be provided to and agreed in writing by the Local Planning Authority. Thereafter, the agreed reasonable avoidance measures shall be fully implemented and adhered to until completion of the development or for any other period as may be agreed in writing by the Local Planning Authority.

Reason. This information is required prior to the commencement of development to ensure that development activity does not cause harm to a Protected Species, pursuant to policies EN6 and EN6/3 of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

17. Prior to any development comprising the erection of external walls of the first dwellinghouse, a bird and bat mitigation and enhancement strategy of integrated bird and bat bricks targeting house sparrow and crevice dwelling bats, written by a suitably qualified ecologist, shall be submitted to and agreed in writing by the Local Planning Authority. Thereafter, the mitigation and enhancement measures shall be implemented in full accordance with the approved details.

Reason. In order to ensure that development activity does not cause harm to a Protected Species, pursuant to policies EN6 and EN6/3 of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

18. No development shall commence until;
a) a scheme of further intrusive investigations has been carried out on site to establish the risks posed to the development by past coal mining activity, and;
b) any remediation works and/or mitigation measures to address land instability arising from coal mining legacy, as may be necessary, have been implemented on site in full in order to ensure that the site is safe and stable for the development proposed.

The intrusive site investigations and remedial works shall be carried out in accordance with authoritative UK guidance.

Reason. The undertaking of intrusive site investigations, prior to the commencement of development is considered to be necessary to ensure that adequate information pertaining to ground conditions and coal mining legacy is available to enable appropriate remedial and mitigatory measures to be identified and carried out before building works commence on site. This is in order to ensure the safety and stability of the development, pursuant to Section 15 of the National Planning Policy Framework.

19. Prior to the occupation of the development, or it being taken into beneficial use, a signed statement or declaration prepared by a suitably competent person confirming that the site is, or has been made, safe and stable for the approved development shall be submitted to the Local Planning Authority for approval in writing. This document shall confirm the methods and findings of the intrusive site investigations and the completion of any remedial works and/or mitigation necessary to address the risks posed by past coal mining activity.

Reason. To ensure that adequate information pertaining to ground conditions and coal mining legacy is available to enable appropriate remedial and mitigatory measures to be identified and carried out before building works commence on site. This is in order to ensure the safety and stability of the development, pursuant to Section 15 of the National Planning Policy Framework.

20. Prior to first occupation of the development, a full residential travel plan, which shall include clear objectives and modal split targets, together with a time-bound programme of implementation, monitoring, regular review and update; and be based on the particulars contained within Travel Plan Framework (by Eddisons - Job No. 3690), shall be submitted to and approved in writing by the local planning authority and thereafter operated in accordance with the approved details.

Reason. To promote and provide access to sustainable transport options,

pursuant to Section 9 of the National Planning Policy Framework.

21. (a) Prior to installation, the following shall be submitted to and agreed in writing by the Local Planning Authority, which shall be accompanied by a written statement from a suitably qualified person to demonstrate compliance with the following stated aims and objectives:
- Detailed design of the noise barriers proposed north of plots 50-82. This system of barriers will achieve the acoustic performance set out within the Noise Impact Assessment (Ref. 50-586-R1-6) Figure 5 and Table 4.2.
 - Detailed design of the noise barriers to be included within the gardens of plots 49-51, 82-83 and 109-126. The barriers proposed can be of wooden fence or brick wall construction, but must be sealed at the base, be free from holes and have a minimum mass of 15 kg/m².
- (b) No development comprising the erection of any external walls shall take place, unless and until the details of the design of windows and external doors to sensitive rooms, the roof, and the means of ventilation in relation to the dwellings at plots 50-82, is submitted to and approved in writing by the Local Planning Authority. The detailed design of dwelling facades will follow the recommendations of the Noise Impact Assessment (Ref. 50-586-R1-6), Section 5.2.
- (c) Prior to occupation, the noise mitigation measures (part a and b) shall be completed in accordance with the approved details and retained thereafter for the lifetime of the development.

Prior to occupation of the first dwellinghouse, a verification report prepared by a suitably qualified person, confirming full implementation of the approved noise mitigation measures, shall be submitted to and approved by the Local Planning Authority.

Reason. In order to ensure that the amenities of the future occupants of the proposed properties are not adversely impacted by noise from nearby industrial, storage and distribution uses in accordance with Policy EN7/2 of the Bury Unitary Development Plan and the Section 12 of the National Planning Policy Framework.

22. Prior to occupation of the first dwelling, a biodiversity enhancement scheme shall be submitted to and agreed in writing by the Local Planning Authority. The scheme shall include a statement in relation to habitat improvement and creation in the context of the DEFRA Biodiversity Metric v3.0 (or amended) and national and local planning policies in relation to biodiversity net gain.

The scheme shall be implemented no later than the first planting season after final occupation and maintained thereafter in accordance with the approved scheme.

Reason. In order to provide biodiversity net gain, pursuant to Section 15 of the National Planning Policy Framework.

23. The approved emergency access route shall be available for use on first occupation of any dwellings and it shall be available for use at all times whilst the residential development is reliant upon this access for an emergency purpose.

Reason. To ensure that the emergency access route is delivered to serve the residential development hereby approved, pursuant to UDP Policies EN1/5 - Crime Prevention and H2/2 - The Layout of New Residential Development of the Bury Unitary Development Plan.

24. No development comprising the erection of any external walls shall take place until details of the materials to be used in the construction of the external surfaces of the development, including the roof, and all boundary treatments, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details. The materials to be used throughout the development shall be consistent in terms of colour, size and texture with the approved details.

Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.

25. The drainage for the development hereby approved shall be carried out in accordance with the principles set out in the submitted Foul & Surface Water Drainage Design Drawing 882534/40-02, Rev P6 dated 21/10/2022, which was prepared by RSK. For the avoidance of doubt, no surface water will be permitted to drain directly or indirectly into the public sewer. Prior to occupation of the first dwellinghouse, the drainage scheme shall be completed in accordance with the approved details and retained thereafter for the lifetime of the development.

Reason. To ensure a satisfactory form of development and to prevent an undue increase in surface water run-off and to reduce the risk of flooding, pursuant to Policy EN5/1 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.

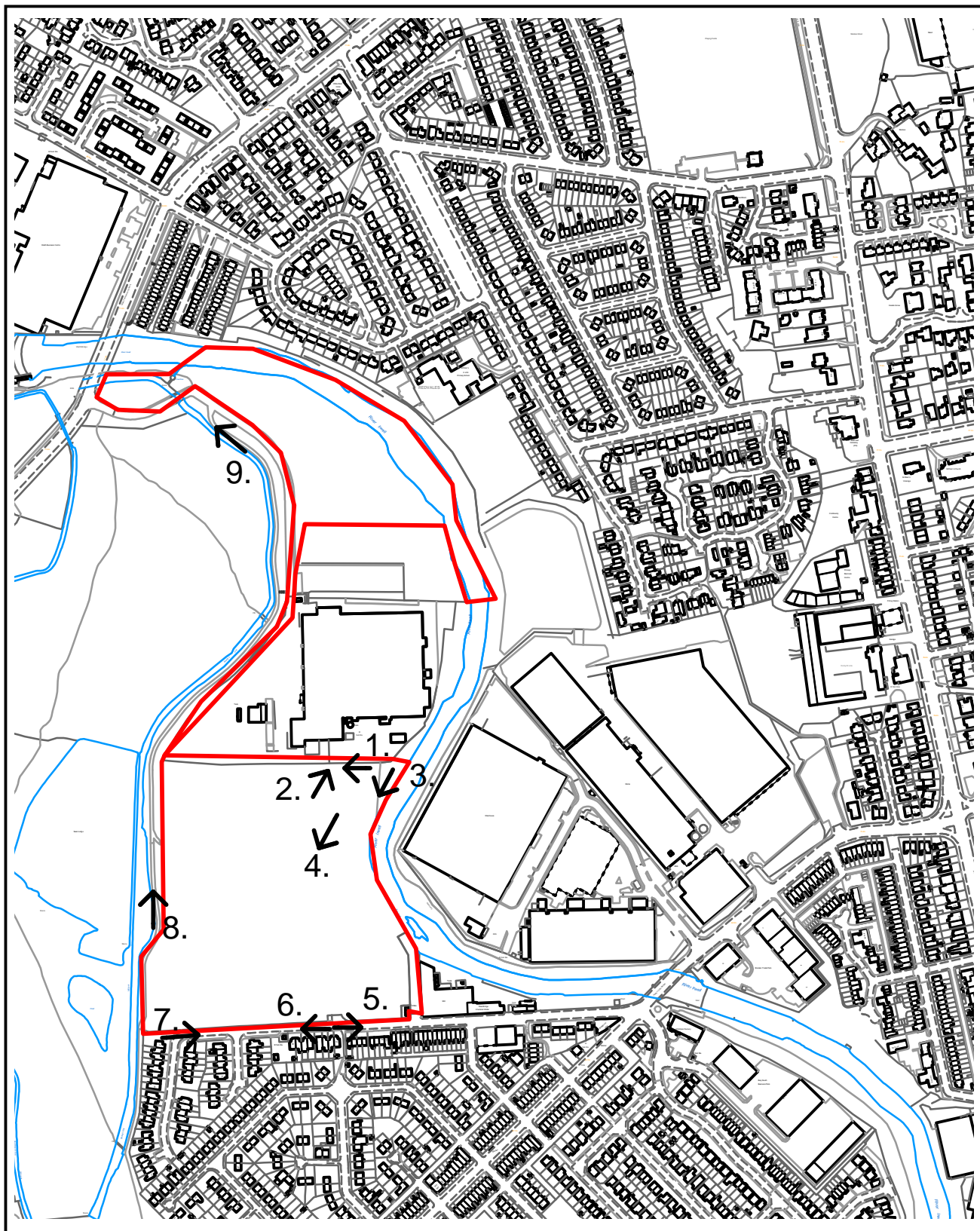
26. Prior to the occupation of the first dwellinghouse hereby approved, a sustainable drainage management and maintenance plan for the lifetime of the development shall be submitted to and agreed in writing with the local planning authority. The sustainable drainage management and maintenance plan shall include as a minimum:

- a. Arrangements for adoption by an appropriate public body or statutory undertaker, or, management and maintenance by a resident's management company; and
- b. Arrangements for inspection and ongoing maintenance of all elements of the sustainable drainage system to secure the operation of the surface water drainage scheme throughout its lifetime.

The development shall subsequently be completed, maintained and managed in accordance with the approved plan.

Reason. To ensure that management arrangements are in place for the sustainable drainage system in order to manage the risk of flooding and pollution during the lifetime of the development, pursuant to Policy EN5/1 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.

For further information on the application please contact **Dean Clapworthy** on **0161 253 5317**



PLANNING APPLICATION LOCATION PLAN

APP. NO 68368

**ADDRESS: Land at York Street/Bury Road
Radcliffe**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

68368 – Item 2 – Aerial and Photos

Aerial 1 – View of full site



Aerial 2 - View of southern parcel from south (residential parcel)



68368 – Item 2 – Aerials and Photos

Aerial 3: Aerial view of northern parcel from south (open space)



Photo 1 - Northern boundary looking west



68368 – Item 2 – Aerial and Photos

Photo 2 - Looking north to distribution centre



Photo 3 - Looking south along east side of development platform



68368 – Item 2 – Aerial and Photos

Photo 4 - Looking south across development platform towards York Street



Photo 5 - Site frontage to York Street looking east



68368 – Item 2 – Aerial and Photos

Photo 6 - Site frontage to York Street looking west



Photo 7 - Dwellings opposing southern boundary to York Street



68368 – Item 2 – Aerial and Photos

Photo 8 - Recently completed emergency vehicle access



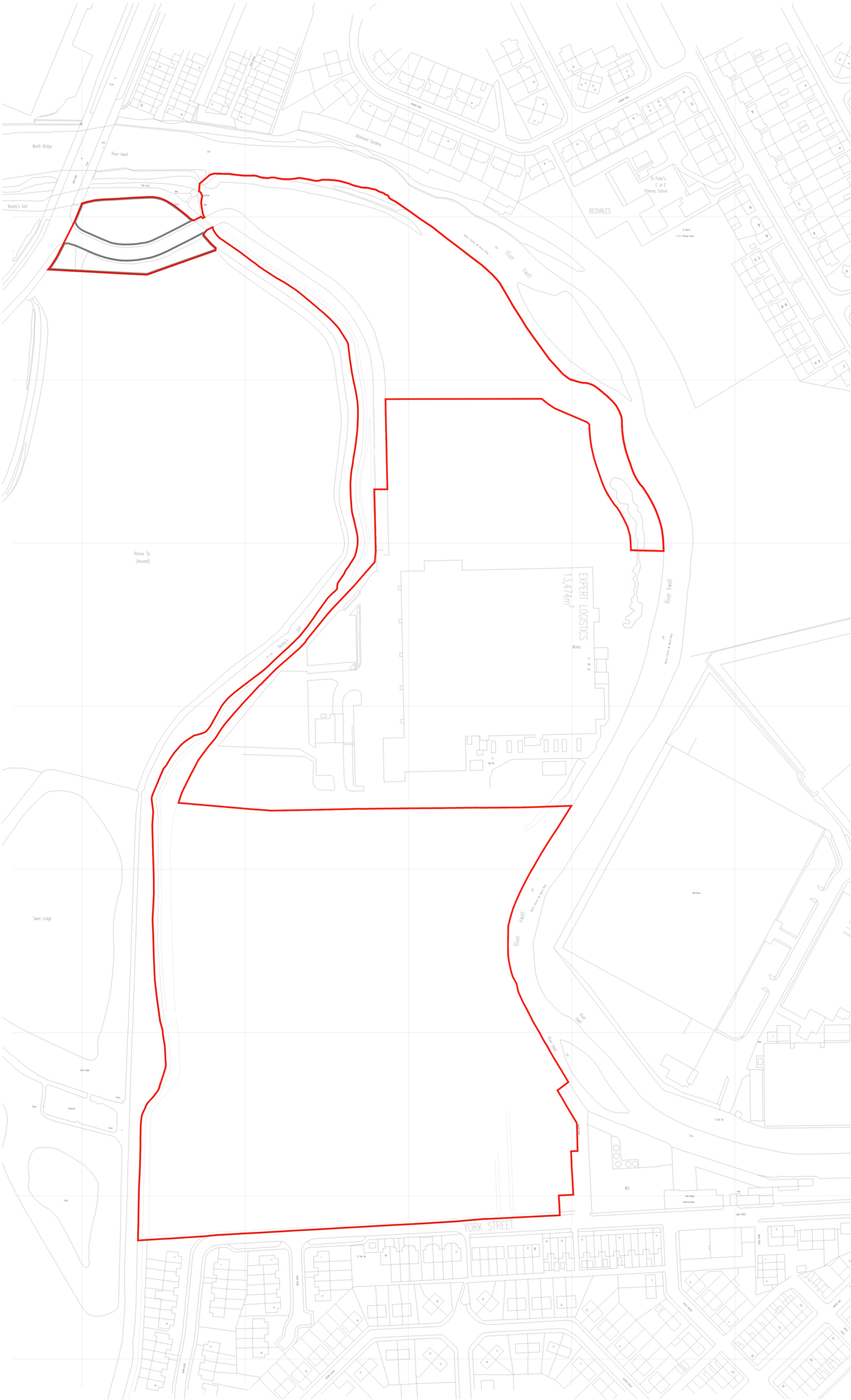
68368 – Item 2 – Aerial and Photos

Photo 9 - Recent widening





0 5 10 15 25 50 m
SCALE BAR 1:1250



Revision	Date	Amendment	Initials
Development YORK STREET			
Location RADCLIFFE			
Marketing Name			
Drawing Title Location Plan			
Drawing Number KH-005- LP			
Revision	Scale @ A0		1:1250
Drawn By	BP	Date Started	15.03.22
Checked by	RG	Date Checked	

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Accommodation Schedule

York Street, Radcliffe
211



KELLEN
HOMES

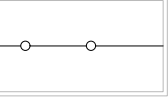
House Type		Bed No.	Type	No.	Sqft	Total Sqft	Split
Hic	Hickory	2	Semi/mews	71	767	54457	33.65%
Garth	Garth	3	Semi	97	908	88076	45.97%
Garth	Garth	3	Detached	1	908	908	0.47%
Garth UP	Garth UP	3	Semi	8	1053	8424	3.79%
Gor	Gorm	3	Semi	16	912	14592	7.58%
Gor	Gorm	3	Detached	8	912	7296	3.79%
Eme	Emerson	4	Semi	10	1064	10640	4.74%
			Total	211		184393	100.00%

Gross Site Area:	21.8	Acres	8.82	Hectares		
POS	8.120		3.29	Hectares		
Undevelopable	1.9	Acres	0.77	Hectares		
Nett Site Area:	11.780	Acres	4.77	Hectares		
Gross Density:	9.68	Units/Acre	23.92	Units/Hectare		
Nett Density:	17.91	Units/Acre	44.26	Units/Hectare		
Gross Footage:	8458.39	SQFT/Acre	1941.76	SQM/Hectare		
Nett Footage:	15653.06	SQFT/Acre	3593.41	SQM/Hectare		

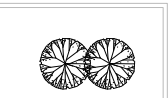
Legend



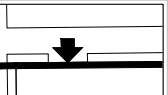
Indicates 1.8m high brick wall with piers



Indicates 1.8m high plot divisional screen fence.



Indicates proposed tree planting subject to a landscape design and specification.



Indicates principle entrance door to dwelling.



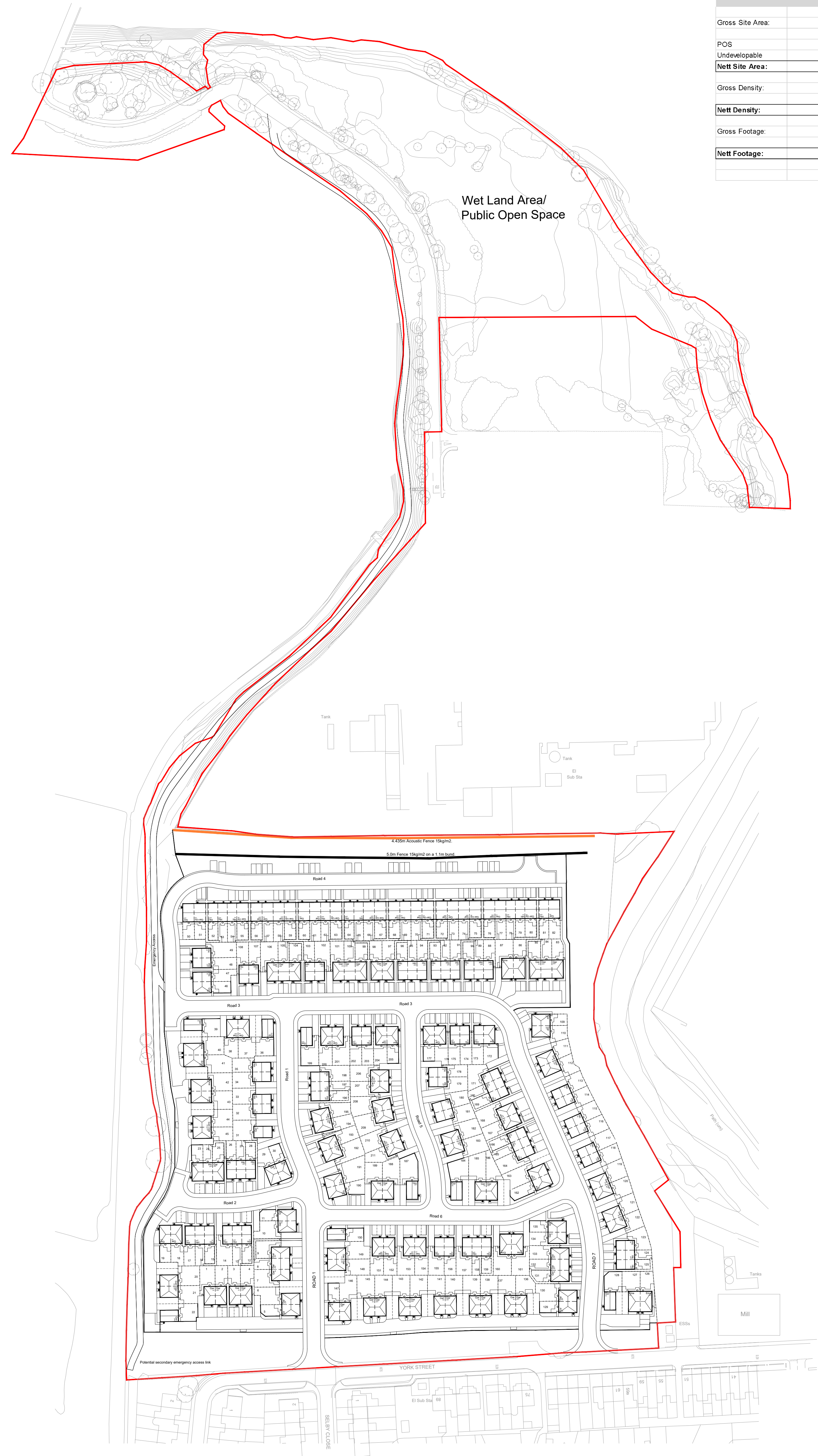
Indicates secondary entrance door to dwelling.



Indicates application site boundary.



Indicates 3no. 600x600mm flags for bins.



O	06.10.22	Flagging amended and bin stores added.	BP
N	13.07.22	Site amended to 100% NDSS compliant. Site renumbered. Scheme redrawn to as per BS4142. North road boundary amended for visibility splay. Plot 82 amended to 3 bed.	BP
M	28.04.22	Scale bar added	BP
L	17.03.22	Road 3 amended to PROW and affordable plots relocated to the West	RESC
K	15.03.22	Amended affordable units	BP
J	10.03.22	Amended to 211 units	BP
I	03.03.22	Amended location of affordable units	BP
H	24.02.22	Amended accommodation schedule. Emergency access added	BP
G	08.02.22	Amended to surface water easement	BP
F	24.01.22	Amended to more 3 beds	BP
E	20.01.22	206 Layout	BP
D	14.12.21	212 Layout	BP
C	11.12.21	Redrawn Layout (PRS)	BP
B	10.12.21	Redrawn Layout	BP
Revision	Date	Amendment	Initials

DEVELOPMENT
YORK STREET

Location
RADCLIFFE

Marking Name
-

Drawing Title
-

PLANNING LAYOUT (COMPOSITE)

Job No.
KH005

Revision
O

Drawn By
BP

Checked by
ATL

Drawing No.
A-100

Scale @ A1
1:1000

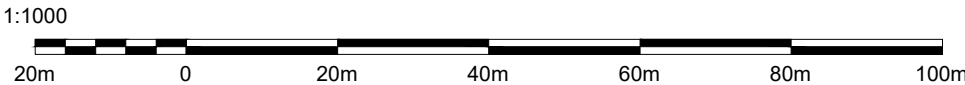
Date
13.12.21

Date Checked
05.10.22

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Accommodation Schedule

York Street, Radcliffe

211							
House Type		Bed No.	Type	No.	Sqft	Total Sqft	Split
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Gross Footage:	8457.66	SQFT/Acre	1941.59	SQM/Hectare		
Nett Footage:	15651.70	SQFT/Acre	3593.10	SQM/Hectare		

Legend

- Indicates 1.8m high feature timber fencing
- Indicates 500mm timber knee rail.
- 2.4m high acoustic fence.
- Indicates 1.8m high plot divisional screen fence.
- Indicates proposed tree planting subject to a landscape design and specification.
- Indicates principle entrance door to dwelling.
- Indicates secondary entrance door to dwelling.
- Indicates application site boundary.
- Indicates 3no. 600x600mm flags for bins.

S	25.10.22	Highways comments added	AB
T	21.10.22	General arrangements following highway comments.	ROC
R	12.10.22	Flagging amended and bin stores added.	BP/ATL
Q	07.09.22	Accommodation schedule updated.	BP
P	26.08.22	Road to north boundary amended to visibility splay and plot 82 amended to 3 bed	BP
O	21.07.22	Redrawn to BS 4142.	BP
N	13.07.22	Previous plots 57-59 switched with plots 183-184 and whole site re-numbered	ROC
M	28.04.22	100% NDCS compliant.	BP
L	17.03.22	Scale bar added	BP
K	15.03.22	Road 3 amended to PROW and affordable plots relocated to the west.	ROC
J	10.03.22	Amended affordable units	BP
I	03.03.22	Amended to 211 units	BP
H	24.02.22	Amended location of affordables	BP
G	08.02.22	Amended accommodation schedule.	BP
F	24.01.22	Emergency access added	BP
E	20.01.22	Amended to surface water easement	BP
D	14.12.21	Amended to more 3 beds	BP
C	13.12.21	205 layout	BP
B	10.12.21	212 layout	BP
Revision	Date	Redrawn Layout (PRS)	BP
		Redrawn Layout	BP
		Amendment	Initials

Development	
YORK STREET	
Location	
RADCLIFFE	
Marketing Name	
-	
Drawing Title	
PLANNING LAYOUT	
Job No.	Drawing No.
KH005	PL2
Revision	Scale @ A1
T	1:500
Drawn By	Date
BP	13.12.21
Checked by	Date Checked
RGC	-

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Street Scene 1-1



Street Scene 1-1



Street Scene 2-2



Street Scene 3-3

Page 60

Revision	Date	Amendment	Initials
Development YORK STREET			
Location RADCLIFFE			
Marketing Name			
Drawing Title Street Scenes			
Drawing Number KH-YSR-SS-01			
Revision	Scale @ A1 1:200		
Drawn By BP	Date Started 25.03.22		
Checked by RGC	Date Checked		



Street Scene 1-1



Street Scene 1-1

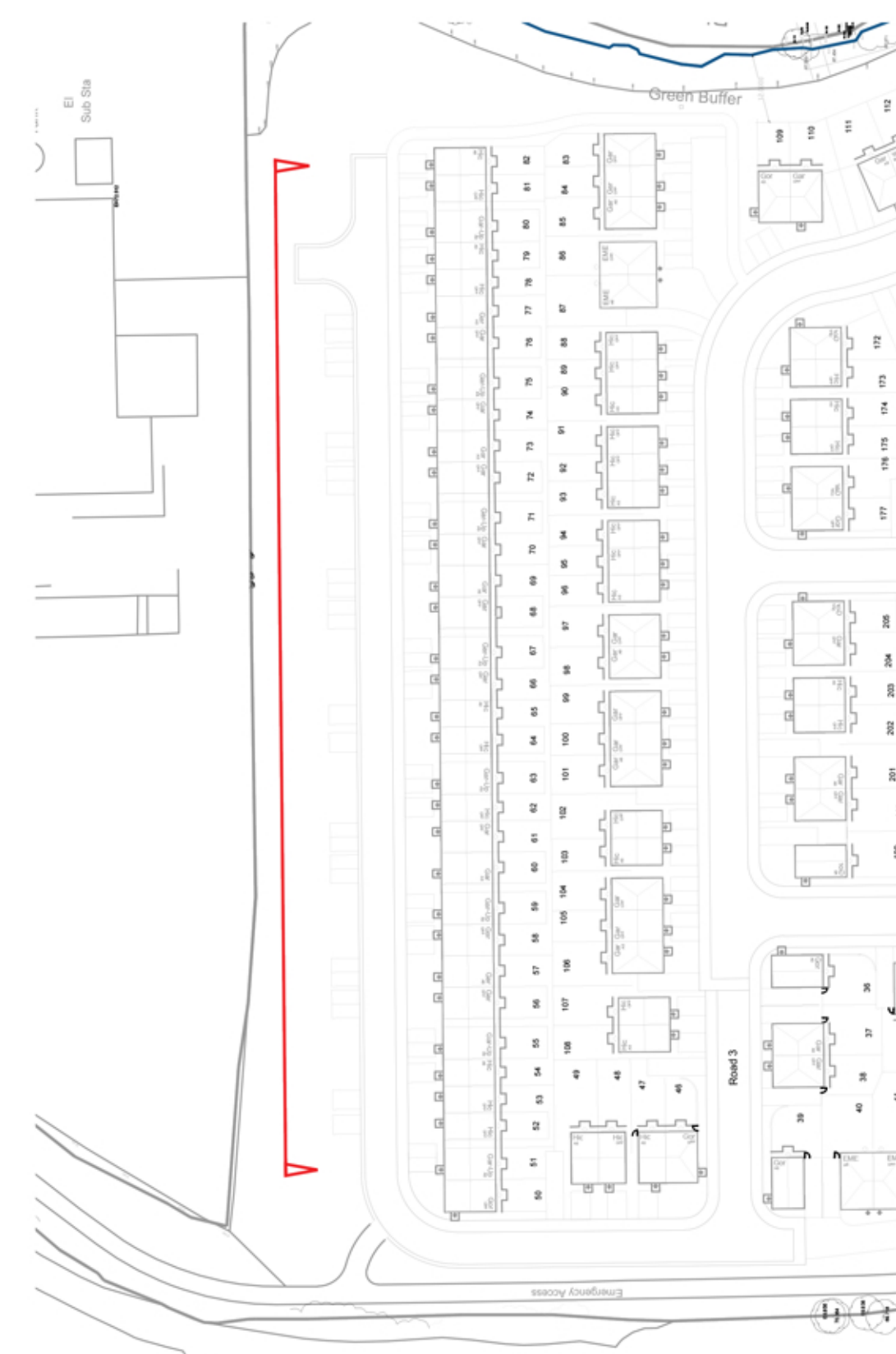


Street Scene 2-2



Street Scene 3-3

Revision	Date	Amendment	Initials
Development YORK STREET			
Location RADCLIFFE			
Marketing Name			
Drawing Title Street Scenes			
Drawing Number KH-YSR-SS-01			
Revision	Scale @ A1	1:200	
Drawn By BP	Date Started	25.03.22	
Checked by RGC	Date Checked		



Revision	Date	Amendment	Initials

Development	
YORK STREET	
Location	
BURY	
Marketing Name	
-	
Drawing Title	
PLOTS 50 TO 82 STREET SCENE	
Job No.	Drawing No.
KH005	A-600
Revision	Scale @ A1
-	1:100
Drawn By	Date
ATL	05.09.22
Checked by	Date Checked
-	-

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K

H

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Ward: Radcliffe - West

Item 3

Applicant: Mr Adewunmi Adesanya

Location: Stand United Reformed Church, Stand Lane, Radcliffe, Manchester, M26 1JE

Proposal: Proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access

Application Ref: 68548/Full

Target Date: 05/08/2022

Recommendation: Approve with Conditions

Description

The application relates to Stand United Reformed Church, a Grade II Listed Building built in 1885. The church is constructed from coursed sandstone with ashlar details and slate roof and original stained glass windows and in an architectural style of Victorian Gothic Revival.

The Church Building is sited on a rectilinear 'island site' bounded by Chapel Field Road (to the north), Stand Lane (to the east), Walker Street (to the south), and a small parking area (to the west) towards the end of Walker Street. The properties within the immediate vicinity are mainly terraced and semi-detached residential properties.

The Church has been vacant since 2019 when it stopped operating and was subsequently sold to the applicant, the Celestial Church of Christ. The applicants' intention is to bring the church back into use as place of worship. To achieve this it is proposed to carry out various alterations and improvements to the church, some of which are required to accord with the church's doctrine of worship.

This application seeks full planning permission.

External

Externally it is proposed to reinstate the railing that would have originally been sited on top of the low stone wall that forms the boundary to the church and replace the existing pedestrian gates to match the new railing design. The railings would be 710mm high, comprising rectangular 50mm x 50mm posts to be fixed to the original centres and of a design to reflect the original railings. Three external metal gates of the same design to the railings are also proposed. These would be the overall same height as the railings around the site circa 1.4m overall.

Originally it was proposed to erect a high fence between the rear of the church and the cemetery as the churches doctrine requires 'contact' separation between the living and those passed away. The fence would have screened views of the headstones from users of the church and created a physical barrier in-between. This proposal has been withdrawn from the application. Also withdrawn from the original submission was the proposed cleaning of external stone and maintenance of the stained glass windows. It is understood this work is to be carried out when full assessments of the condition of the stone and glass can be carried out. As such an updated heritage statement has been submitted to reflect the amendment to the application submission.

There are no other external works proposed as part of this application.

Internal

The proposed internal alterations are as follows:

- The covering of the horizontal gravestones in the basement area with a removable timber framed reinforced walk on glass structure. The area would be opened up to become a usable space.
- Retention and cleaning of parquet flooring and Victorian tiles on the ground floor (Church).
- Retention and restoring (re-varnish) of wood panelling on ground floor.
- Removal of 20th century partitions and doors, painting and other alterations to existing 20th century features in the basement.
- New and replacement internal railings in the main church area (ground floor) with new rail around stair well to the basement and glass balustrade.
- Re-painting white the existing render to the internal ground floor elevations.

For the avoidance of doubt.

The proposed internal alterations are not development within the meaning of Section 55(2) of The Town and Country Planning Act 1990 (as amended) and as such do not require planning permission. The element to be assessed that does require planning permission is the reinstatement of the perimeter railing along the top of the perimeter low stone wall and the new matching gates.

An application for listed building consent, 68548, has also been submitted which assesses the impact of the proposals on the architectural and historic interest of the church.

Use of the building

The church is in a lawfully established use as a place of worship - Learning and Non-Residential Institutions Class F1(f), formerly Use Class D1 of the Use Classes Order 1987.

Up to 2019 the United Reformed Church hosted various local community groups in the basement area of the church where there were toilets and a kitchen. This provision was ancillary to the primary established use of the church as a place of worship and the church would continue to be used as a place of worship by its new owner who is the applicant of this application.

It is understood that the new occupiers of the church would not continue this provision of hosting community groups. For the avoidance of doubt the occupant's intentions does not therefore constitute a loss of an established community facility in planning use terms as the primary use of the building/church as a place of worship or change of use class.

Relevant Planning History

02317/E - Change of use to restaurant and wedding banquet venue (Class A3) - Enquiry completed 05/07/2019.

02524/E - Renovation of the interior of the building and including plastering, painting and replacement of old benches with stackable chairs. External cleaning of carbon affected stone work restoring them to former glory. Enquiry completed 09/03/2021.

67226 - Internal works to church building; Erection of fencing around the perimeter of the church; Erection of fencing at rear to create a secure and maintained space for the cemetery with gate for community access - Withdrawn by Applicant 07/10/2021

67227 - Listed building consent for proposed internal works to church building; Erection of fencing around the perimeter of the church; Erection of fencing at rear to create a secure and maintained space for the cemetery with gate for community access - Withdrawn by Applicant 07/10/2021.

68549 - Listed building consent for proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access - concurrent application.

Publicity

Site Notice posted on 15/07/2022

Press Notice published in the Bury Times on 21/07/2022.

A letter was sent to 36 addresses at Chapel Field, Stand Lane and Walker Street dated 12/07/2022.

4 objections and 2 letters of support have been received and are summarised below.

Objections

- The problem with works to the floors would be where the pews were removed without listed building consent.
- Concerned that this application is a cover to push work through that previous applications have not in the past.
- Covering or removing memorials including the war graves in the Church yard is disrespectful to the people who gave service and even their lives for their community and their country. A stained glass windows is dedicated to the first Freeman of Radcliffe. How does it portray Bury Council if these are removed and/or spoilt. I would urge a visit to this Church before any decisions are made.
- Concerns over antisocial sermon hours . This congregation in other areas of Manchester have early morning 4am sermons and late night sermons that are very loud especially when congregation leaves.
- Concerns regarding parking as its such a large community.
- Concerns about works being done currently despite not having planning permission yet and a listed building.
- Not collecting their bin off Chapelfield which has been full for months now and left on grass despite informing them its their bin.
- As a community we are concerned about sermon times, noise and parking, there is no comment on how this will be managed in the application. Please can this be part of the conditions.
- The community have worked with both Greater Manchester Police and Six Town Housing to reduce crime in the area.
- These plans will negatively impact my safety, health and wellbeing. Many do not have internet access so are unable to participate in the planning process and respectfully request the planning department extend the deadline for public comment to ensure that I am not disadvantaged by my disabilities whilst engaging with the planning application process.
- The Heritage Statement submitted with these applications is so outdated as to be misleading.
- If there is damp etc this is due to the actions of the new owners who have also ripped out internal structures before consent is granted.
- Failed to maintain the fabric of the building and its surrounding churchyard.
- New owners made it clear the local community would not only be unable to reinstate the previous community use of the church but intend to deprive residents use of their own communal gardens.
- The owners state that they intend to reinstate metal railings around the church to prevent vandalism and fly tipping, yet there has not been any fly tipping in the churchyard and the building has never been vandalised. Its open aspect without railings that has ensured the "crime free" status of the church and its graveyard.
- The new owners have demonstrated a complete disregard for the church and grounds having never undertaken even the most basic maintenance, allowing the graveyard to become an overgrown mess.
- At no time since taking ownership of this church, have their congregants / representatives been in contact with local community groups to discuss what the local community need in terms of utilising the church building, nor have they attended any of the meetings convened by local councillors.
- The Planning Statement incorrectly describes the local area and makes no reference to the vast green space areas surrounding this church.

- There is no parking area for general public use anywhere in the neighbourhood. The planning application refers to the proposed changes providing space for 95 attendees, yet makes no reference to the Church being "within walking distance" of those attendees.
- Become apparent that our diverse community would not be welcome.

Support

- Celestial Church of Christ is a good and reputable organisation. The church will support the local communities for food bank, provide clothes and shelter for people in needs. The build will be open for the local community use twice in a week.
- The church will encourage coffee morning for elderly people within local area. They will also organise youth activities which includes learning musical instrument which include piano, drums guitar and organise summer holiday classes for young people who are interested with local community.
- The church will not cause any nuisance or do any overnight sermon or preaching but rather support people providing community facilities.
- The major worship day is on Sunday between 10:00am to 3pm, Wednesday 6pm to 8pm, Friday 6pm to 8pm and Saturdays 12noon for cleaning building premises.

Since the original application, revised plans and information have been received to confirm the proposed external works would now comprise perimeter boundary railings and entrance gates.

A revised Heritage Statement has been received.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection.

Pre-start Conditions - None.

Unitary Development Plan and Policies

EN1	Built Environment
EN1/1	Visual Amenity
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:

The elimination of discrimination, harassment and victimisation;

The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;

The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

It is important that the religious/cultural needs of the any community are recognised and catered for. It is also important that the residential amenity of existing residents is also taken into consideration and balanced.

Public Sector Equality Duty

This report sets out the Council's duties with regards to the Equality Act 2010 and the associated Public Sector Equality Duty.

Religion or belief is a protected characteristic under the 2010 Act and the main report sets out how, in considering this application, regard has been had to how any negative implication in relation to equality is based on an objective justification. The recommendation to approve the application with more restricted opening times than those applied for could be seen as negatively impacting on religion and belief. However, it is considered that this is objectively justified and proportionate because of the harm that would be caused to the residential amenity in terms of noise and disturbance as set out in the main report.

Policies

EN1/1 - Visual Amenity - Development will not be permitted where proposals would have a detrimental effect on:

- public views of prominent or important buildings, especially those in areas of architectural or historic interest;
- the visual amenity both within, or viewed from, areas of environmental interest such as the Green Belt, Special Landscape Areas or the river valleys.

Paragraph 130(b) of the National Planning Policy (NPPF) states - Planning policies and decisions should ensure that development are visually attractive as a result of good architecture, layout and appropriate and effective landscaping.

Assessment

To clarify the parameters of this planning application, proposals relate to the external works only and for the installation of perimeter railings and gates.

All internal works to the church summarised above together with the installation of external railings and gates would be subject to the concurrent Listed Building application reference 68549.

Visual Amenity

The proposed railing and gates would have a black cast iron appearance, traditional and historic in design approach and would be a type frequently used to enclose the grounds of buildings of worship. The proposed railings would reinstate the boundary treatment which previously enclosed the church grounds.

The railings and gates would also aide in the maintenance and security of the church and its grounds.

It is therefore considered that the proposed development would not represent incongruous features or cause detriment to the visual amenity of the street scene or surrounding built environment, and would be appropriate within the context of the historical setting.

As such the proposed development would comply with adopted UDP policies EN1/1 - Visual Amenity and paragraph 130(b) of the NPPF.

It is recommended a condition is added to ensure the colour of the railing and gates be maintained black.

Response to objections raised -

- The proposal would not constitute a loss or change of use of a community facility in planning terms and matters raised are not considerations for this planning application.
- Proposed internal works would be the subject of the Listed Building Consent application

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

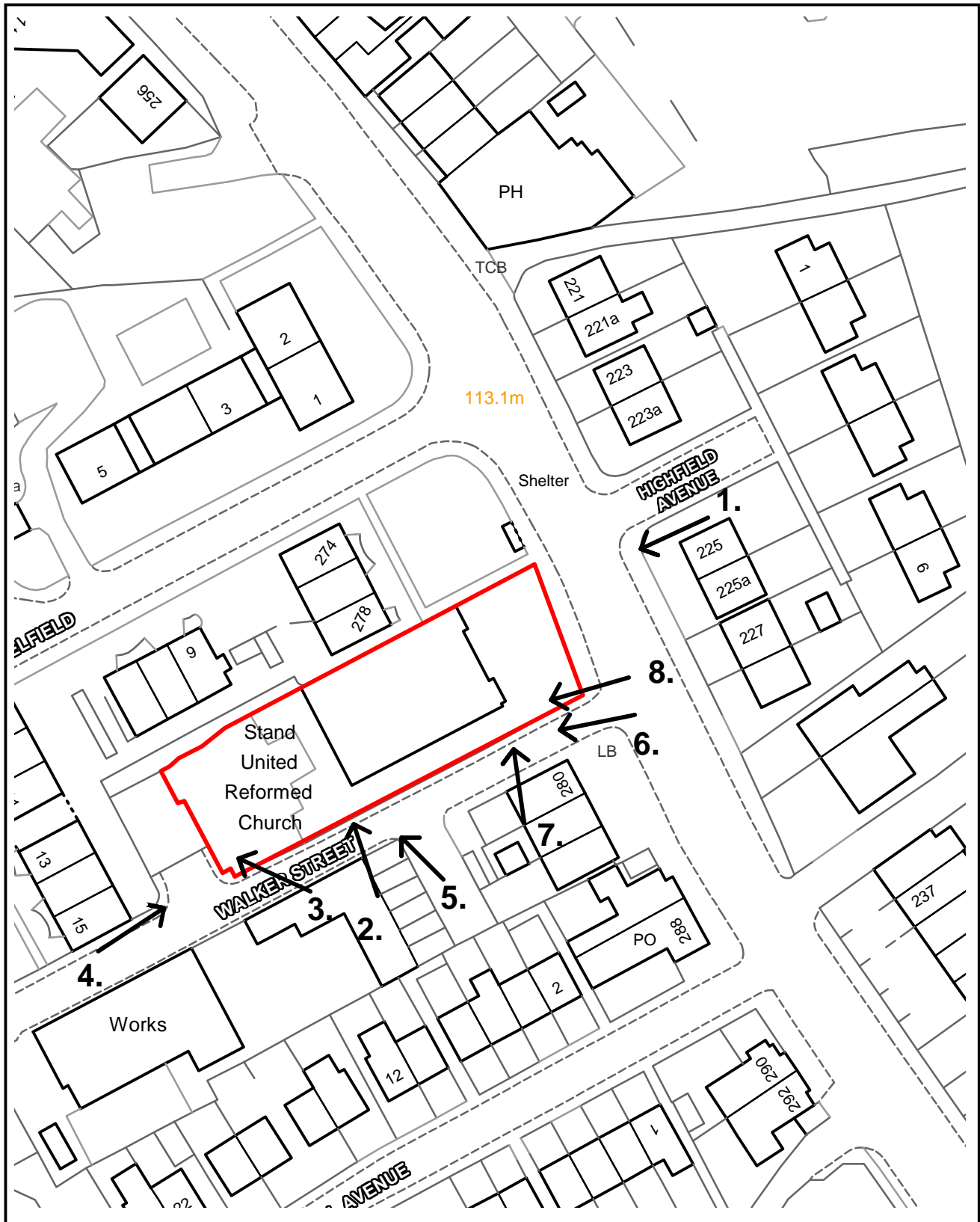
Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered Proposed Site Block Plan - D.01 B, Proposed External Gate G1 - G.04 C, Proposed External Gate G2 - G.07 C, Proposed External Railing - G.08 B, Proposed External Gate G4 - G.10 C, Existing GA Front & Rear Elevations – A.06 A, Existing Walker Street Elevation – A.07 A, Existing GA Flank Elevation 02 – A.08 A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The external perimeter railing and gates detailed on drawings G.07 B, G.08 A and G.10 B and hereby approved shall be painted black within 3 months of the date of this decision and maintained as such thereafter.
Reason: To preserve the special architectural and Historic interest of the Grade II listed Stand United Reformed Church.

For further information on the application please contact **Mark Kilby** on **0161 253 7639**

Viewpoints - Item 03 (68548) and Item 04 (68549)



PLANNING APPLICATION LOCATION PLAN

APP. NOS **68548 and 68549**

ADDRESS: Stand United Reformed Church,
Stand Lane, Radcliffe,

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

68548 (Item 3) and 68549 (Item 4) – External Photos

Photo 1



Photo 2



68548 (Item 3) and 68549 (Item 4) – External Photos

Photo 3



Photo 4



68548 (Item 3) and 68549 (Item 4) – External Photos

Photo 5



Photo 6



68548 (Item 3) and 68549 (Item 4) – External Photos

Photo 7



Photo 8





Page 74

PROPOSED SITE BLOCK PLAN
1 : 200

- PROPOSED RAILING
- 82 LINEAR METERS
- PROPOSED GATES
- 3 NO SINGLE SWING (G1, G2, G3)
- 1 NO DOUBLE SWING (G4)
- SITE BOUNDARY

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date 09 JUN 22 Scale 1 : 200

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PROPOSED SITE
BLOCK PLAN

Project no.	Drawing no.	Rev.
305	D.01	B



RAILING EXAMPLE

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 10**

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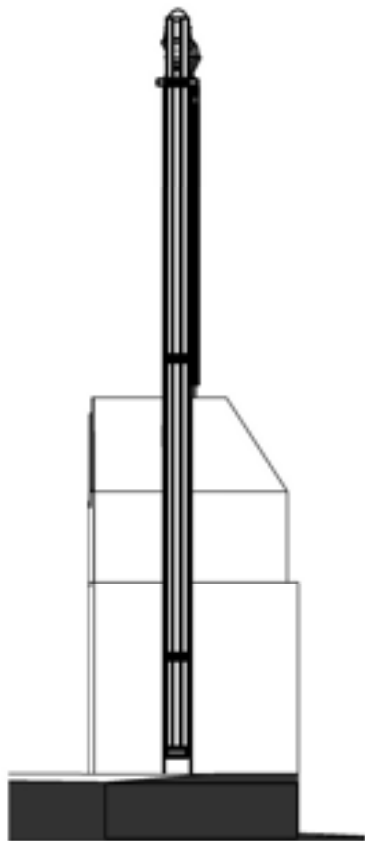
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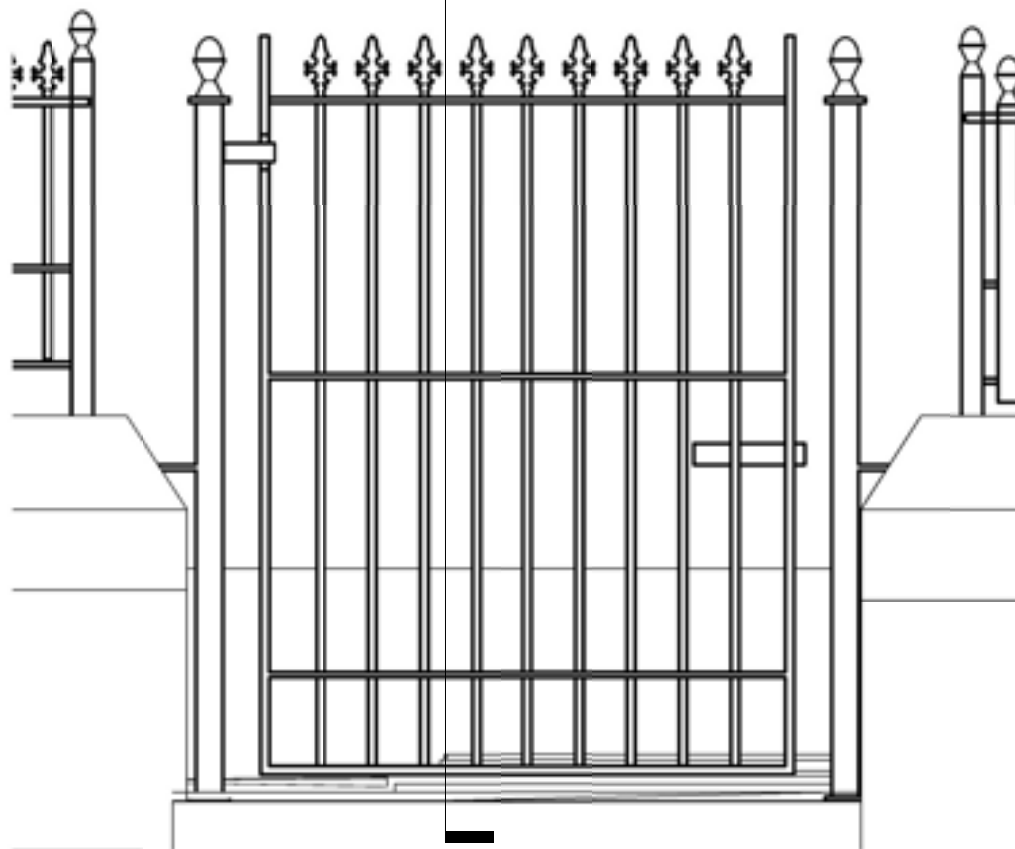
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**PROPOSED
EXTERNAL GATE
G1**

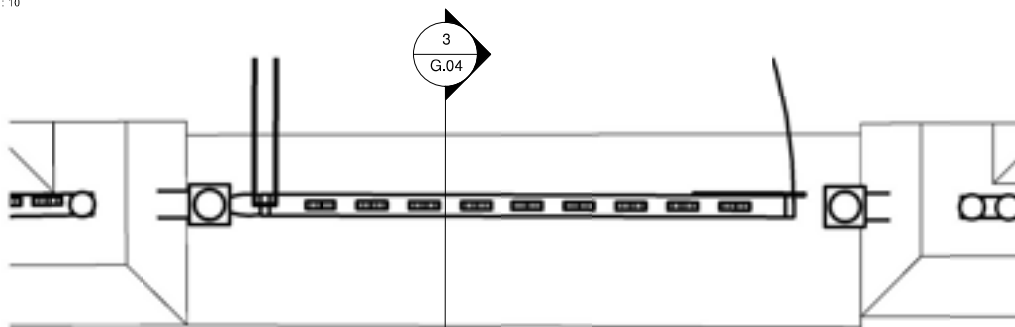
Project no.	Drawing no.	Rev.
305	G.04	B



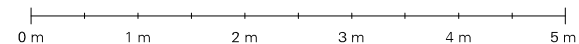
3 PROPOSED SECTION - GATE G1
1 : 10



2 PROPOSED ELEVATION - GATE G1
1 : 10



1 PROPOSED FLOOR PLAN - GATE G1
1 : 10



NEW PROPOSED METAL GATE



RAILING EXAMPLE

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	IO
B	9,11,22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
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Project address
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MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 10**

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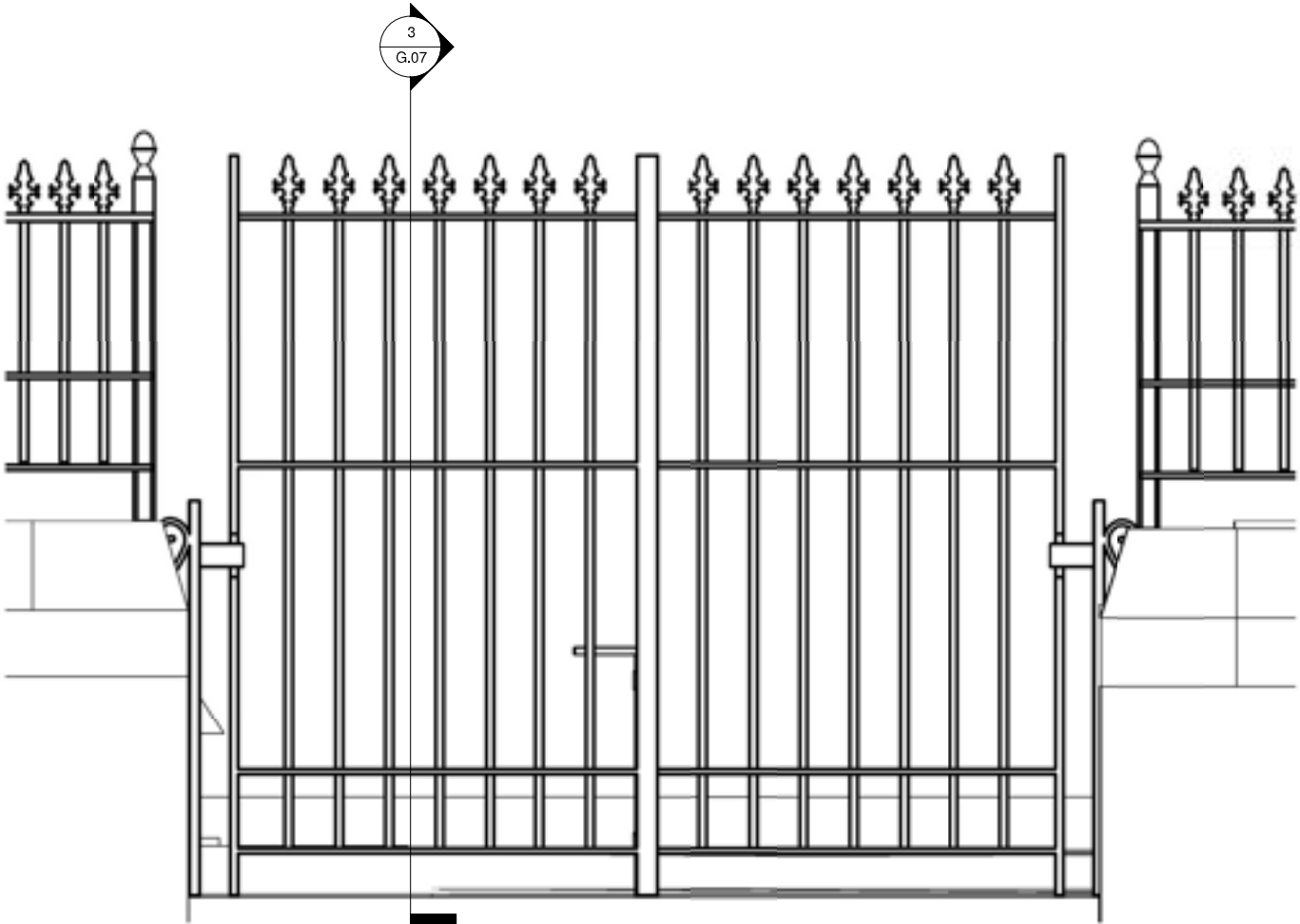
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PROPOSED EXTERNAL GATE G2

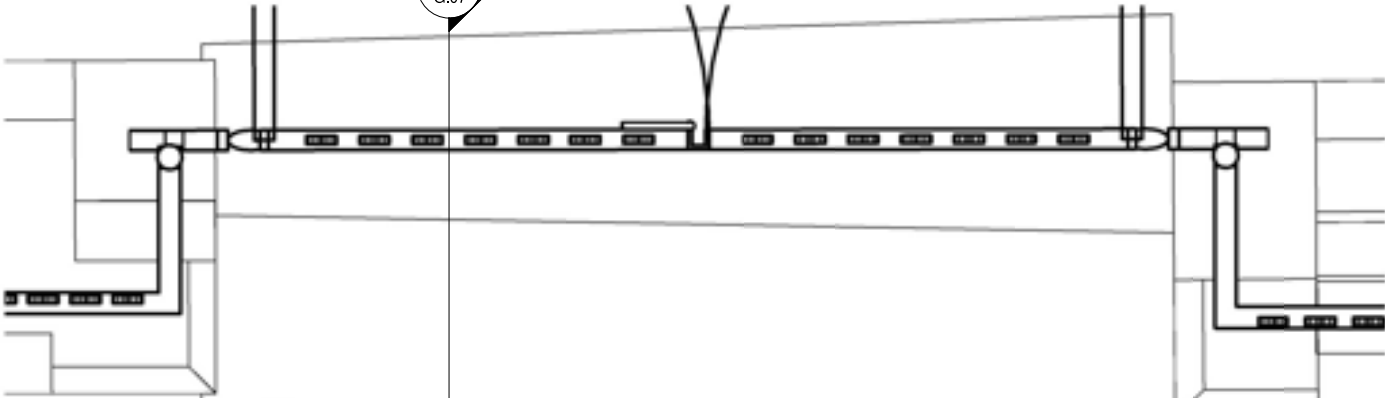
Project no.	Drawing no.	Rev.
305	G.07	B



3 PROPOSED SECTION - GATE G2
1 : 10



2 PROPOSED ELEVATION - GATE G2
1 : 10



1 PROPOSED FLOOR PLAN - GATE G2
1 : 10

NEW PROPOSED GATE



RAILING EXAMPLE

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

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**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

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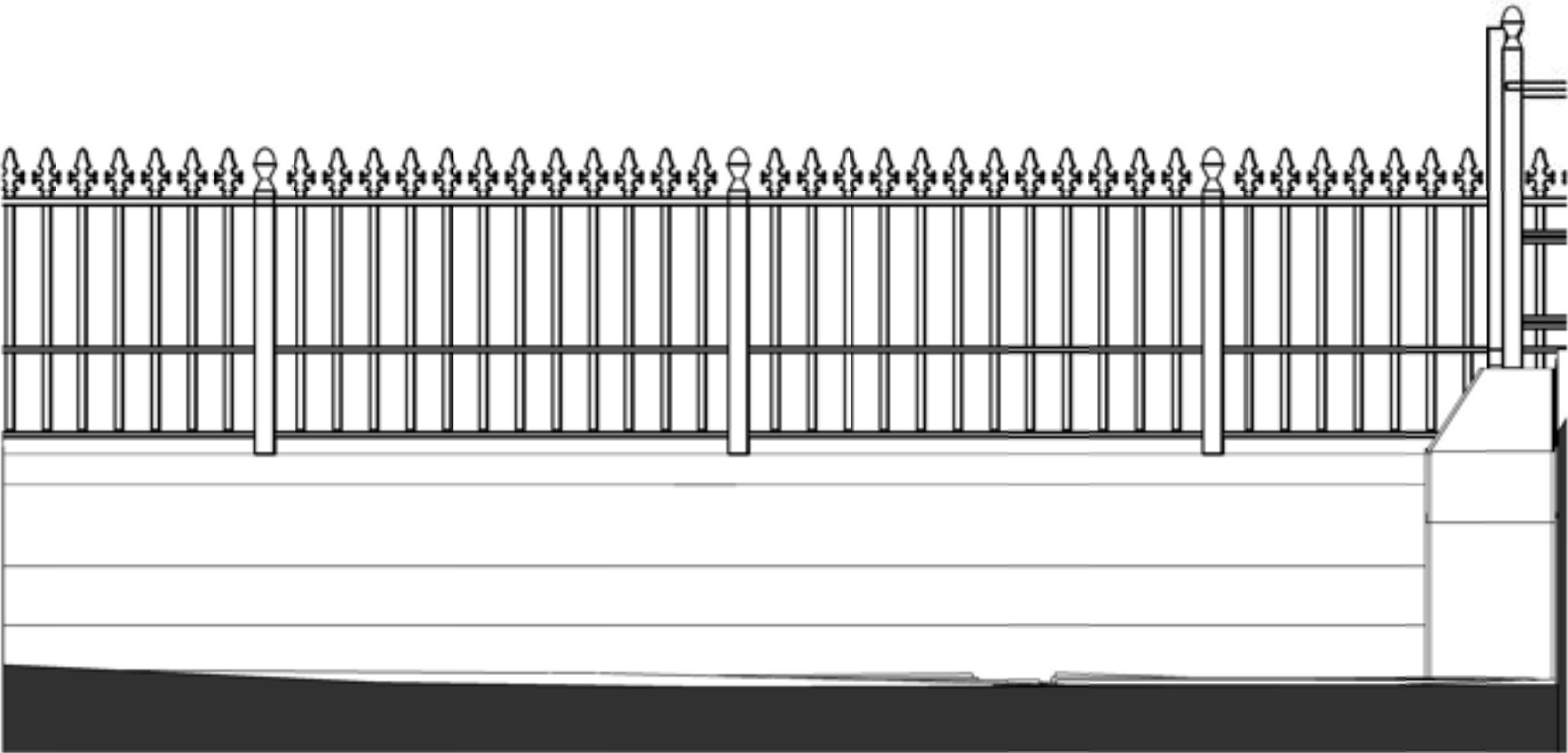
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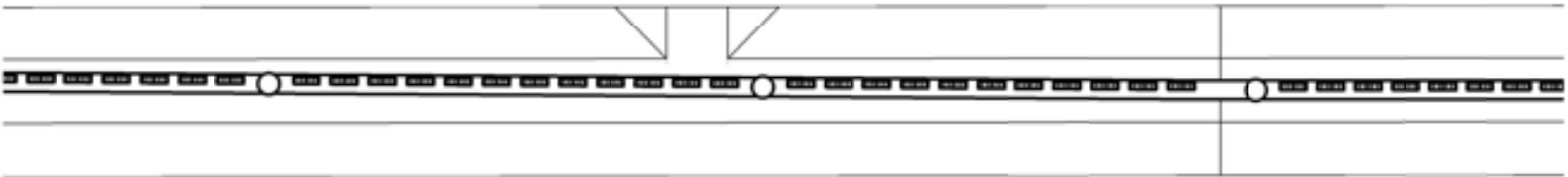
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**PROPOSED
EXTERNAL
RAILING**

Project no.	Drawing no.	Rev.
305	G.08	A

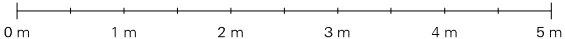


2 ELEVATION
1 : 10



1 FLOOR PLAN

NEW PROPOSED EXTERNAL RAILINGS





RAILING EXAMPLE

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client

MR A ADESANYA

Project address

SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date 09 JUN 22 Scale 1 : 20

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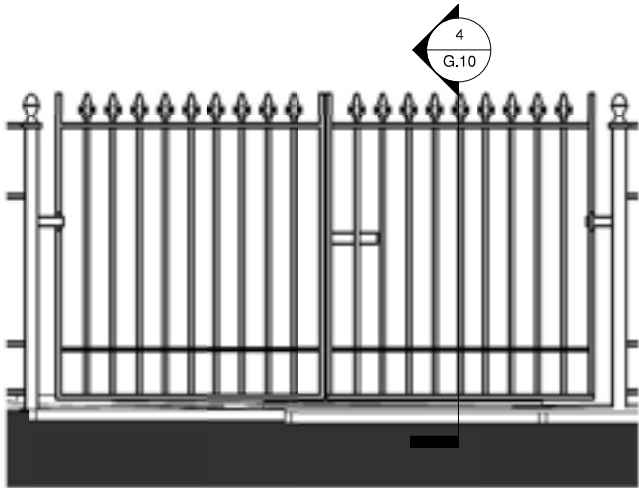
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PROPOSED
EXTERNAL GATE
G4 (ELEVATIONS)

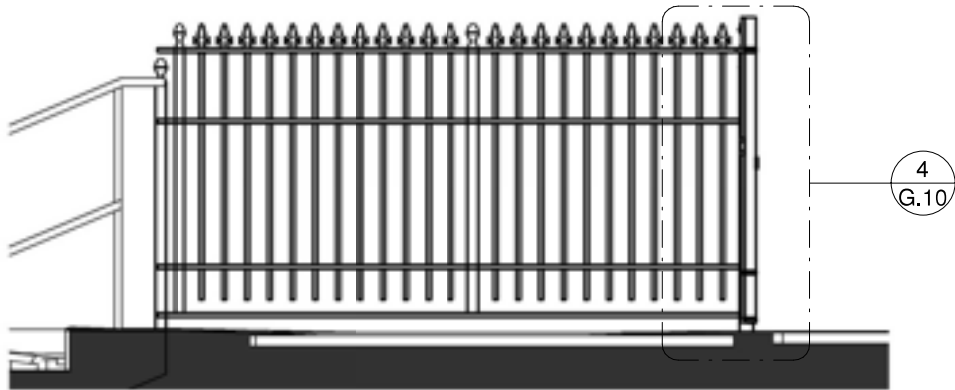
Project no.	Drawing no.	Rev.
305	G.10	B



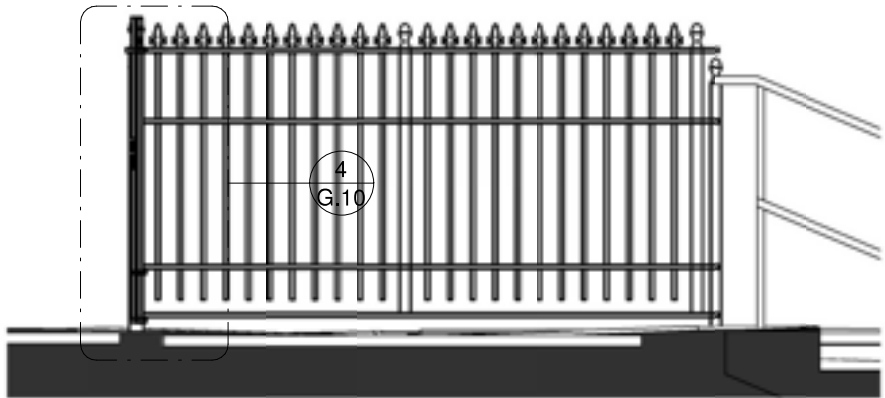
4 Section 3
1 : 20



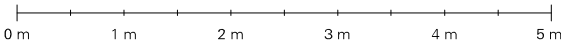
1 Elevation 24 - a
1 : 20



3 Elevation 24 - d
1 : 20
NEW PROPOSED METAL GATE

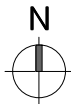


2 Elevation 24 - b
1 : 20



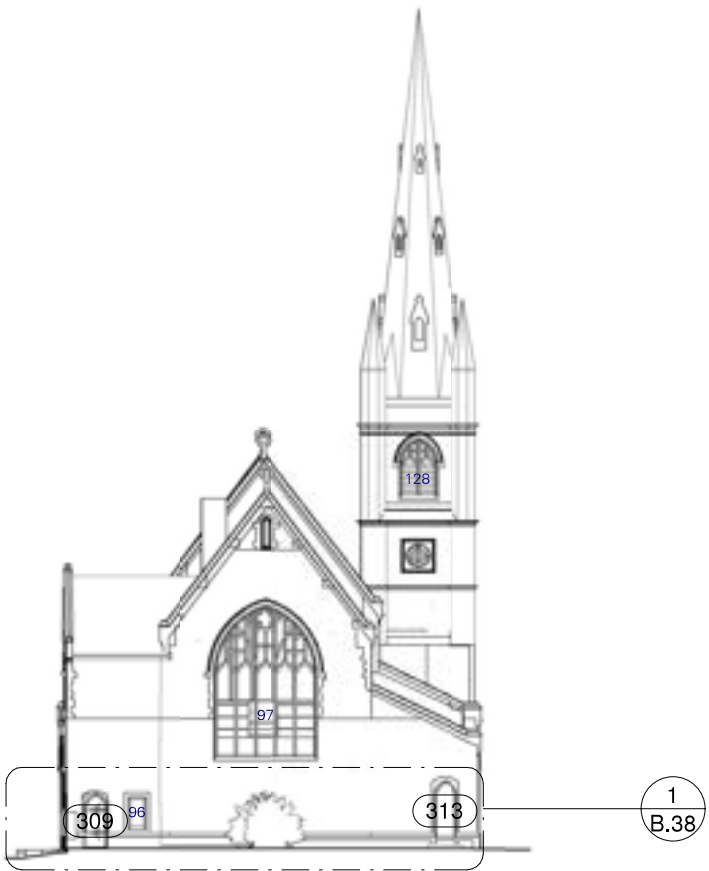


LOCATION MAP

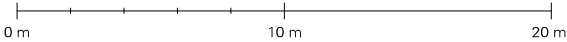




FRONT ELEVATION
(FACING STAND LANE)



REAR ELEVATION
(FACING CHAPELFIELD ROAD)



IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 200**

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**EXISTING GA
FRONT & REAR
ELEVATIONS**

Project no.	Drawing no.	Rev.
305	A.06	A



- A. RUSTICATED DWARF SANDSTONE WALLS WITH STONE COPING STONES.
- B. EXISTING RED BRICK WALL

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE, MANCHESTER, M26 1JE

Date **09 JUN 22** Scale **1 : 200**

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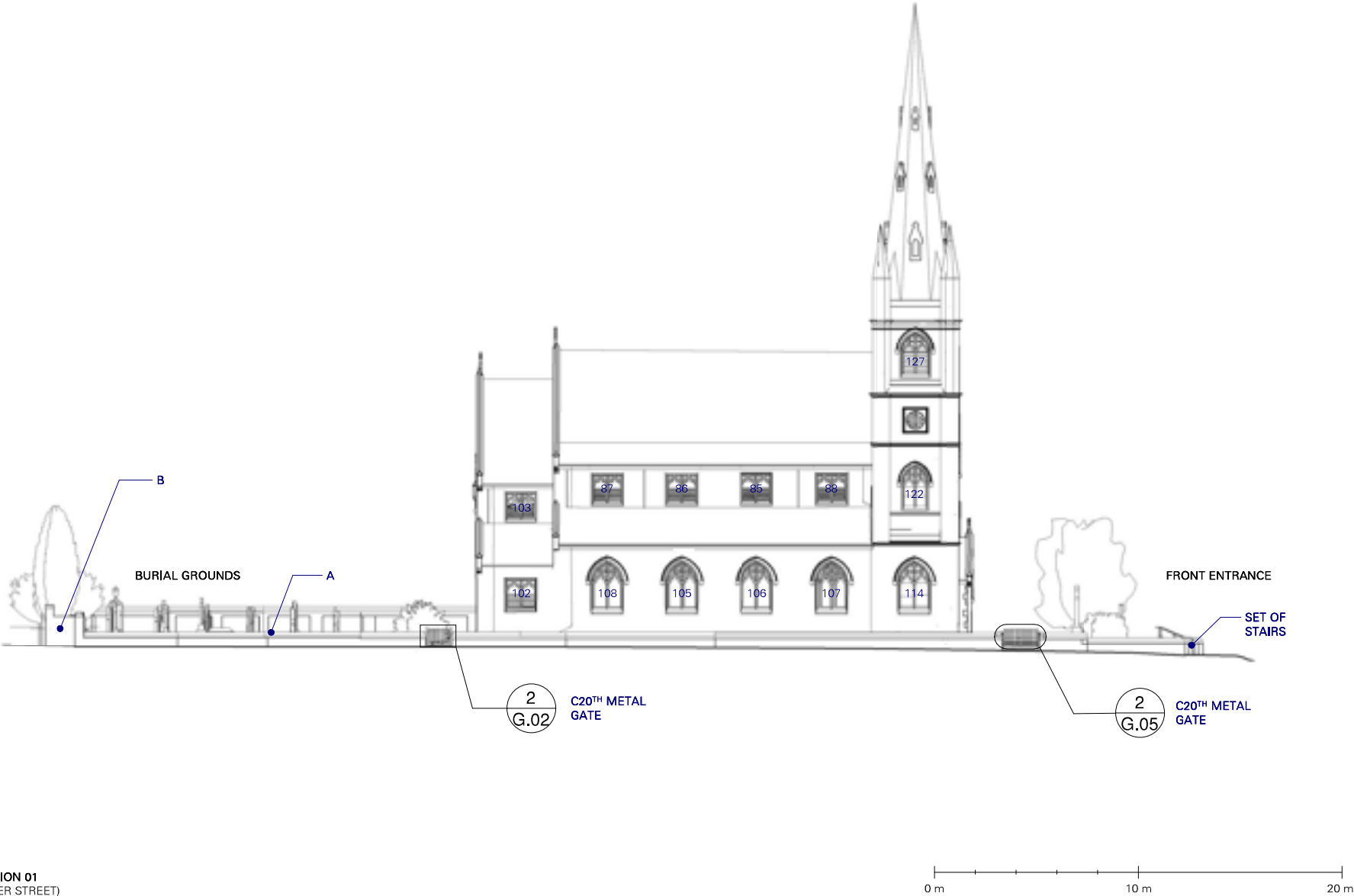
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EXISTING
WALKER STREET
ELEVATION

Project no.	Drawing no.	Rev.
305	A.07	A



FLANK ELEVATION 01
(FACING WALKER STREET)



FLANK ELEVATION 02
(FACING CHAPELFIELD ROAD)

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 200**

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EXISTING GA
FLANK
ELEVATION 02

Project no.	Drawing no.	Rev.
305	A.08	A

Ward: Radcliffe - West

Item 4

Applicant: Mr Adesanya

Location: Stand United Reformed Church, Stand Lane, Radcliffe, Manchester, M26 1JE

Proposal: Listed building consent for proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access

Application Ref: 68549/Listed Building Consent **Target Date:** 05/08/2022

Recommendation: Approve with Conditions

Description

The application relates to Stand United Reformed Church, a Grade II Listed Building built in 1885. The church is constructed from coursed sandstone with ashlar details and slate roof and original stained glass windows and in an architectural style of Victorian Gothic Revival.

The Church Building is sited on a rectilinear 'island site' bounded by Chapel Field Road (to the north), Stand Lane (to the east), Walker Street (to the south), and a small parking area (to the west) towards the end of Walker Street. The properties within the immediate vicinity are mainly terraced and semi-detached residential properties.

The Church has been vacant since 2019 when it stopped operating and was subsequently sold to the applicant, the Celestial Church of Christ. The applicants' intention is to bring the church back into use as place of worship. To achieve this it is proposed to carry out various alterations and improvements to the church, some of which are required to accord with the church's doctrine of worship.

This application seeks Listed Building Consent

External

Externally it is proposed to reinstate the railing that would have originally been sited on top of the low stone wall that forms the boundary to the church and replace the existing pedestrian gates to match the new railing design.

Originally it was proposed to erect a high fence between the rear of the church and the cemetery as the churches doctrine requires 'contact' separation between the living and those passed away. The fence would have screened views of the headstones from users of the church and created a physical barrier in-between. This proposal has been withdrawn from the application. Also withdrawn from the original submission was the proposed cleaning of external stone and maintenance of the stained glass windows. It is understood this work is to be carried out when full assessments of the condition of the stone and glass can be carried out. As such an updated heritage statement has been submitted to reflect the amendment to the application submission.

There are no other external works proposed as part of this application.

Internal

The proposed internal alterations are as follows:

- The covering of the horizontal gravestones in the basement area with a removable timber framed reinforced walk on glass structure. The area would be opened up to become a usable space.
- Retention and cleaning of parquet flooring and Victorian tiles on the ground floor

(Church).

- Retention and restoring (re-varnish) of wood panelling on ground floor.
- Removal of 20th century partitions and doors, painting and other alterations to existing 20th century features in the basement.
- New and replacement internal railings in the main church area (ground floor) with new rail around stair well to the basement and glass balustrade.
- Re-painting white the existing render to the internal ground floor elevations.

An application for full planning permission, 68549, for the external perimeter railings has also been submitted.

Use of the building

The church is in a lawfully established use as a place of worship - Learning and Non-Residential Institutions Class F1(f), formerly Use Class D1 of the Use Classes Order 1987.

Up to 2019 the United Reformed Church hosted various local community groups in the basement area of the church where there were toilets and a kitchen. This provision was ancillary to the primary established use of the church as a place of worship and the church would continue to be used as a place of worship by its new owner who is the applicant of this application.

It is understood that the new occupiers of the church would not continue this provision of hosting community groups. For the avoidance of doubt this does not therefore constitute a loss of an established community facility in planning use terms as the primary use of the church as a place of worship remains.

Relevant Planning History

02317/E - Change of use to restaurant and wedding banquet venue (Class A3) - Enquiry completed 05/07/2019.

02524/E - Renovation of the interior of the building and including plastering, painting and replacement of old benches with stackable chairs. External cleaning of carbon affected stone work restoring them to former glory. Enquiry completed 09/03/2021.

67226 - Internal works to church building; Erection of fencing around the perimeter of the church; Erection of fencing at rear to create a secure and maintained space for the cemetery with gate for community access - Withdrawn by Applicant 07/10/2021

67227 - Listed building consent for proposed internal works to church building; Erection of fencing around the perimeter of the church; Erection of fencing at rear to create a secure and maintained space for the cemetery with gate for community access - Withdrawn by Applicant 07/10/2021.

68548 - Proposed internal works to church building; Erection of fencing and railings around the perimeter of the church with gates for access - Concurrent application

21/0250 - Unauthorised works within a listed building. - 06/08/2021.

Publicity

Site Notice posted on 15/07/2022

Press Notice published in the Bury Times on 21/07/2022.

A letter was sent to 36 addresses at Chapel Field, Stand Lane and Walker Street dated 12/07/2022.

2 objections and 6 letters of support and have been received and are summarised below.

- The community have worked with both Greater Manchester Police and Six Town Housing to reduce crime in the area.
- These plans will negatively impact my safety, health and wellbeing. Many do not have

internet access so are unable to participate in the planning process and respectfully request the planning department extend the deadline for public comment to ensure that I am not disadvantaged by my disabilities whilst engaging with the planning application process.

- The Heritage Statement submitted with these applications is so outdated as to be misleading.
- If there is damp etc this is due to the actions of the new owners who have also ripped out internal structures before consent is granted.
- Failed to maintain the fabric of the building and its surrounding churchyard.
- New owners made it clear the local community would not only be unable to reinstate the previous community use of the church but intend to deprive residents use of their own communal gardens.
- The owners state that they intend to reinstate metal railings around the church to prevent vandalism and fly tipping, yet there has not been any fly tipping in the churchyard and the building has never been vandalised. Its open aspect without railings that has ensured the "crime free" status of the church and its graveyard.
- The new owners have demonstrated a complete disregard for the church and grounds having never undertaken even the most basic maintenance, allowing the graveyard to become an overgrown mess.
- At no time since taking ownership of this church, have their congregants / representatives been in contact with local community groups to discuss what the local community need in terms of utilising the church building, nor have they attended any of the meetings convened by local councillors.
- The Planning Statement incorrectly describes the local area and makes no reference to the vast green space areas surrounding this church.
- There is no parking area for general public use anywhere in the neighbourhood. The planning application refers to the proposed changes providing space for 95 attendees, yet makes no reference to the Church being "within walking distance" of those attendees.
- Become apparent that our diverse community would not be welcome.

Support

- Celestial Church of Christ is a good and reputable organisation. The church will support the local communities for food bank, provide clothes and shelter for people in needs. The build will be open for the local community use twice in a week.
- The church will encourage coffee morning for elderly people within local area. They will also organise youth activities which includes learning musical instrument which include piano, drums guitar and organise summer holiday classes for young people who are interested with local community.
- The church will not cause any nuisance or do any overnight sermon or preaching but rather support people providing community facilities.
- The major worship day is on Sunday between 10:00am to 3pm, Wednesday 6pm to 8pm, Friday 6pm to 8pm and Saturdays 12noon for cleaning building premises.
- I believe the Church is a charity organisation which do charitable work, which I believe will help and support someone or some people in that community. They are peace loving people.
- Provide a safe environment to express their life stories and given help to tackle their problems.

Since the original application, revised plans and information have been received to confirm the proposed external works would now comprise perimeter boundary railings and entrance gates.

A revised Heritage Statement has been received.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Conservation Officer - No Objection.

Traffic Section - No comment received.

Pre-start Conditions - None.

Unitary Development Plan and Policies

EN2/3 Listed Buildings

NPPF National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:

The elimination of discrimination, harassment and victimisation;

The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;

The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

It is important that the religious/cultural needs of the any community are recognised and catered for. It is also important that the residential amenity of existing residents is also taken into consideration and balanced.

Public Sector Equality Duty

This report sets out the Council's duties with regards to the Equality Act 2010 and the associated Public Sector Equality Duty.

Religion or belief is a protected characteristic under the 2010 Act and the main report sets out how, in considering this application, regard has been had to how any negative implication in relation to equality is based on an objective justification. The recommendation to approve the application with more restricted opening times than those applied for could be seen as negatively impacting on religion and belief, however, it is considered that this is objectively justified and proportionate because of the harm that would be caused to the residential amenity in terms of noise and disturbance as set out in the main report.

Policies

Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires Local Planning Authorities in the exercise of their planning functions to have regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

Paragraph 199 of the National Planning Policy Framework (NPPF) states when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 200 of the NPPF states any harm to, or loss of, the significance of a designated

heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification. Substantial harm to or loss of:

- a) grade II listed buildings, or grade II registered parks or gardens, should be exceptional;
- b) assets of the highest significance, notably scheduled monuments, protected wreck sites, registered battlefields, grade I and II* listed buildings, grade I and II* registered parks and gardens, and World Heritage Sites, should be wholly exceptional.

Paragraph 202 of the NPPF states where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

EN2/3 - Listed Buildings - The Council will actively safeguard the character and setting of Listed Buildings by not permitting works, alterations or changes of use which would have a detrimental effect on their historical or architectural character and features. Proposals for demolition will be opposed and will only be considered where it is demonstrated conclusively that the building(s) cannot be retained.

In considering applications for Listed Building Consent, the Council will have regard to the following criteria:

- the impact of the proposal on the historic fabric of the building;
- the relationship of any extension to the Listed Building in terms of its height, size, design, and roofscape;
- the need to protect the setting of the Listed Building;
- the impact of associated ancillary facilities and infrastructure works.

Assessment

Buildings of Special Architectural or Historic interest are "listed" by the Secretary of State for the Environment. The listing of a building provides statutory protection against its alteration or demolition. The Borough's valuable heritage is reflected in the large number of Listed Buildings in the area (316 in June 1996) and the Council is mindful of the desirability to preserve them and enhance their setting and appearance.

New uses for Listed Buildings may often be the key to their preservation especially for buildings which have become wholly or partly redundant. However, new uses must not adversely affect the architectural or historic features and character or appearance of such buildings.

The setting of a Listed Building is often an essential feature of its character; unsympathetic development in close proximity to a Listed Building can mar its appearance, make its future use unattractive or untenable, or physically damage its structure. It is therefore important to consider the effect that a proposed development may have on such buildings.

Proposals

The proposals are considered to be at the lower end of the spectrum of less than substantial harm:

Internal works

- The basement works would involve the removal of 20th/21st century partitions and replacement of 20th/21st century kitchen and toilets which are not part of the historic fabric of the church and as such do not form part of its architectural or historic interest. This would open up the original plan form of the basement and as such would better reveal its significance.
- The covering of the horizontal gravestones in the basement area with a removable

timber framed reinforced walk on glass structure is proposed as part of the churches doctrine and for health and safety reasons. This would represent a non-permanent reversible structure that would still allow visibility of the stones. This area is to be opened up by the removal of non-historic partition wall to become a usable space and the current uneven surface created by the gravestones which represents a trip hazard would be removed by the proposal.

- The retention and cleaning of parquet flooring and Victorian tiles on the ground floor (Church), retention and restoring (re-varnish) of wood panelling on ground floor and removal of 20th century partitions and doors, painting and other alterations to existing 20th century features are considered to be routine maintenance that would be acceptable as representing less than substantial harm. New and replacement internal railings in the main church area (ground floor) with a new rail around the stair well to the basement and glass balustrade are also considered reversible and would not harm significance by adversely impacting on the legibility of the internal space as a church.

External works

- The re-instatement of the perimeter railings would restore the original boundary treatment to the church and whilst it is not possible at this time to know exactly what the original design of the railings would have been, with an understanding of the period and there still being original Victorian ecclesiastical railings in existence, the proposed size and design of the railings and gates are considered acceptable and would enhance its architectural interest. It is recommended a condition is added to ensure the colour of the railing and gates be maintained black to preserve the special architectural and Historic interest of the Grade II listed Stand United Reformed Church.

In representing less than substantial harm, the proposals would maintain the legibility of the building as a church therefore conserving its architectural and historic interest. The proposals would have the public benefit of bringing back a vacant listed church into a sustainable and viable use, and as such the proposals would outweigh the harm identified.

The application would therefore comply with paragraphs 194, 195, 199, 200 and 203 of the NPPF and adopted UDP policy EN2/3 - Listed Buildings.

Matters relating to unauthorised works

Legal Framework and Planning Policy

Prior to the submission of this application, the church pews at ground floor had been removed. The balcony and pews at the east end of the church have been retained. The pulpit was also removed.

Section 7(1) of the Planning (Listed Building and Conservation Areas) Act 1990 states - Subject to the following provisions of this Act, no person shall execute or cause to be executed any works for the demolition of a listed building or for its alteration or extension in any manner which would affect its character as a building of special architectural or historic interest, unless the works are authorised.

Section 9(1) states - If a person contravenes Section 7 they shall be guilty of an offence.

In consideration of the particular merits of these actions, it is clear that consent should have been sought in the first place prior to anything being undertaken or removed. However, it is not considered expedient to consider a prosecution in this instance, notwithstanding any evidence of who allegedly committed the offence. The pews and pulpit did possess a lower level of historic and architectural interest. The pews were not considered to be unique in their design or to pre-date the church nor to be of exceptional aesthetic quality. The same consideration can be applied to the pulpit. Listed churches that become disused can deteriorate very quickly to a point where the building is lost completely, or its architectural and historic significance have been severely reduced. In this instance the level of harm caused by the removal of pews and pulpit would be outweighed by the public benefit of

bringing a vacant listed church back into sustainable viable use.

Response to objections raised -

- The submitted updated heritage statement has been referred to in the report. The other matters raised are not considerations for Listed Building Consent.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 18 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2. This decision relates to drawings numbered Location Plan - 305 3, Proposed Site Block Plan - D.01 B, Proposed External Gate G1 - G.04 C, Proposed External Gate G2 - G.07 C, Proposed External Railing - G.08 B, Proposed External Gate G4 - G.10 C, Existing Site Block Plan – A.01 A, Existing GA Basement Floor Plan – A.02 A, Existing GA Ground Floor Plan – A.03 A, Existing GA Front & Rear Elevations – A.06 A, Existing Walker Street Elevation – A.07 A, Existing GA Flank Elevation 02 – A.08 A, Existing Crypt Floor Plan – H.01 A, Existing Internal Elevations BT.13 – H.02 A, Proposed Crypt Floor Plan – H.03 A, New Raised Floor in Crypt – H.05, Proposed KS Ground Floor Plan – E.02 A, Proposed Basement Floor – E.01 A, Proposed Internal Elevation GD22 – E.24A A and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. The external perimeter railing and gates detailed on drawings G.07 B, G.08 A and G.10 B and hereby approved shall be painted black within 3 months of the date of this decision and maintained as such thereafter.

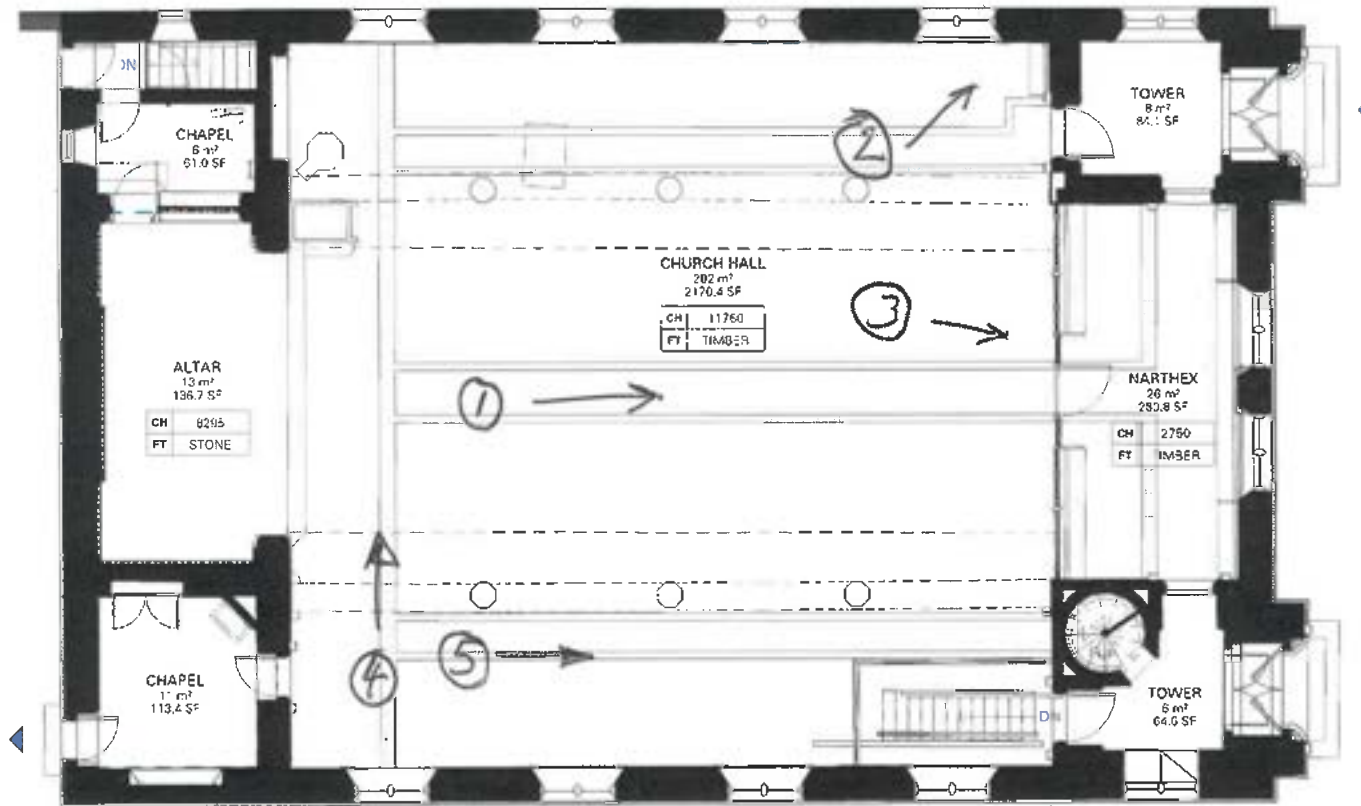
Reason: To preserve the special architectural and Historic interest of the Grade II listed Stand United Reformed Church pursuant to the principles of the NPPF and UDP Policy EN2/3.

For further information on the application please contact **Mark Kilby** on **0161 253 7639**



Ground Floor INTERNAL VIEWPOINTS 68549 - LISTED BUILDING CONSENT

Item 04:



KETEKTURE
RIBA CHARTERED ARCHITECTS

Revision Date Description Author

STAGED
FEASIBILITY

Client
MR A ADESANYA

Project
SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date 09 JUN 22 1:100

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EXISTING GA
GROUND FLOOR
PLAN

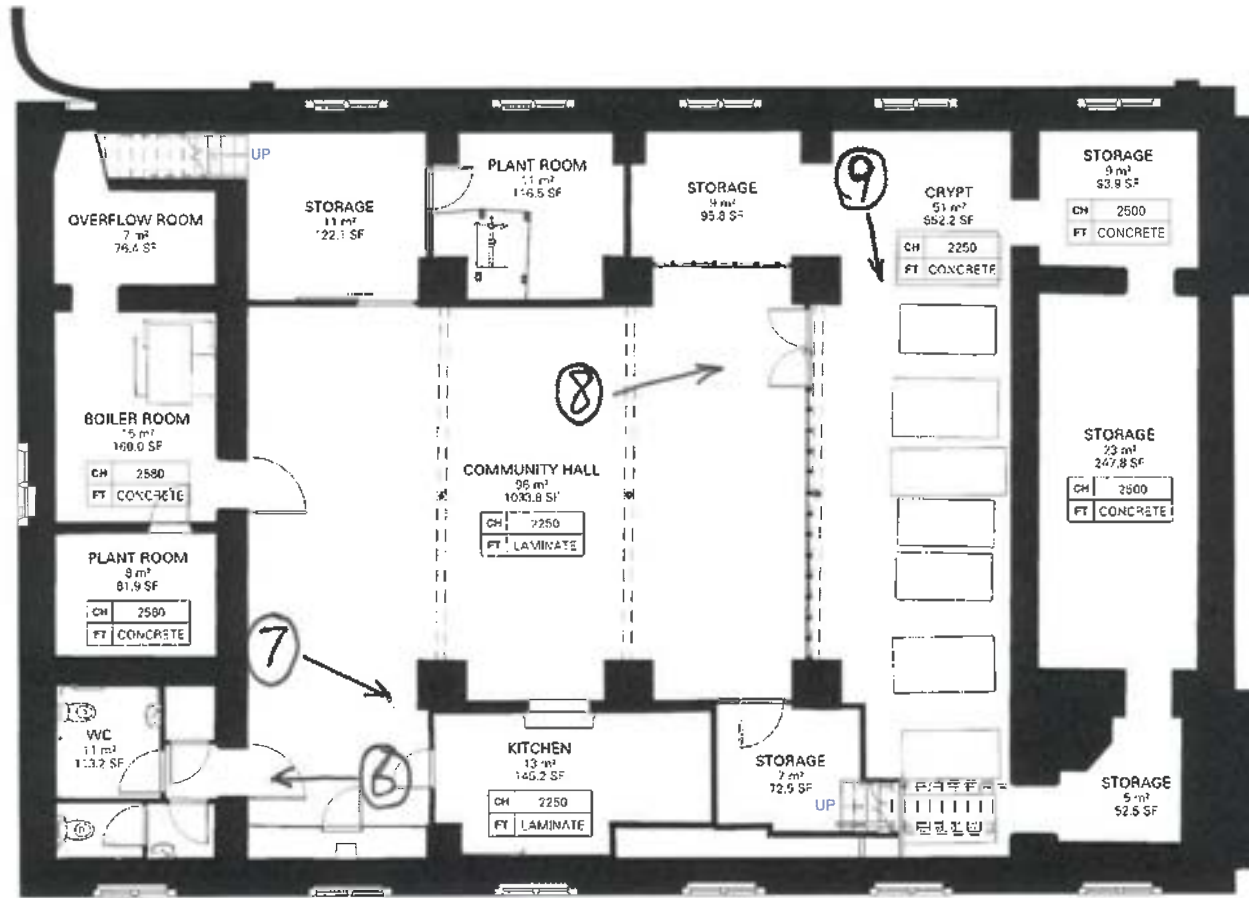
Page no	Revision	Date
305	A.03	

EXISTING GROUND FLOOR PLAN

0 m 5 m 10 m



Item 04: BASEMENT VIEWPOINTS 68549 - LISTED BUILDING CONSENT ONLY



KETEKTURE
RIBA CHARTERED ARCHITECTS

Revision Date Description Author

PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date: 09 JUN 22 Scale: 1:100

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EXISTING GA BASEMENT FLOOR PLAN

Project no. Drawing no. Rev.
305 A.02

68549 – Item 4 – Internal photos

Photo 1



Photo 2



68549 – Item 4 – Internal photos

Photo 3



Photo 4

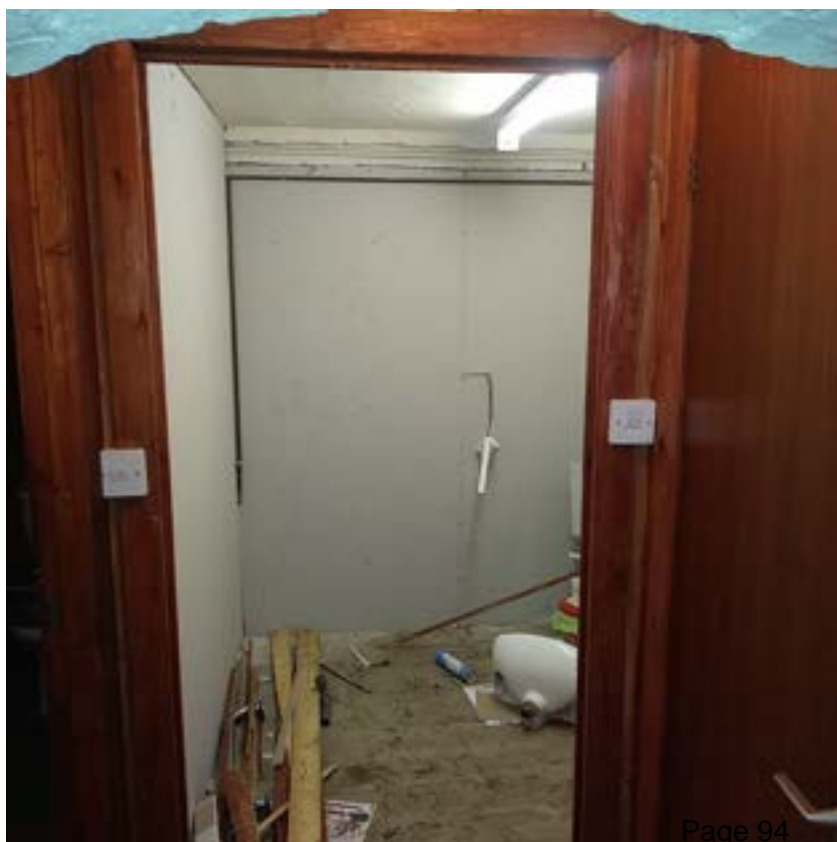


68549 – Item 4 – Internal photos

Photo 5



Photo 6



68549 – Item 4 – Internal photos

Photo 7



Photo 8



Photo 9





Page 97

PROPOSED SITE BLOCK PLAN
1 : 200

- PROPOSED RAILING
- 62 LINEAR METERS
- PROPOSED GATES
- 3 NO SINGLE SWING (G1, G2, G3)
- 1 NO DOUBLE SWING (G4)
- SITE BOUNDARY

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 200**

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PROPOSED SITE
BLOCK PLAN

Project no.	Drawing no.	Rev.
305	D.01	B



RAILING EXAMPLE

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
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Date **09 JUN 22** Scale **1 : 10**

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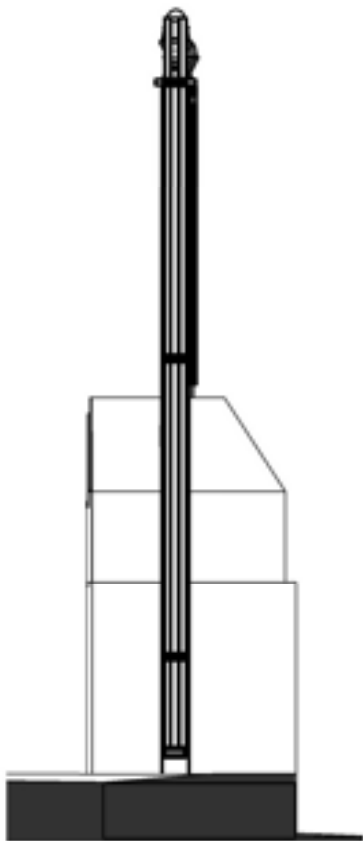
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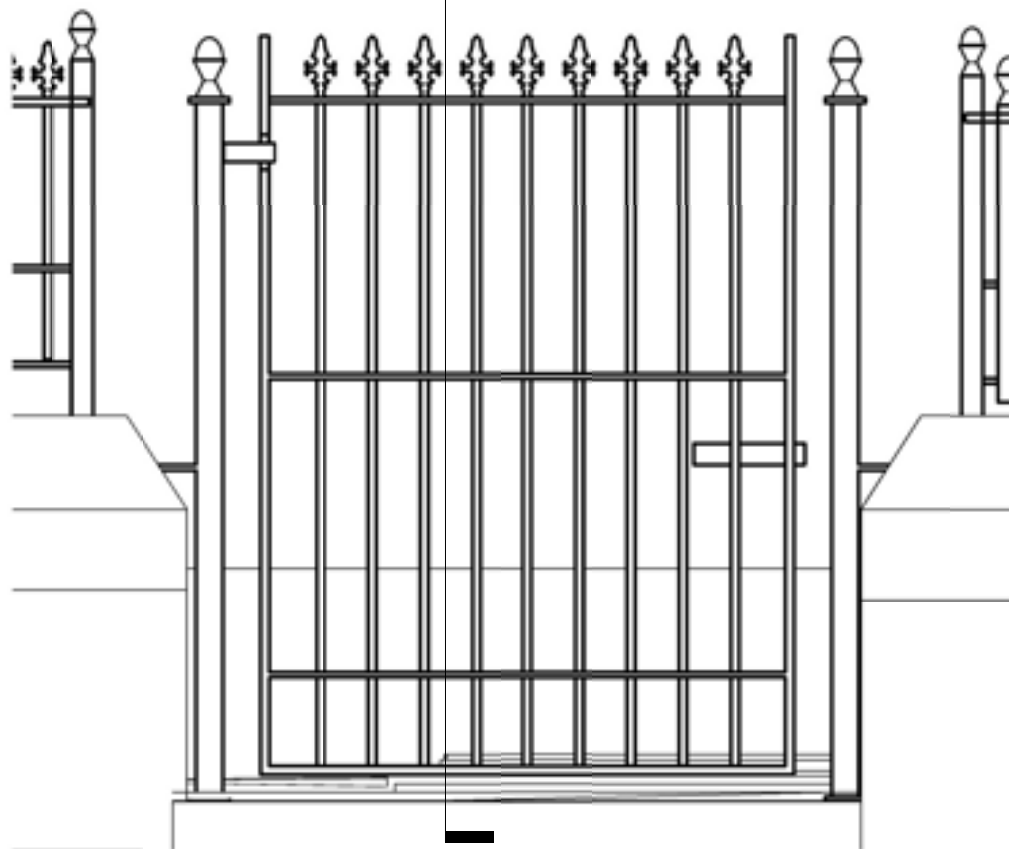
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**PROPOSED
EXTERNAL GATE
G1**

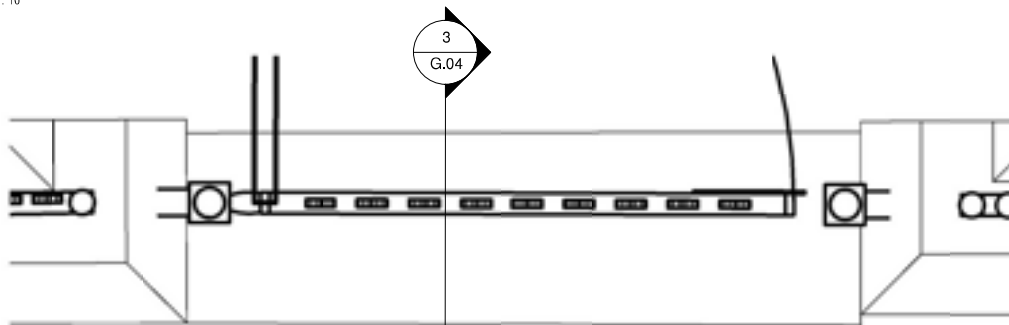
Project no.	Drawing no.	Rev.
305	G.04	B



3 PROPOSED SECTION - GATE G1
1 : 10



2 PROPOSED ELEVATION - GATE G1
1 : 10



1 PROPOSED FLOOR PLAN - GATE G1
1 : 10

0 m 1 m 2 m 3 m 4 m 5 m

NEW PROPOSED METAL GATE



RAILING EXAMPLE

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	IO
B	9,11,22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 10**

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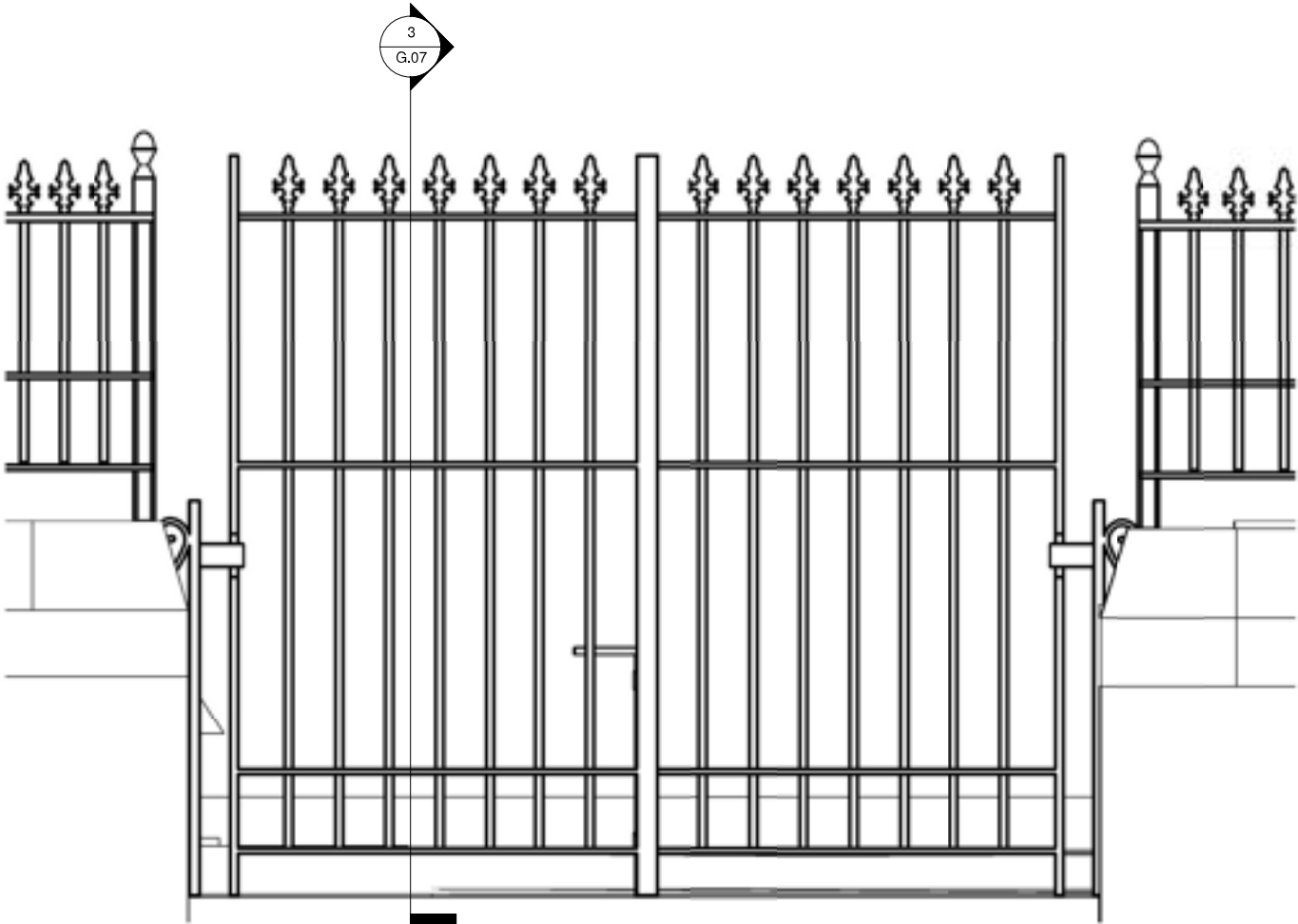
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**PROPOSED
EXTERNAL GATE
G2**

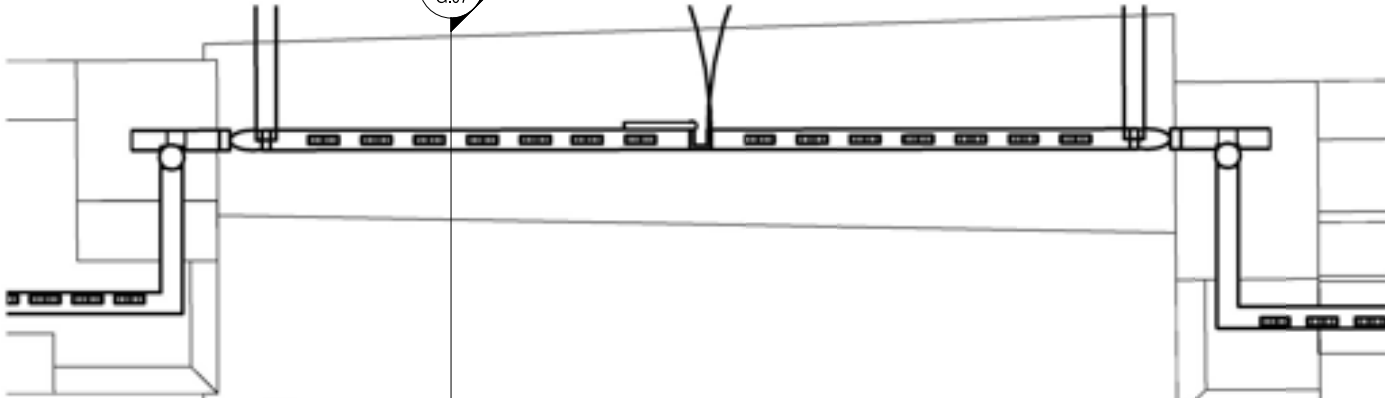
Project no.	Drawing no.	Rev.
305	G.07	B



3 PROPOSED SECTION - GATE G2
1 : 10



2 PROPOSED ELEVATION - GATE G2
1 : 10



1 PROPOSED FLOOR PLAN - GATE G2
1 : 10

NEW PROPOSED GATE



RAILING EXAMPLE

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
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Date **09 JUN 22** Scale **1 : 10**

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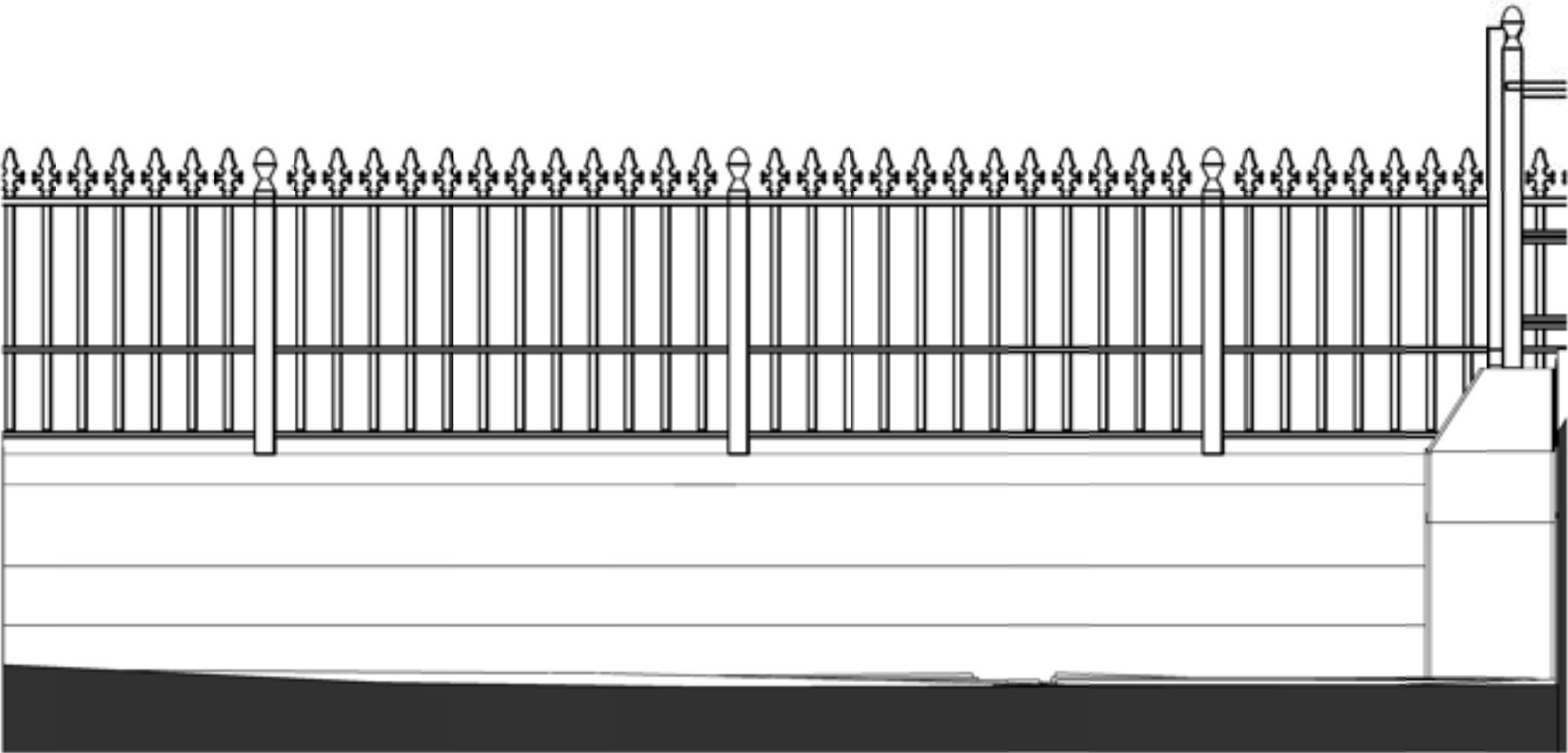
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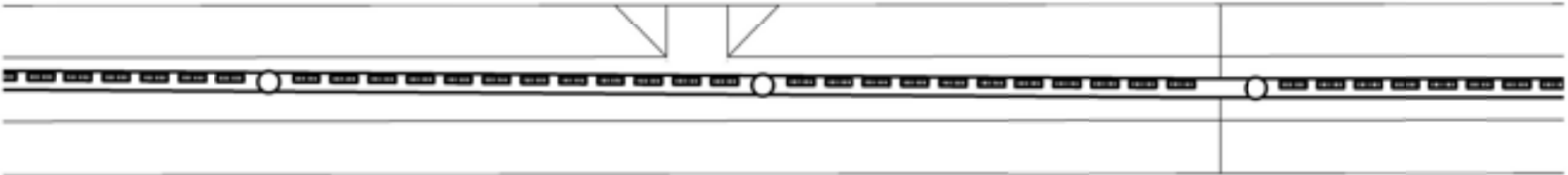
PROPOSED
EXTERNAL
RAILING

Project no.	Drawing no.	Rev.
305	G.08	A

Page 100

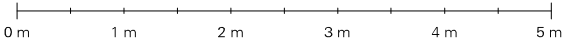


2 ELEVATION
1 : 10



1 FLOOR PLAN

NEW PROPOSED EXTERNAL RAILINGS





RAILING EXAMPLE

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RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO
B	9.11.22	EXTERNAL RAILING DESIGN UPDATED	IO

STAGE 3
PLANNING

Client

MR A ADESANYA

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Date 09 JUN 22 Scale 1 : 20

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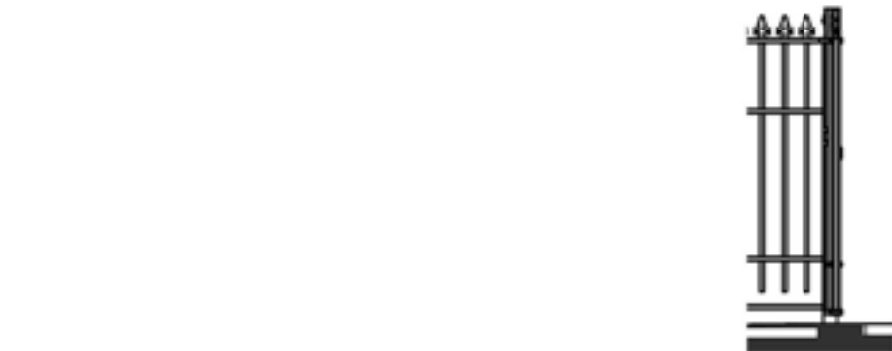
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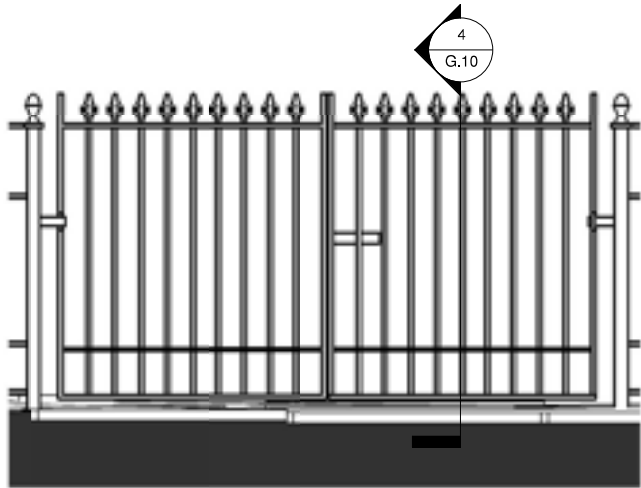
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PROPOSED
EXTERNAL GATE
G4 (ELEVATIONS)

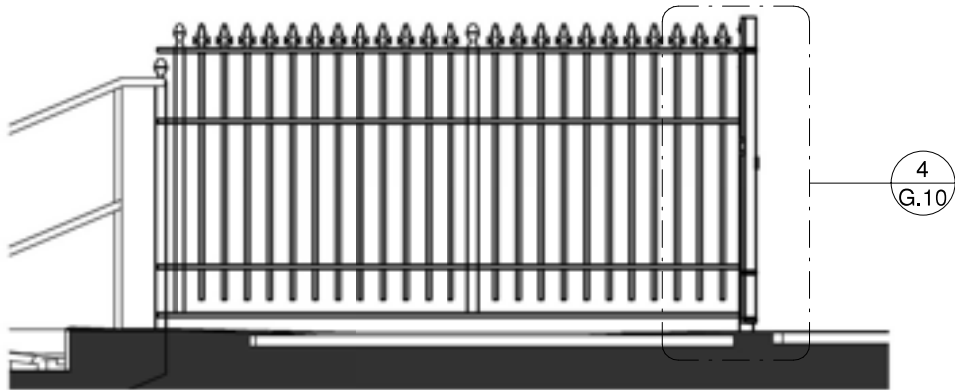
Project no.	Drawing no.	Rev.
305	G.10	B



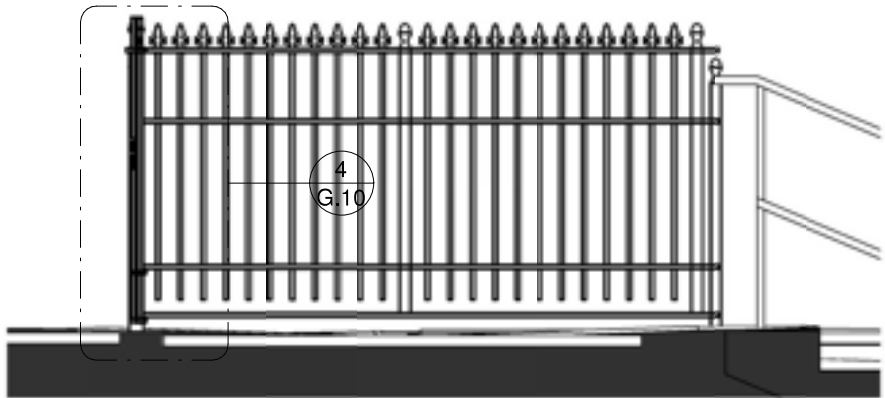
4 Section 3
1 : 20



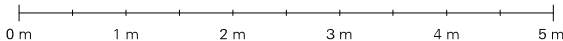
1 Elevation 24 - a
1 : 20



3 Elevation 24 - d
1 : 20
NEW PROPOSED METAL GATE

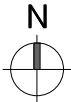


2 Elevation 24 - b
1 : 20





LOCATION MAP

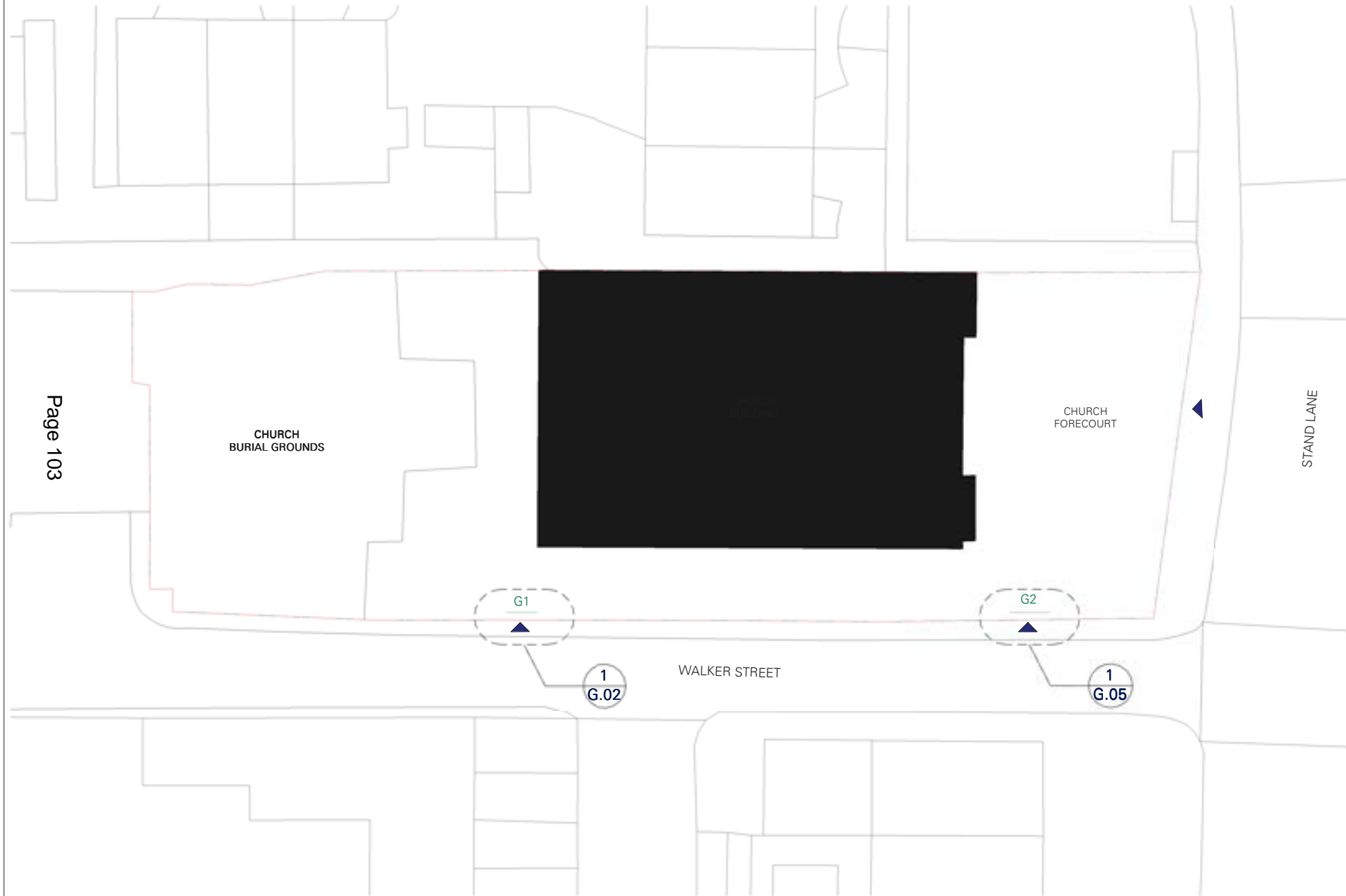




SITE BOUNDARY



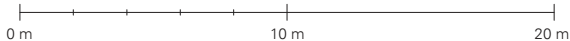
EXISTING
APPLICATION PROPERTY



Page 103

EXISTING SITE BLOCK PLAN

1 : 200



PROPOSED RAILING
- 82 LINEAR METERS

EXISTING GATES
- 2NO SINGLE SWING (G1 + G2)

SITE

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19.7.22	NOTES ADDED	IO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **As indicated**

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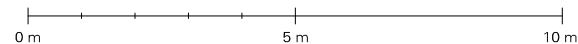
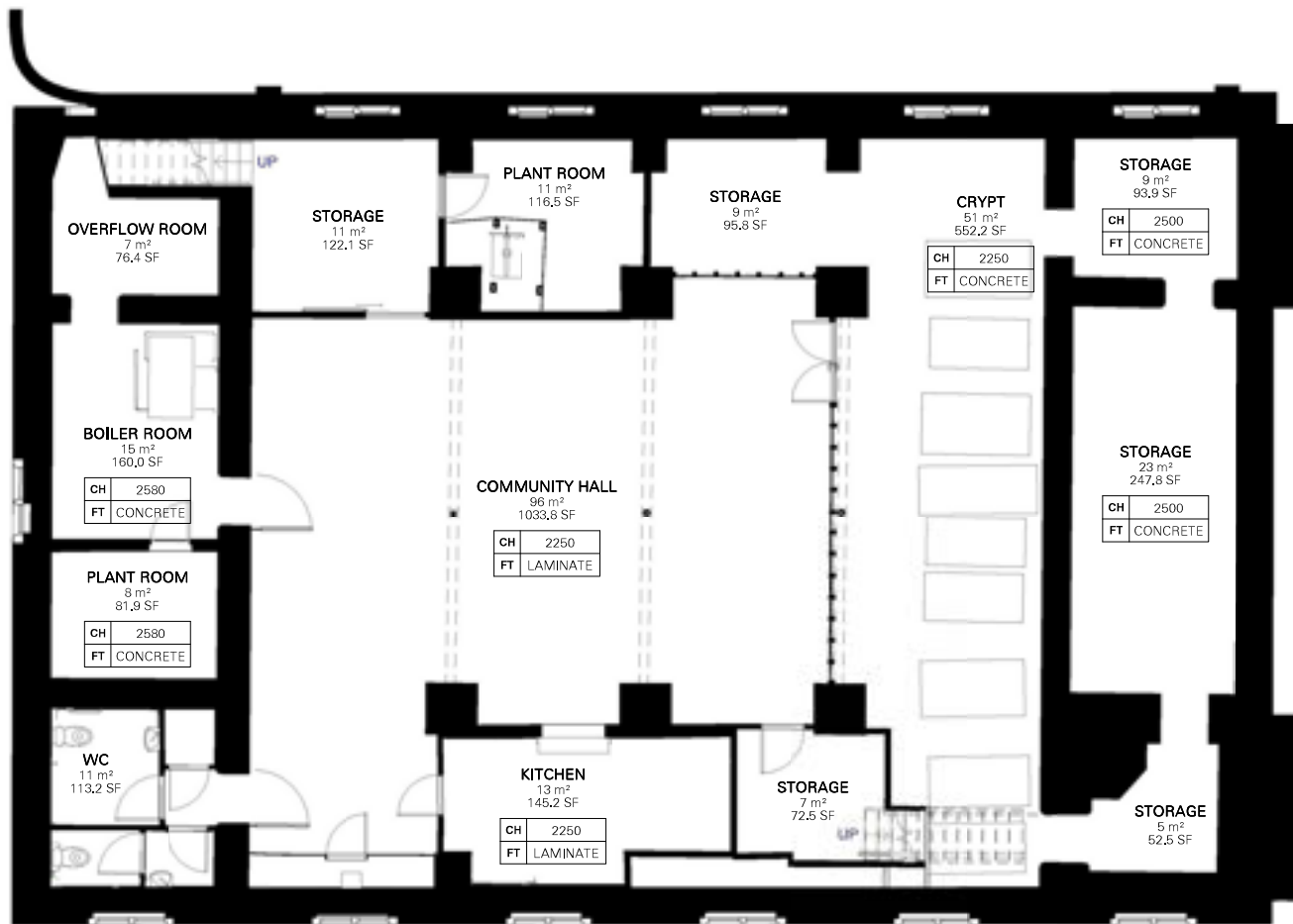
EXISTING SITE
BLOCK PLAN

Project no.	Drawing no.	Rev.
305	A.01	A



BOUNDARY

EXISTING
BUILT FABRIC



IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 100**

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**EXISTING GA
BASEMENT
FLOOR PLAN**

Project no.	Drawing no.	Rev.
305	A.02	A



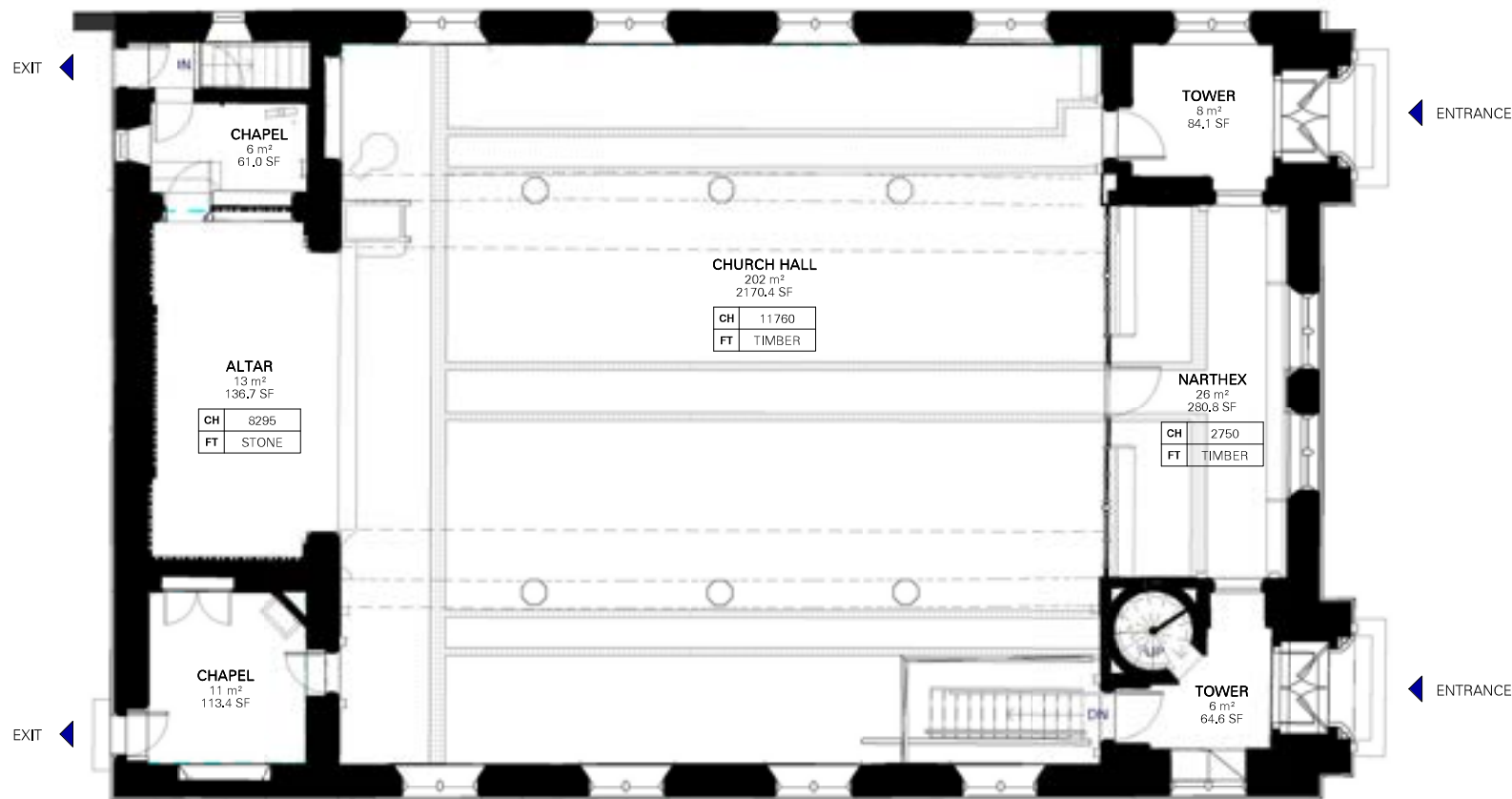
BOUNDARY

EXISTING
BUILT FABRIC

EXISTING FLOOR GRILLS
:SERVICING PIPES



EXISTING FLOOR
GRILLES



IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 100**

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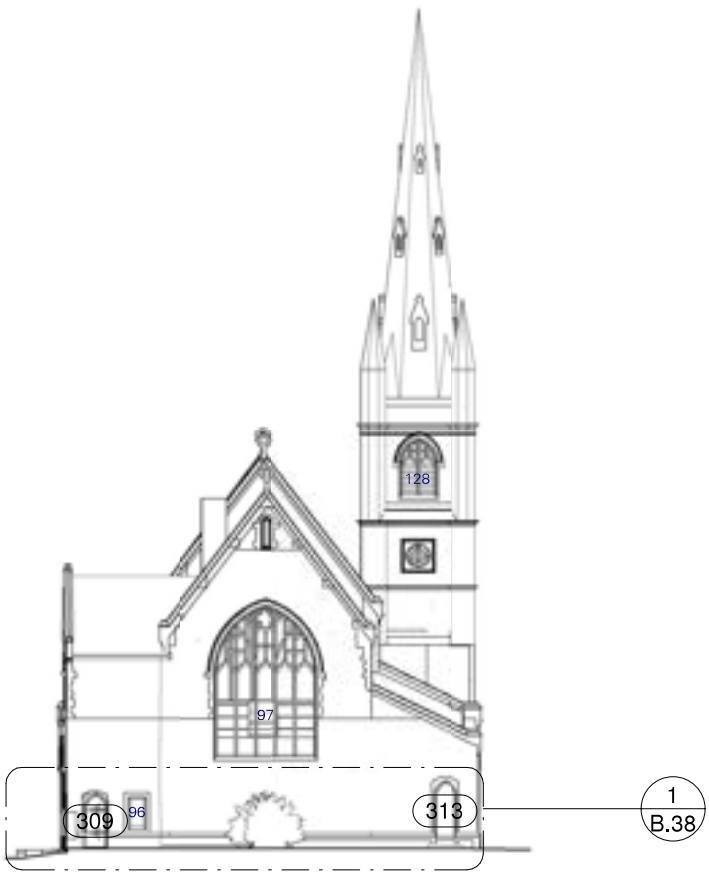
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EXISTING GA
GROUND FLOOR
PLAN

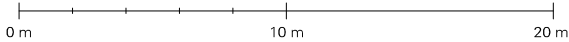
Project no.	Drawing no.	Rev.
305	A.03	A



FRONT ELEVATION
(FACING STAND LANE)



REAR ELEVATION
(FACING CHAPELFIELD ROAD)



IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 200**

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**EXISTING GA
FRONT & REAR
ELEVATIONS**

Project no.	Drawing no.	Rev.
305	A.06	A



- A. RUSTICATED DWARF SANDSTONE WALLS WITH STONE COPING STONES.
- B. EXISTING RED BRICK WALL

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE, MANCHESTER, M26 1JE

Date 09 JUN 22 Scale 1 : 200

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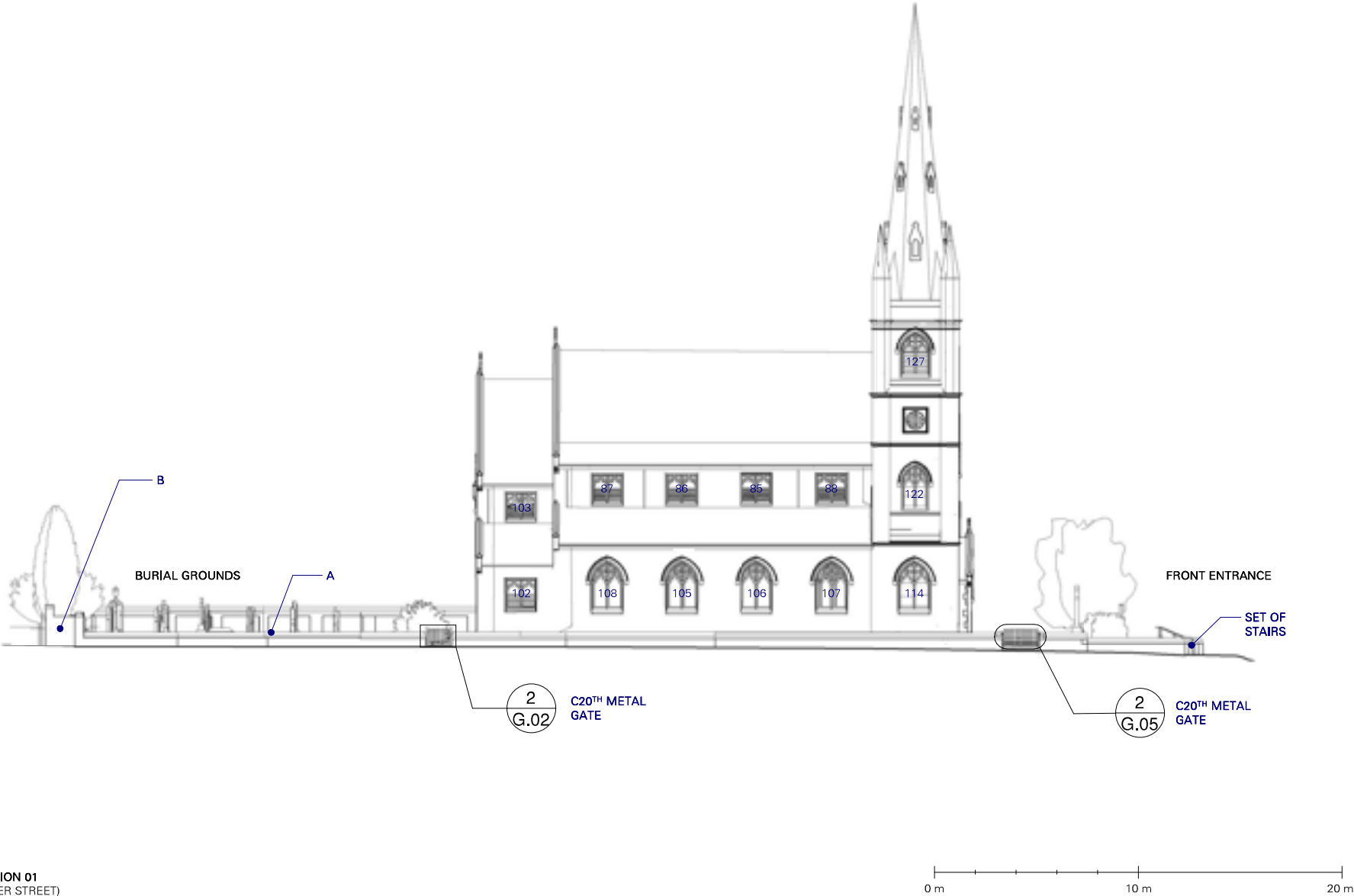
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EXISTING
WALKER STREET
ELEVATION

Project no.	Drawing no.	Rev.
305	A.07	A



FLANK ELEVATION 01
(FACING WALKER STREET)



FLANK ELEVATION 02
(FACING CHAPELFIELD ROAD)

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

Date **09 JUN 22** Scale **1 : 200**

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EXISTING GA
FLANK
ELEVATION 02

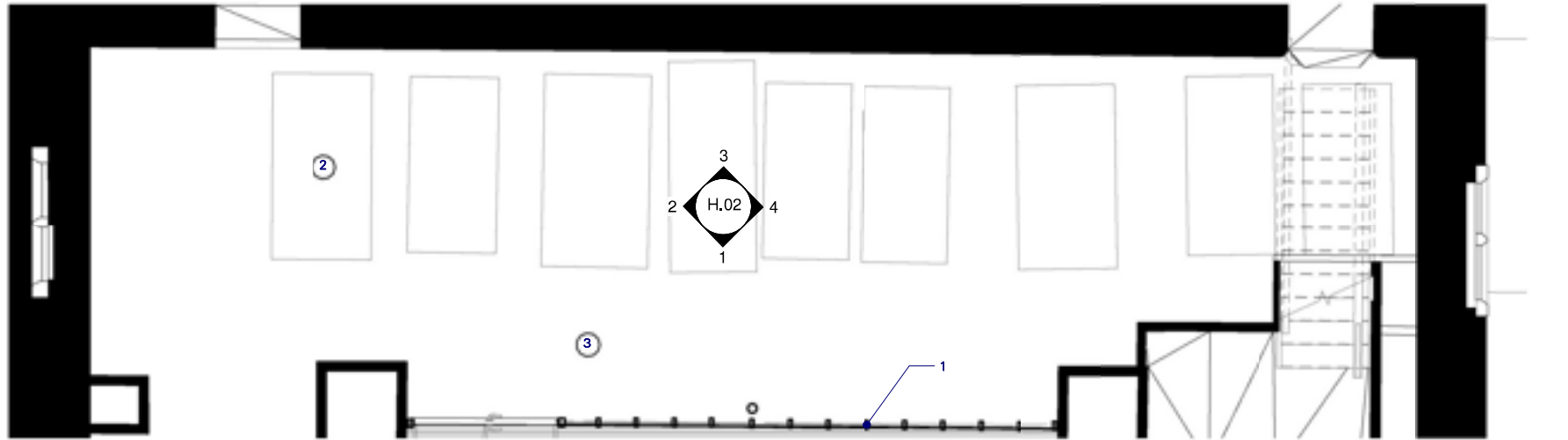
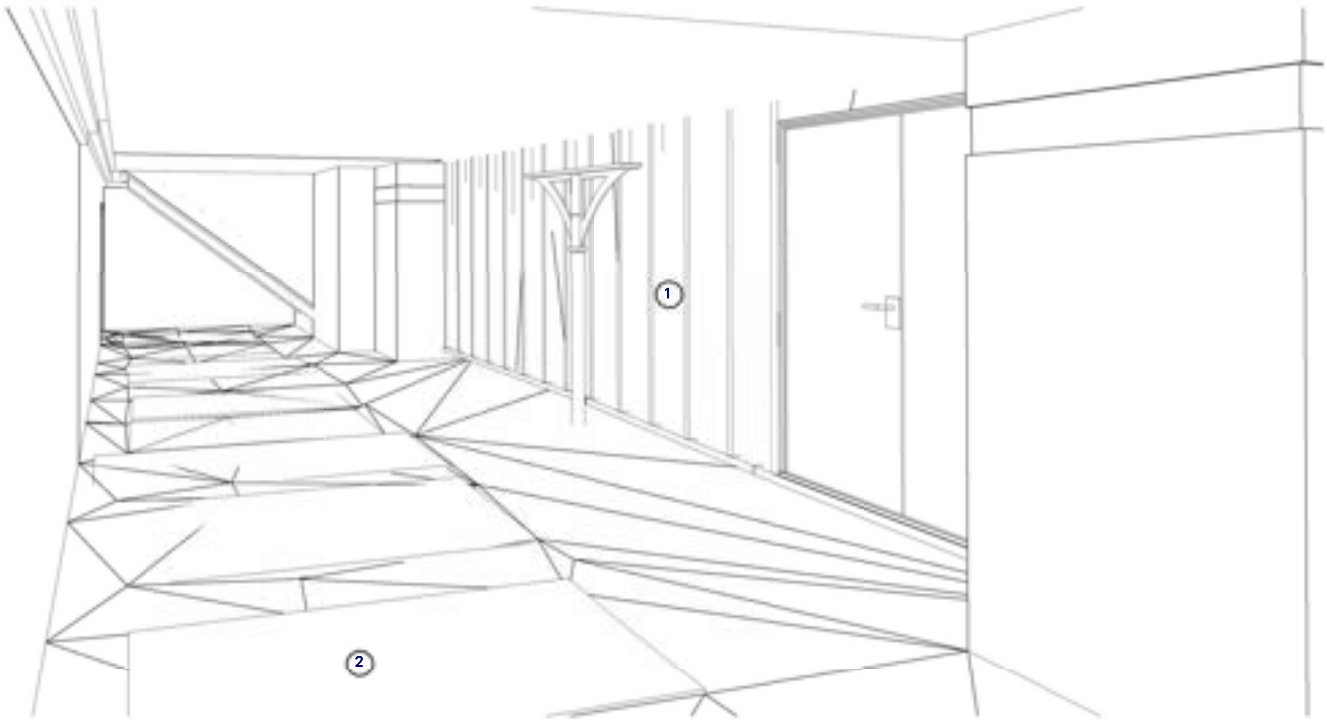
Project no.	Drawing no.	Rev.
305	A.08	A



- 1. C20TH MODERN INTERNAL PARTITION + DOOR SET
- 2. HORIZONTAL LAID FLOOR HEADSTONES
- 3.UNLEVELLED EXPOSED CONCRETE FLOOR

Page 109

2 BASEMENT CRYPT 3D VIEW - EXISTING



1 BASEMENT CRYPT FLOOR PLAN- EXISTING
1 : 50

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

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**SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE**

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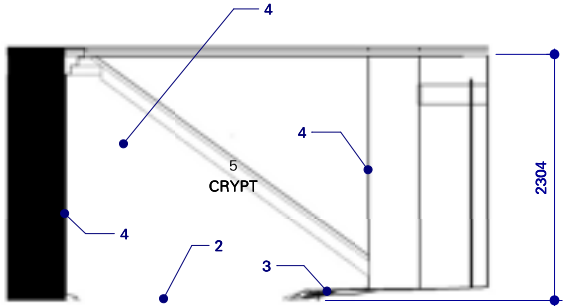
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**EXISTING - CRYPT
FLOOR PLAN**

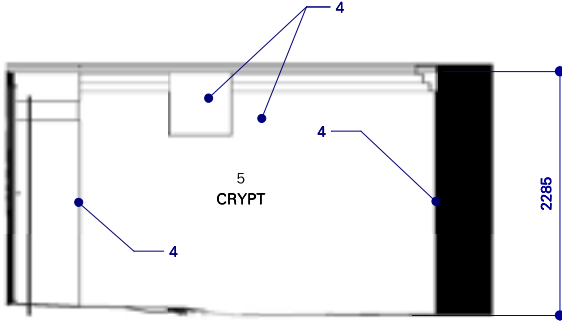
Project no.	Drawing no.	Rev.
305	H.01	A



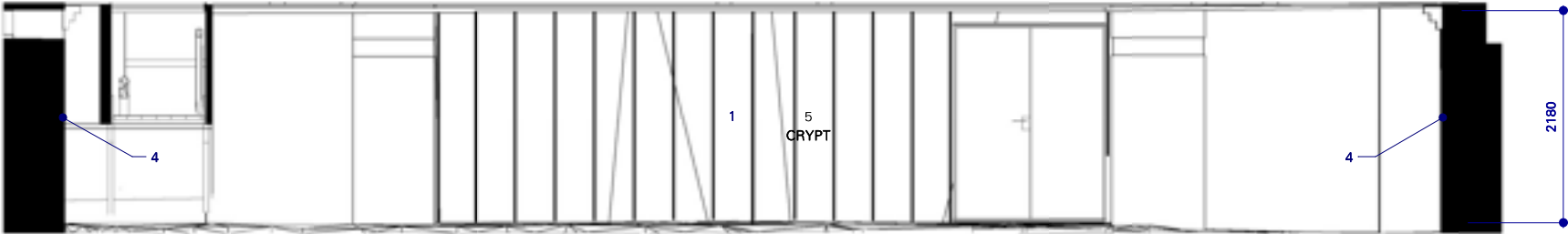
- 1. C20TH MODERN INTERNAL PARTITION + DOOR SET
- 2. FLOOR MOUNTED HEADSTONES
- 3. UNLEVELLED EXPOSED CONCRETE FLOOR
- 4. WALLS PAINTED



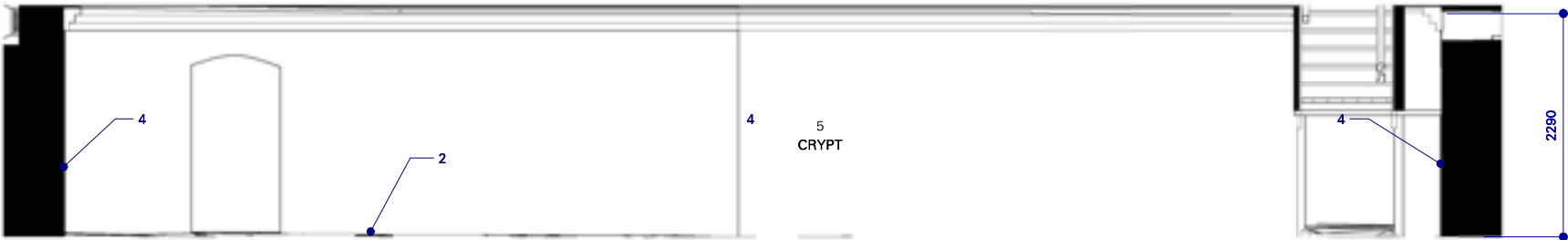
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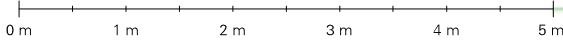
5. CRYPT: BASEMENT



5. CRYPT: BASEMENT



5. CRYPT: BASEMENT



IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
**SUR CHURCH, RADCLIFFE,
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Date **09 JUN 22** Scale **1 : 50**

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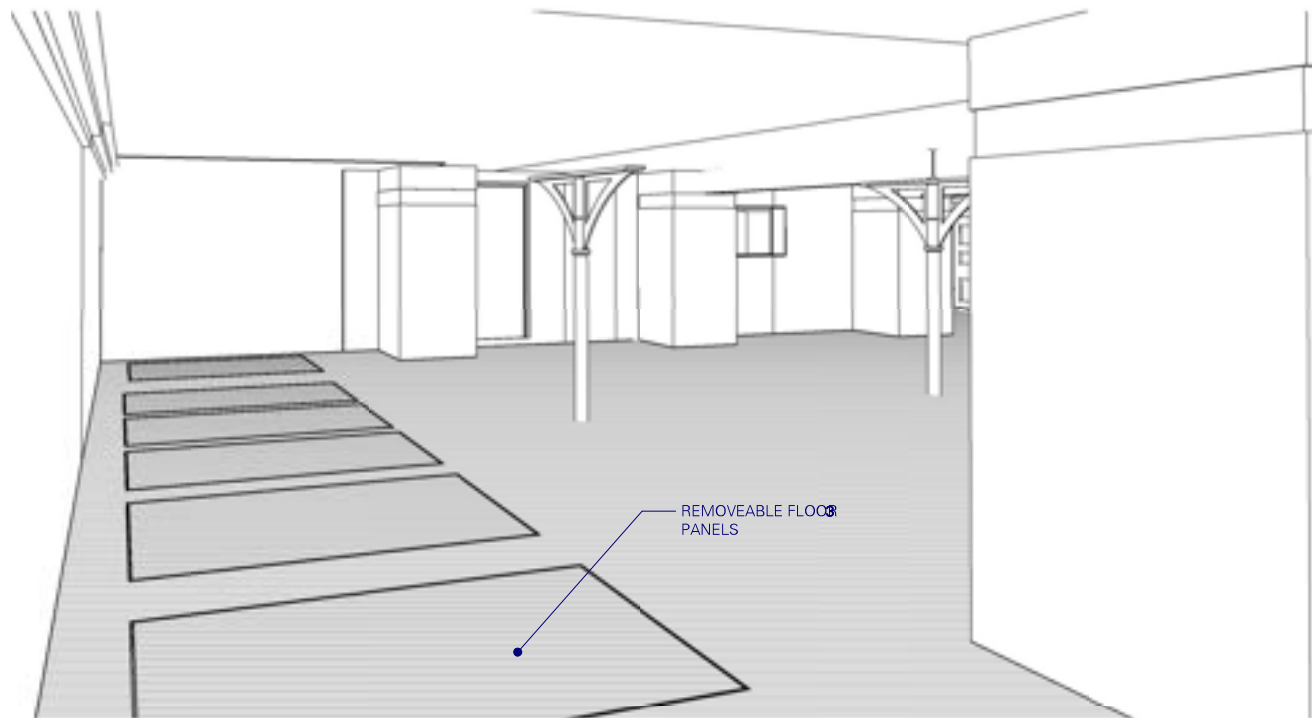
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EXISTING
INTERNAL
ELEVATIONS -
BT.13

Project no.	Drawing no.	Rev.
305	H.02	A

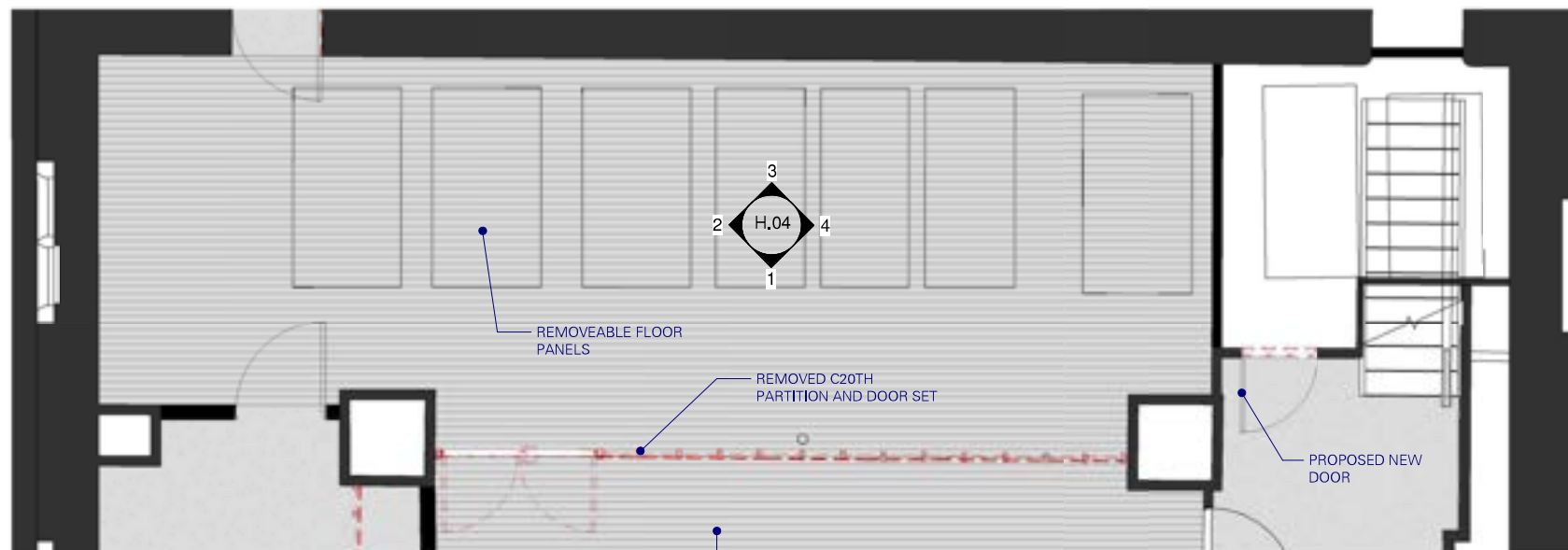


NEW 22MM ENGINEERED TIMBER
FLOOR FINISH ON FLOATING
TIMBER FLOOR (ONTOP OF
EXISTING CONCRETE FLOOR)



BASEMENT CRYPT 3D VIEW - PROPOSED

Page 111



2 BASEMENT CRYPT FLOOR PLAN- PROPOSED

1 : 50

NEW FLOOR FINISHES
THROUGHOUT

0 m 1 m 2 m 3 m 4 m 5 m

IKETECTURE

RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3 PLANNING

Client

MR A ADESANYA

Project address

SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date **09 JUN 22** Scale **As indicated**

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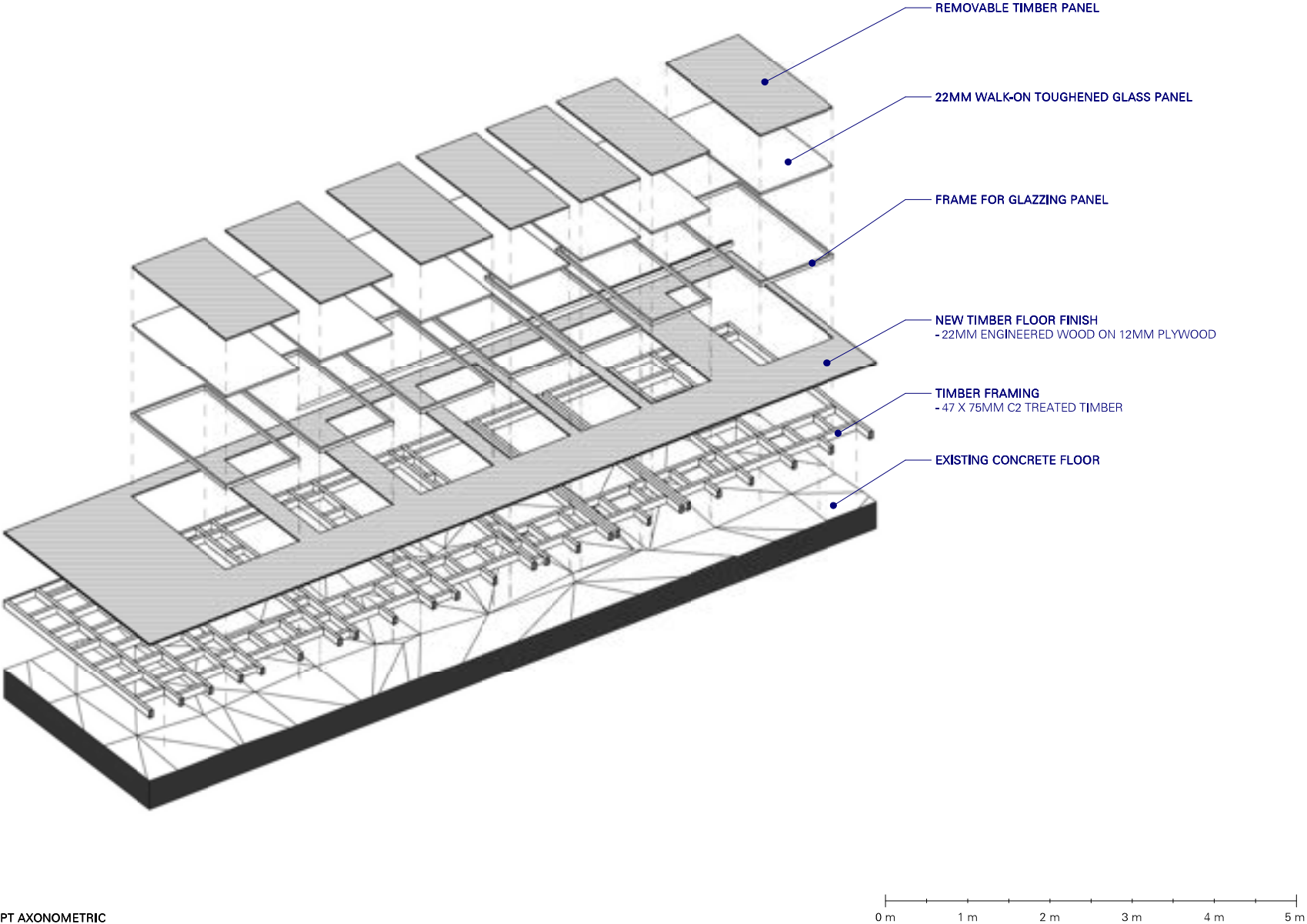
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PROPOSED - CRYPT FLOOR PLAN

Project no.	Drawing no.	Rev.
305	H.03	A



NEW RAISED FLOOR IN CRYPT AXONOMETRIC

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision Date Description Author

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date 09 JUN 22 Scale

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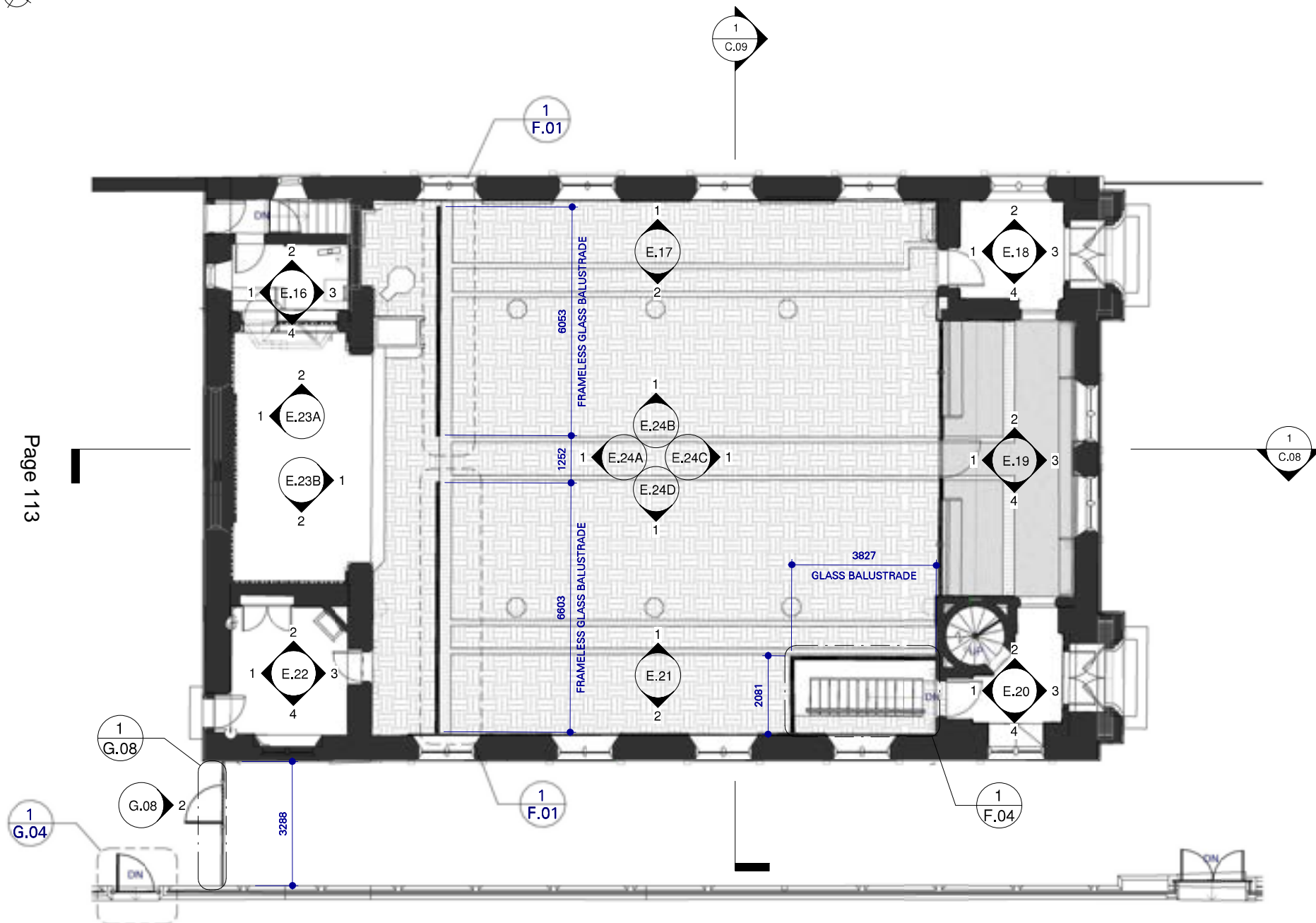
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NEW RAISED
FLOOR IN CRYPT
AXONOMETRIC

Project no.	Drawing no.	Rev.
305	H.05	



1:100

A horizontal number line representing distance in meters. It starts at 0 m on the left and ends at 10 m on the right. There are major tick marks at 0 m, 5 m, and 10 m. Between 0 m and 5 m, there are four minor tick marks, dividing the 5 m segment into five equal parts of 1 m each. Similarly, between 5 m and 10 m, there are four minor tick marks, dividing the 5 m segment into five equal parts of 1 m each. This means there are tick marks every 1 m along the entire line.

Project no.	Drawing no.	Rev.
305	E.02	A

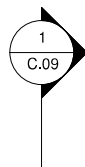


EXISTING
BUILT FABRIC

STONE FLOOR FINISH

ENGINEERED TIMBER
FLOOR FINISH

NEW NAVY BLUE,
HIGH TRAFFIC CARPET



Page 114

PROPOSED KS BASEMENT FLOOR PLAN

1 : 100

0 m 5 m 10 m

IKETECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client

MR A ADESANYA

Project address

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Date **09 JUN 22** Scale **1 : 100**

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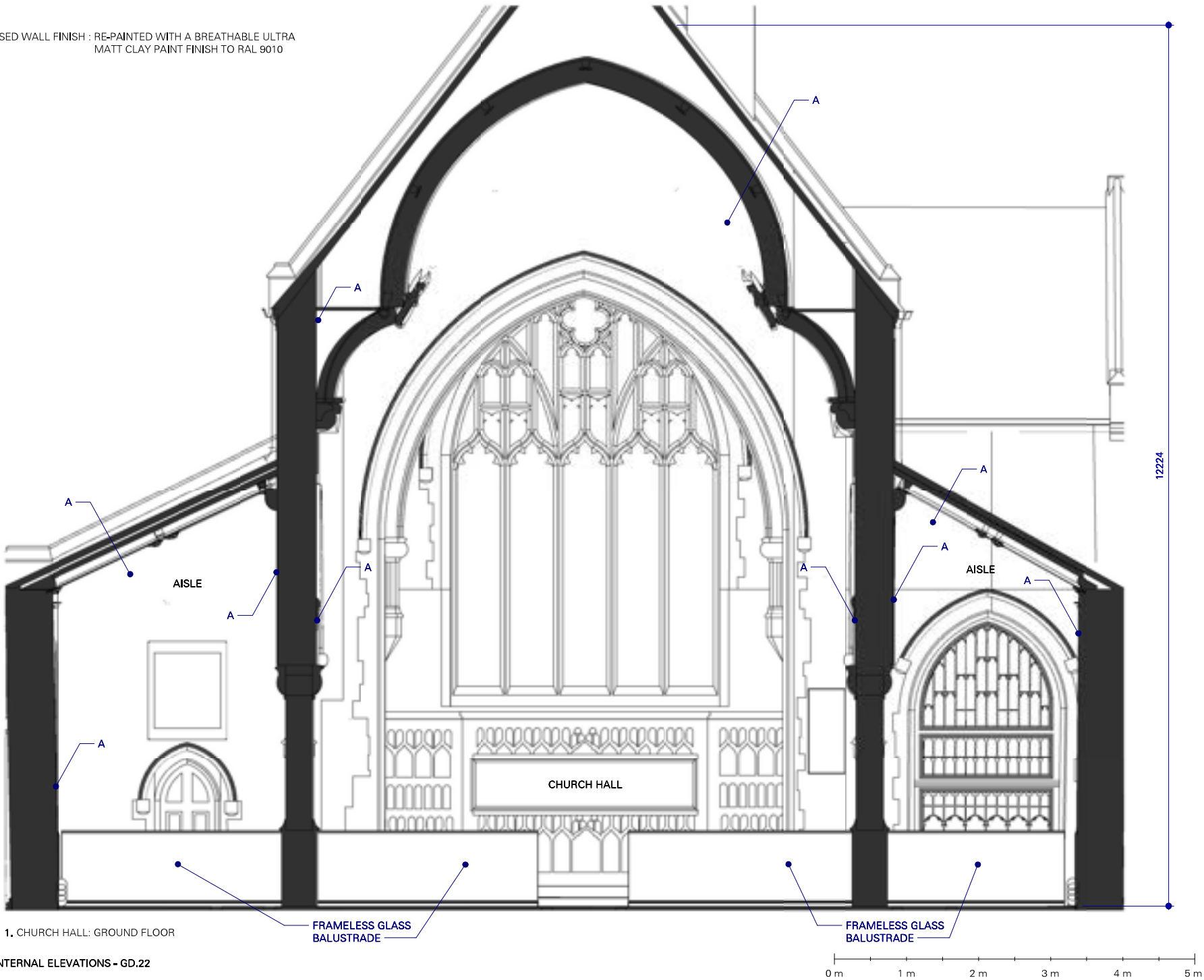
PROPOSED KS
BASEMENT
FLOOR PLAN

Project no.	Drawing no.	Rev.
305	E.01	A



A: PROPOSED WALL FINISH : RE-PAINTED WITH A BREATHABLE ULTRA
MATT CLAY PAINT FINISH TO RAL 9010

Page 115



PROPOSED INTERNAL ELEVATIONS - GD.22
1 : 50

IKECTURE
RIBA CHARTERED ARCHITECTS

Revision	Date	Description	Author
A	19,7,22	NOTES ADDED	JO

STAGE 3
PLANNING

Client
MR A ADESANYA

Project address
SUR CHURCH, RADCLIFFE,
MANCHESTER, M26 1JE

Date 09 JUN 22 Scale 1 : 50

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PROPOSED
INTERNAL
ELEVATIONS -
GD.22

Project no.	Drawing no.	Rev.
305	E.24A	A

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Ward: North Manor

Item 5

Applicant: Mr J Buckley

Location: Hollymount Farm, Hollymount Lane, Tottington, Bury, BL8 4HP

Proposal: Demolition works and redevelopment for 4no. new dwellings (3 new build, 1 barn conversion) with car ports and two storey extension at side of existing farmhouse; associated landscaping; access works.

Application Ref: 68505/Full

Target Date: 22/07/2022

Recommendation: Approve with Conditions

Description

The site is located at the lower end of a single lane track which continues past the site and leads to a residential hamlet, primary school and golf club. Claremont Lodge is the nearest residential property to the north. The site is otherwise bounded by open fields and farmland. The site is located on land designated as Green Belt, Area of Special landscape, West Pennine Moors, River Valley and Wildlife Corridor.

The site comprises an existing dwelling - Hollymount farmhouse, which is a 2 storey stone built cottage. Attached are single storey buildings, (also constructed of stone and formally used as a barn) and a lean-to open sided outbuilding and farm shop, which ceased use a number of years ago when the business became unviable. The remaining farm buildings comprise other agricultural buildings and some dilapidated outbuildings. Metal storage containers have also been brought onto the site over time and there is an extensive area of hardstanding within and around the site.

In the western corner of the site, is a mobile home which has a planning status as a permanent dwelling following the grant of a Certificate of Lawfulness in 2011 by virtue of it being in situ for more than 10 years.

The access to the site is via Hollymount Lane which leads to a relatively wide opening and a large area of hardstanding which surfaces the majority of the site. The access continues through the site in front of the existing farmhouse, barn and to the mobile home where there are areas for parking.

The farm and associated infrastructure have ceased to be used for any agricultural purposes for circa 10 years, save for habitation in parts of the site. the land surrounding the site is no longer utilised by the farm but is utilised by other farmsteads for sheep grazing as part of the land management.

The application seeks demolition works, removal of the mobile home, all temporary structures and agricultural buildings and the redevelopment of the site for 5 no. dwellings on the site, removal of hardstanding and returning land to Green Belt, SuDS pond and landscaping.

In detail the application comprises -

Type 1 - Existing farmhouse - new 2 storey side extension. 4 bed.

Type 2 - Barn conversion with 1st floor extension and attached car port. 5 bed.

Type 3 - 2 no. new detached farmhouses with attached car ports. 5 bed.

Type 4 - New detached dwelling to replace existing lawful mobile home. 5 bed.

All properties would be 2 storey in height and built of reclaimed stone with natural slate roof tiles.

The existing farmhouse and barn form a short linear row of buildings running east/west across the northerly part of the site (Types 1, 2 and 4) and two dwellings would front on to Hollymount Lane (Type 3).

The two new farmhouses (Type 3) would be located along the eastern boundary fronting Hollymount Lane, between which would be a double car port and bin store area for each property separated by a 1m gap. There would be a pedestrian access to the front of the houses created off Hollymount Lane. Gardens amenity space would be located at the rear.

The existing access to the site would be utilised with a driveway retained for Houses 1,2 and 4 and for dwellings fronting Hollymount Lane (Type 3), these would be accessed via an internal driveway which would lead to a turning area at the rear of the dwellings and the parking and carports for these properties.

The development also proposes to create two new passing places which would be formed on the approach to the site to assist traffic flows to and from the site and along the lane. New hedge planting within the lay-by's would replace areas of hedging removed. A pedestrian footway would also be provided to the site entrance with an area for bin storage collections.

The existing pedestrian access and public right of way which runs to the north of Hollymount Lane would remain unaffected.

A large part of the site is currently covered by the buildings (the existing 2 dwellings, agricultural barns and shelters and containers) on a concreted surface. The remaining site area comprises the access and an expanse of hardcore and hardstanding with a grassed area and tree planting along part of the northern boundary with Hollymount Lane and areas of self seeded grass near to the southern part of the site .

The proposed development would seek to reduce the amount of hardstanding within the development site and the built development area would be concentrated in the north and eastern parts.

The remaining land to the south of the development and within the site area would be 'given back' to the Green Belt. A SuDS pond and ecology area would be formed within this area to support the drainage of the site and seeks to enhance the appearance and ecology of the site.

Comparative site areas comprise:

Existing buildings, containers and hardstanding areas - 84.4% of the site.

Existing grassed area - 15.6% of the site.

Proposed buildings and hardstanding areas - 51.1% of the site.

Proposed grassed area - 48.9% of the site.

Total volume increase of built development of 7.7%

Relevant Planning History

01802/E - Conversion of farm buildings into 4 no. dwellings - Enquiry completed 14/06/2016

02170/E - Proposed residential scheme of 5 no. dwellings - Enquiry completed 06/09/2018

02650/E - Residential development - 6 dwellings - Enquiry completed 07/01/2022

53930 - Lawful Development Certificate for existing use of site for siting of a mobile home in breach of condition no. 3 of planning permission 28271/93 - Lawful Development 17/06/2011

54962 - Demolition of 4 barn buildings & conversion of redundant agricultural buildings to form 3 dwellings, including reconstruction of external walls & raising of roof (Resubmission

of 54376) - Refused 15/06/2012

60437 - Conversion of farm building into 4 no. residential units; Demolition of the other farm buildings and replacement of mobile home with permanent dwelling - Approve with Conditions 28/09/2016

Publicity

Letters sent to 41 addresses on 20/6/22 within the vicinity of the site. A full address list can be found on the file but covers broadly Hollymount Lane and Brookside Crescent.

Site notice posted 13/7/22

Press advert 23/6/22

52 representations received.

6 objections

45 support

1 comment

Objections:

- Drastically alter the natural beauty of the surrounding landscape, which in turn will spoil the enjoyment of the local walks.
- Spoil the view for many surrounding houses, particularly those situated close to and directly opposite the site. The area is well known for being quiet and peaceful.
- What's to say if this is approved, that another and then another application isn't also approved.
- Many more cars now use Brookside Crescent as a car park when collecting children from Holly Mount School and this often impacts access to our own properties.
- The proposed development is of a far greater footprint than the original application that has since expired. It will extend out into green fields. The land is Greenbelt land with rich bio diversity, wild deer, newts, bats and fauna.
- The proposed development is only accessible by a private lane that the Hollymount lane association pay for its maintenance. We do not give permission for access and have not been consulted.
- Fails to meet affordable housing criteria.
- There is unique and historical significance to Hollymount, its farm and the catholic community.
- Such a development will probably impact negatively on school access, and local businesses such as nursery & golf club.
- If it is to go ahead it should be restricted to the original brown field scale & size and maintain the Greenbelt
- Bats have been seen in the farm barn and buildings so I would like reassurance on how the bats and their place of dwellings are being protected? Have they been considered in this application?
- Traffic - Hollymount Lane school traffic, access and parking has long been a serious problem for both emergency service access to the lane, residents, school and golf club. The lane is a single file lane with only a very few small passing points. Adding any additional traffic is only going to create more mayhem and danger to residents, the traffic on Holcombe Road and children travelling to and from Hollymount school. The situation is likely to get even worse with more dwellings on the lane. How can you assure residents that in the event of an emergency that emergency vehicles can have free access up the single file Hollymount Lane - increasing the number of vehicles on the lane is going to increase the risk and danger associated to all of all of the points above.
- The water main supplying the residents of Hollymount is old, out dated and already unable to deal with the water supply with frequent severe flooding exacerbating the traffic situation as the manhole floods and raises leaving a small gap for traffic to pass. The council do not accept responsibility and I believe that the connection to the mains water supply was one that was not council approved - are you now going to let the developer tap into what is already a failing water supply system? How can you reassure residents that our water supply will not be negatively impacted by this development?
- Hollymount Orchard is one of the oldest orchards in England. Has any consideration

been given for how the development, construction and disruption will impact the wildlife and ecosystem including bees that live within Hollymount Orchard?

Support

- Aesthetically, this is a much more appealing scheme than the previous proposal and is in keeping with the surrounding area.
- As a local resident, a member of the Golf Club and a contributor to the Lanes committee fund, I strongly disagree with previous comments about the impact of just 5 dwellings on Hollymount Lane. The one and only element that impacts congestion on the Lane is school traffic for just 20 minutes twice a day, 5 days a week, something most residents and golfers try to avoid.
- In respect of the size and scale of the development or impact on Greenbelt, the plans appear to show the development within the existing footprint of buildings and concrete / hard standing and whilst it is larger than the previously proposed scheme, I fail to see how there will be any detrimental effect on the surrounding wildlife.
- This looks like a potentially fantastic development, certainly in keeping with the already established surrounding properties along Hollymount Lane in terms of building materials and quality.
- The farm at the bottom of the lane is rundown and slowly becoming an "eyesore" and therefore the proposed development is timely and welcome.
- Looking at the plans this is a great opportunity to update and re-purpose a much loved local family farm using brown field area. This will visibly elevate the start of Hollymount lane and bring it in character with the rest of Greenmount.
- The developer has recently completed a fabulous 4 property development in Hawkshaw, and if this is anything to go by, the standard of the new properties will only enhance the surrounding area and local properties. My guess would be that the properties would not be affordable housing, but any new housing in the Bury area will naturally filter down releasing properties at the lower price range in the borough.
- I support this application on the basis the existing buildings are dilapidated and new buildings will improve the overall aesthetic of Greenmount. The proposed buildings are simply replacing old for new, enhancing the surrounding area.
- Will not impact on any Green belt land
- The widening of the road will make car journeys more convenient and safer for pedestrians.

Comment

- Please can responsibility for Hollymount lane be checked as part of this planning application? My understanding is Hollymount farm owns the lane and is responsible for the maintenance. If this is correct does responsibility rest with the new dwellings or the local authority?
- Also why were the bankings next to the stream built up by the developer last summer? The fields and lane regularly flood and are impassable here when there is heavy rain, I understand the work done will temporarily reduce the flooding but it is a short term measure that will quickly erode. If flood defenses are needed does this need to be included in the development plans?
- Believe it should be developed within the planning guidelines and not over developed. The previous granted planning permission was for 5 houses. The foot print of the site has not changed but the developer is proposing increasing the plans to 6 houses which is much larger than the original footprint.
- I note that the architect has included in the existing buildings square meterage at least 7 mobile caravans and also at least 5 shipping containers which have been a recent addition to the site and were not included or even present when the previous permission was granted. The shipping containers have only arrived in the last 18 months. None of these should be classed as existing buildings for planning purposes.

Following original notification of the application, revised plans have been received to show amendments to the reduction to the number of dwellings, amendments to the proposed site layout and amended house types.

One comment received:

- The new plans seem to be less houses than the original application so I assume means the development is to be contained on the brownfield site only and not extend into the green fields as per original plans. This I now generally support. The builder has stated on here that he plans to put in passing places on the lane - hedgerows are protected by law and so this must not happen. It is still my belief the builder would like to erode boundaries with the ultimate end of building all over the field. It is disappointing no environmental impact or wildlife surveys are listed in the documents. If we can ensure these final items are taken into account I'll withdraw my objection.

Those who have made representations have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection subject to conditions

Greater Manchester Ecology Unit - No objection subject to conditions/informatives

Public Rights of Way Officer - No objection.

Environmental Health - Contaminated Land - No objection subject to conditions

United Utilities (Water and waste) - No objection subject to condition.

Waste Management - No objection

Greater Manchester Fire and Rescue Service - To be reported in the supplementary report.

Pre-start Conditions - Applicant/Agent agreed with pre-start conditions

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN6/3	Features of Ecological Value
EN6/4	Wildlife Links and Corridors
EN7	Pollution Control
EN7/5	Waste Water Management
EN9/1	Special Landscape Areas
OL1/4	Conversion and Re-use of Buildings in the Green Belt
OL5/2	Development in River Valleys
OL7/2	West Pennine Moors
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD8	DC Policy Guidance Note 8 - New Buildings in the Green Belt
SPD 9	Conversion and Re-use of Buildings in the Green Belt
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be

specifically mentioned.

Green Belt/Environment Policies - Paragraphs 147 and 148 state that inappropriate development in the Green Belt is by definition, harmful and should not be approved except in Very Special Circumstances (VSC). Planning Authorities should ensure that substantial weight is given to any harm in the Green Belt. VSC will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Paragraph 149 considers the construction of new buildings as inappropriate development in the Green Belt. Exceptions to this include:

- c) the extension or alteration of a building provided it does not result in disproportionate additions over and above the size of the original building;
- d) replacement of a building provided the new dwelling does not result in disproportionate additions over and above the size of the original building;
- g) limited infilling or partial or complete redevelopment of previously developed land, whether redundant or in continuing use (excluding temporary buildings), which would: not have a greater impact on the openness of the green belt than the existing development:

Paragraph 150 states that other forms of development are also not inappropriate provided they preserve its openness and do not conflict with the purposes of including land within it, which includes (d) the re-use of buildings provided that the buildings are of permanent and substantial construction.

Policy EN9/1 - Special landscape areas strictly control development which is required to be sympathetic to surroundings in terms of visual impact. High standards of design siting and landscaping are expected.

UDP Policy OL5/2 - Development in River valleys states development including new buildings should not be permitted unless it would not lead to the division of open parts of the valley where designated as Green Belt established policies will apply.

UDP Policy OL7/2 - West Pennine Moors ensures the important character of the area is protected and focuses not only on the control of development but measures to enhance the environment.

SPD8 - New Buildings and Associated Development in the Green Belt offers more detailed advice in terms of acceptability including scale and form, siting and visual design and materials. SPD8 advises that extension should not be disproportionate to the existing dwelling and generally considers extensions up to a third of the volume of the original dwelling, and in consideration of individual merits.

SPD9 - Conversion of Buildings in the Green Belt advises further on acceptability of schemes in terms of impact on character and appearance of the retained building, impact on openness of the Green Belt and capability of the conversion itself.

Green Belt principle

The site has previously benefited from the grant of a planning permission for redevelopment for 5 dwellings in total to include conversion, extensions and demolition and a replacement dwelling which was approved in 2016.

This development proposes an extension to an existing dwelling, conversion works to form a new dwelling, a replacement dwelling, two new dwellings and demolition of existing buildings and removal of temporary structures.

Whilst the whole of the application site is on land that has been previously developed, in that there are dwellings, agricultural barns, structures and storage buildings and containers and such like, the historical use was as a farm and therefore the site cannot wholly be considered as previously developed land, as agricultural buildings are exempt under the

definition of previously developed land in the NPPF.

With this in mind, there are elements of the proposed development which could be considered appropriate development in the Green Belt. These are for the proposed extensions, conversion, and a replacement dwelling which relate to Types 1, 2 and 4 in accordance with (c) extension/alteration of an existing dwelling, (d) replacement of a building of para 149 and (d) re-use of buildings of permanent/substantial construction of para 150 of the NPPF, providing there would not be a harmful impact on the openness of the Green Belt. Other elements - Type 3 would be inappropriate development and therefore require very special circumstances to be demonstrated.

Considering each individual element:

Type 1 - Existing farmhouse dwelling - It is proposed to add a 2 storey side extension which would replace a single storey store/garage. The extension would be set back from the front elevation and lower in height than the existing ridge. The existing dwelling is located internally within the site and not highly visible from public views or Hollymount Lane. The proposed extension would be a subservient addition to the existing property and the extension would be sensitively designed and constructed of random stone with a slate roof which would match the existing house.

It is therefore considered the proposed development would be acceptable and compliant with OL1/2, SPD8 and the principles of para 149 c) of the NPPF.

Type 2 - Barn conversion - The existing barn is a single storey stone built structure which is located adjacent to the farmhouse. It is proposed to extend and add a 1st floor extension. The proposal to convert buildings and add extensions in the Green Belt is considered appropriate development under d) para 150 of the NPPF provided it would preserve openness and not conflict with purposes of including land within it. There are 2 storey agricultural buildings on the site as well as the existing dwelling and within its setting and the context of the redevelopment of the site, the conversion would centrally integrate within the site and development as a whole and as such would not significantly harm the openness of the Green Belt.

It is therefore considered the proposed development would be acceptable and compliant with OL1/4, SPD9 and the principles of para 150 d) of the NPPF.

Type 4 - Replacement dwelling - The proposed dwelling would be located on part of the footprint of the existing prefabricated home which has the benefit of a lawful residential use following the grant of a lawful development certificate in 2011 and as such it can reasonably be considered as a replacement dwelling.

Replacement dwellings are acceptable in principle under d) para 149 of the NPPF provided the new dwelling does not result in disproportionate additions over and above the size of the original building.

The dwelling would be located adjacent to the existing farmhouse and when viewed from the front (north), would be perceived as a single storey cottage type build which would integrate as a subservient addition to an established pattern of existing built form. Similarly, and due to the difference in levels, the western elevation would appear single storey and not significantly higher than the existing mobile home. It is only from the rear that the dwelling would be viewed as a 2 storey build and as views from that direction would be long range, and given the backdrop of the development and integration within the site levels and land, it is considered the development would maintain the openness of the Green Belt for this element.

It is therefore considered the proposed development would be acceptable and compliant with OL1/2, SPD8 and the principles of para 149 d) of the NPPF.

The proposals as described above would relate to existing dwellings and buildings which are set within the nucleus of built form within the site.

Contextually and visually, the proposals would not impact on views through the site and character and openness of Green Belt land considered not to be significantly affected. Design and materials of the new additions would emulate the rural and agricultural setting, with reference taken from the existing farmhouse by the use of reclaimed stone and natural slate tiles. Window sizes and positions would be randomly inserted with some openings designed to replicate those of a typical barn building. As such, this approach is considered to be an appropriate solution which would not affect the character of the Green Belt.

Type 3 - Two new dwellings

The proposed development comprising the two new dwellings - Type 3 - would be considered to be inappropriate development as they would be new buildings in the Green Belt, would not replace existing structures and would not be on land which is considered to be previously developed land. In accordance with the NPPF, Very Special Circumstances (VSC) would need to demonstrate that potential harm would be outweighed by other considerations.

A case has been submitted for Very Special Circumstances.

1. A crucial element of the scheme is the proposal to 'gift back' a significant amount of the existing developed site to 'open' Green Belt land which would comprise 17.26% (931 sqm) of the total site area. This would be of a considerable benefit in terms of reducing the impact on the openness of the Green Belt.
2. The existing buildings, containers and hardstanding equates to 84.4% of the site with only 15.6% grassed. Comparatively, the proposed development would involve only 51.1% of built form and hardstanding, which would result in almost half the site 'greened' over and free from structures and hard surfacing which would enhance and moreover 'open up' areas of the Green Belt.
3. In terms of built form there would be a volume increase of 7.7% comparative to the existing buildings on site. Local Policy guidance states that up to a third increase may be accepted, although each development is assessed on its own merits. In this case, 7.7% is significantly below guidance. According to case law in the Court of Appeal judgement *Turner v Secretary of State* 2016 at para 14, *"The concept of openness of the green belt is not narrowly limited to volumetric approach..... (in the context of which, volumetric matters may be a material concern, but are by no means the only one) and factors relevant to the visual impact on the aspects which the Green Belt presents."*
4. The demolition of the dilapidated and run-down agricultural buildings and sheds together with the removal of the existing containers would result in a considerable improvement to the visual amenity of the area and in turn, enrich the special character of the Green Belt.
5. Views from Hollymount Lane southwards through the site would be retained and more so, views would be opened up in the middle areas of the site by the removal of the existing concrete and steel structured barns currently located centrally within the site, thereby giving a sense of space and openness between the areas of built development.
6. A SuDS pond would be incorporated into the southern area of the site which would significantly improve drainage of the site and the area and which would be a valuable contribution to sustainable development.
7. Significant ecological enhancements and landscaping of the site.
8. The development proposes a range of access improvements along Hollymount Lane to provide 2 passing spaces at the entrance to Hollymount Lane and approximately halfway to the development site. This would be of particularly benefit when the lane is more heavily trafficked at peak school pick up/drop off's times and generally for residents who live at the upper end of Hollymount Lane.

As a holistic development, it is considered that the applicant has put forward a compelling and rational case, and there can be no doubting the significant benefit which would be gained by the relinquishing of developed land to 'open' Green Belt land together with the

extensive landscaping, ecological enhancement and SuDS pond which would be incorporated within the scheme.

As is clear from the Turner case, the characteristics of a Green Belt site can be wide and varied and this must be taken into consideration when a site's lawful use, operations, structures and other built form are considered. The proposals in part have to be assessed as appropriate development as set out above in this report. The two additional dwellings, removal of excessive hardstand, land use (deliveries, stacking of materials and other paraphernalia, farm shop and associated visitors) all play a part in the consideration of a justification against which VSC have been put forward.

It cannot be sustainably argued that the development requiring VSC would have a greater impact than the current, as much of the land (circa half of the site) would be given over to Green Belt and ecology. The siting of the new dwellings are carefully considered such that these would be seen against the backdrop of the other houses forming part of this application and that the hedging which is substantial, would be retained. It is considered that these factors would substantially and categorically reduce the expanse of unsightly hardstanding areas and in terms of the overall development would add great weight to the applicant's case.

Additional benefits and betterments would also be achieved by the two new lay-by's which would be provided at the lower end of Hollymount Lane which would ease the pressure of traffic at busy times.

In terms of the design and layout, the approach has been to emulate the existing farm/cottage style settlement and sensitively designed using materials local to the area to deliver a scheme which would be in keeping and integrate well within the rural setting. Car ports, which are less urban in character than garages would be incorporated and turning areas for vehicles to move within the site would be minimal and only as much as be required for the scale of the scheme.

Permitted development rights would need to be removed to ensure that domestic type structures and paraphernalia would ensure there would be controls to retain the open areas of Green Belt within the 'developed' site.

In terms of para 138 of the NPPF and the 5 purposes of Green Belt, to a) to check the unrestricted sprawl of large built up areas; b) to prevent neighbouring towns merging into one another; c) to assist in safeguarding the countryside from encroachment; d) to preserve the setting and special character of historic towns; and e) to assist in urban regeneration, by encouraging the recycling of derelict and other urban land, the proposed development would not result in any harm to any of these purposes.

Collectively, and in totality, it is considered that Very Special Circumstances have been demonstrated which would outweigh any harm which would be caused to the Green Belt and that a well-presented, well thought out quality scheme which would bring about undeniable benefits and improvements to the area in terms of retaining and improving the openness of Green Belt land, would be acceptable and comply with UDP Policies OL1/2, OL1/4, EN9/1 and the principles of the NPPF.

Housing policies

UDP Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of

neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

Housing principle

Whilst the site itself is semi rural, it is located close to an established residential area and urban fringe location. There has also been residential development further along the lane to the north of the site which created a small hamlet.

There is existing infrastructure in place to facilitate the scale of development proposed and the proposed layout demonstrates that the site could satisfactorily accommodate the development in relation to the surrounding area.

The proposed development would also introduce mitigation measures and betterments to the immediate area with the provision of a SuDs pond and 2 additional vehicular passing places.

As such, the principle is considered to be acceptable and in compliance with the NPPF and UDP Policies H1/2, H2/1 and H2/2.

Scale and layout - Apart from the existing farmhouse and barn, all other structures would be removed from the site and 3 dwellings comprising the existing farmhouse, barn conversion and replacement house would be located in a row. A wide gap would be formed close to the site entrance and the 2 new dwellings would be located along the Hollymount lane frontage, set from and partly screened by the existing hedgerow.

Other than a turning area to the rear of the Type 3 properties, the southern part of the site would be fully grassed over and to provide gardens which would be separated by natural stone walls which are commonly found to field boundaries within Bury's rural and countryside settings.

Beyond the gardens and the 'development' site and separated by a native boundary hedge would be an ecology and SuDs area which would remain free of any built development.

As described in some detail above, the proposed development and site layout would be the best solution to achieve optimal openness of the Green Belt.

The existing access to the site would be utilised and the hardstanding internal drive retained to serve the row of 3 houses. Carports to serve these properties would be located close to the northern boundary but well set back from Hollymount Road, screened by a high embankment of trees.

The scale of development has been reduced from the original submission for a total of 6 dwellings and it is considered the scale of the development now proposed for 5 dwellings in the configuration and arrangement shown would be maximise the amount of land which would be 'open' Green Belt land.

It is therefore considered the form, scale and layout proposal would be acceptable and comply with EN1/2, H2/1, H2/2 and the principles of the NPPF.

Design and appearance - The design ethos has been driven by the style and appearance of the existing farmhouse dwelling and associated stone barns within this semi-rural and agrarian setting and with this in mind, has been the approach adopted to delivering the new dwellings.

The dwellings would be built from reclaimed and natural dressed stone with natural grey slate roofs. The elevations would incorporate randomly placed 'cottage style' windows with

traditional stone headers and cills and quoin detailing. There would be elements of modern interventions with some dwellings incorporating full height glazed windows but these have been sensitively designed to emulate barn type openings which would be typically found within Green Belt settings and similar settlements and as such are considered to be acceptable and in keeping with the character of the surrounding area.

Dark window frames and rainwater goods would introduce subtle detailing to the fenestrations and would blend within the overall appearance of the dwellings.

Car ports which are less urbanising and domestic in design comparative to garages are proposed and would contribute to the overall feeling of the openness of the site. The car ports to Type 3 dwellings would have a timber rear elevation facing Hollymount Lane to provide a secure storage facility and given the lightweight appearance the design is considered acceptable.

Natural stone retaining walls would be erected between properties which would imitate the low dry stone walls found in rural settings and again would emulate a feeling of openness within the site.

It is therefore considered a sensitive and considered approach has been taken to not only the design of the dwellings but how the development would integrate within the landscape and Green Belt setting.

The proposed development would therefore comply with Policies H2/2, EN1/2, EN9/1, OL5/2 and the principles of the NPPF.

Highways issues

The development would be served by Hollymount Lane which is a single width road. There are a couple of existing passing places along the route, one located more or less opposite the site entrance. At certain times of the day, the lane can be fairly heavily trafficked serving the morning and afternoon school peak drop off and collection times, as well as being used by the golf club, residents and of course historically, former farm and business traffic.

In terms of net gain to the site, there would be an additional 3 dwellings on the site and whilst the scale of development would not add significantly to the traffic along the lane, to introduce a betterment to the existing situation the applicant proposes to provide 2 new passing places, one at the entry point to Hollymount Lane and the other located approximately halfway to the site entrance.

The applicant has confirmed their ownership of the lower part of Hollymount Lane which is shown within the red edge application site and therefore the passing places are within their 'gift' to provide.

As such, the development would improve traffic flows along Hollymount Lane to a considerable degree.

The new passing places would require the removal of part of the hedgerow which would be replaced within the new bays and therefore would be acceptable.

The Public Right of Way which runs separately to the road would remain unaffected.

Access points into and out of the site would incorporate adequate visibility splays and turning areas for vehicles within the site would allow cars to exit the site in a forward gear. The existing gated field access to the south of the development off Hollymount Lane would be would be retained to allow access to the field.

In terms of parking, the scheme would provide car ports to each property plus additional parking facilities for occupiers and visitors.

The Highway's Section welcome the improvements and betterments which would be

provided on Hollymount Lane and have raised no objection to the proposed access and parking provision and as such the proposed scheme is considered to be acceptable and in compliance with H2/2, HT2/4 and HT6/2.

Ecology

Summary

Issues relating to bats, nesting birds and biodiversity enhancements can be dealt with via condition.

Bats

An updated and valid bat report has been provided. This has found no change in circumstances since the previous surveys ie all outbuildings have negligible potential with the main farmhouse still high risk and still probably utilised as a day roost.

No emergence surveys have been provided as no significant disturbance is planned to the farm house roof, with development restricted to demolition of existing single storey extensions with negligible potential and a new extension that will not impact on the roof. It has therefore concluded that reasonable avoidance measures as previously agreed as part of the previous proposal are still valid. GMEU have no reason not to disagree with this proposal and recommend a condition that the recommendations contained in the Preliminary Bat Roost Assessment at Holly Mount Farm Ecology Solutions, Appendix 1 Section 5 shall be carried out in full.

Nesting Birds

Swallow nests were previously found in one of the outbuildings. All British birds nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife & Countryside Act 1981, as amended.

A condition would be applied to restrict building works including demolition between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the LPA.

Contributing to and Enhancing the Natural Environment

Section 174 of the NPPF 2021 states that the planning policies and decisions should contribute to and enhance the natural and local environment. The development would have little impact on vegetated habitats and the development would deliver significantly more soft landscaping than is currently present on the site. In terms of species mitigation and enhancement, measures for bats and nesting birds are recommended.

GMEU have no objection therefore to the conditioning of a landscape plan and recommend the condition also include the requirement for bat and bird boxes, bricks or slates on the new build.

Drainage - For a number of years, Hollymount Lane and more so at the lower end, experiences periods of flooding when the weather is particularly wet and this becomes a problem for those accessing the lane.

The development proposes to incorporate a SuDS pond in the southern part of the site which would considerably improve the drainage in the area which would be a welcomed and beneficial addition to the development. As noted above, a large part of the 'development site' (31.7%) in addition to the SuDS and ecology area would be landscaped which would also be of great value in terms of contributing to sustainable development, improving rain collection and on site retention of water through the removal of hardstanding.

A sustainable drainage strategy has been submitted with the application which United Utilities have confirmed is acceptable in principle. A condition for the submission of details of the scheme and maintenance where necessary would be secured by condition.

It is therefore considered that the proposed drainage arrangements for the site would be acceptable and crucially would improve and bring a betterment to the area by the incorporation of the SuDS pond and extensive landscaped areas.

The proposed development would therefore comply with the principles of the NPPF.

Response to objectors

- The proposed development would be contained within the existing site area and would not encroach any further into the Green Belt than already exists. Indeed, the existing 'developed' area would be reduced in size by returning the southern part of the site to an ecology and SuDS area of open greened land.
- The applicant has confirmed the part of the lane shown in the red edge is within the applicant's ownership. Private rights of access along the lane would be a private matter.
- The provision of the two passing places would improve traffic flows along Hollymount Lane particularly at busy peak periods and the net addition of 3 dwellings would not significantly add to the traffic in the area.
- Hedgerow to be removed along the land to facilitate the new passing places would be replaced within the new bays and be conditioned accordingly.
- Any future applications for the site or the surrounding area would be assessed on their own merits.
- The current site has little in the way ecological value, comprising of buildings and hardstanding and minimal greened areas. Existing trees on site would be retained and therefore unaffected. A bat survey has been carried out on the buildings to be demolished and identified negligible potential for bat roosts and ecological enhancements on site are proposed and GMEU have raised no issues.
- United Utilities have accepted the drainage proposals in principle.
- The incorporation of a SuDS pond would improve drainage of the area.
- Hollymount Orchard is more than 300m away to the north west of the site and would be completely unaffected by the proposals.
- The containers on site have not been included within the volumetric calculations when comparing the volumes of the existing buildings comparative to the proposed built development.
- The scale of development would not trigger Affordable Housing obligations.
- All other issues raised have been covered in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered -

Job No. 108 - dgs numbered:
01 Location plan rev F

L04 Constraints plan rev E
L14 Proposed site plan rev A
L15 Proposed access plan rev A
L 16 Proposed site areas rev A
L 17 Proposed site plan
L18 Proposed level details

P02 Proposed barn conversion plans rev C
P03 Proposed farmhouse extension rev C
P08 New Byre plans (Type 4) rev A
P09 New farmhouse plans (Type 3)

E03 Barn conversion elevation 1 of 2 rev A
E04 barn conversion elevation 2 of 2 rev A
E05 Farmhouse extension elevation 1 of 2 rev A
E06 Farmhouse extension elevation 2 of 2 rev A

E17 New Byre (Type 4) elevations 1 of 2 rev A
E18 New Byre (Type 4) elevation 2 of 2 rev A
E19 New Byre (Type 4) section rev A
E20 New car port type 1 and 4 rev B
E21 New farmhouse (Type 3) elevations 1 of 2
E22 New farmhouse (Type 3) elevations 2 of 2

Preliminary Bat Roost Assessment by Ecologysolutions
Sustainable Drainage Strategy FRA22 1030 dated April 2022
Preliminary Risk Assessment

and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence unless and until:-

- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural

environment.

5. Prior to the commencement of development, details of a sustainable surface water drainage scheme and a foul water drainage scheme shall be submitted to and approved in writing by the Local Planning Authority. The drainage schemes must include:
 - (i) An investigation of the hierarchy of drainage options in the National Planning Practice Guidance (or any subsequent amendment thereof). This investigation shall include evidence of an assessment of ground conditions and the potential for infiltration of surface water in accordance with BRE365;
 - (ii) A restricted rate of discharge of surface water agreed with the local planning authority (if it is agreed that infiltration is discounted by the investigations);
 - (iii) Levels of the proposed drainage systems including proposed ground and finished floor levels in AOD;
 - (iv) Incorporate mitigation measures to manage the risk of sewer surcharge where applicable; and
 - (v) Foul and surface water shall drain on separate systems.
 - (vi) Prior to occupation of the proposed development, the drainage scheme shall be implemented in accordance with the approved details, made available for use to the development and be retained thereafter for the lifetime of the development.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
6. The recommendations contained in the Preliminary Bat Roost Assessment at Holly Mount Farm Ecology Solutions, Appendix 1 Section 5 shall be carried out in full.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
7. No building works including demolition shall commence between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
8. The development hereby approved shall be carried out in accordance with the submitted landscape plan dwg YD2_HMF_LP001 Revision A.

The soft landscaping scheme including the SuDs pond shall be implemented in accordance with a schedule of implementation which shall be submitted to and approved in writing by the Local Planning Authority prior to any commencement on site.

The landscaping details shall be implemented in accordance with the approved schedule and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted. Each new dwelling should also provide bat/bird boxes, bricks or slates within the build and shall thereafter be maintained.

Reason. To secure the satisfactory development of the site and in the interests of

visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

9. The replacement hedgerow to the two new passing places hereby approved shall be implemented (in accordance with the details for native hedge planting contained within the landscaping condition 8) following the completion of the passing places and in any event, prior to the first occupation of the new dwellings hereby approved. The hedgerows shall be of a native species, of a similar height and density and shall thereafter be maintained.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

10. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:

- Photographic dilapidation survey of Holly Mount Lane (Public Footpath No. 26, Tottington) leading to and abutting the site in the event that subsequent remedial works are required following construction of the development and as a result of statutory undertakers connections to the site;
- Access point(s) for demolition/construction traffic from Holly Mount Lane (Public Footpath No. 26, Tottington) and all temporary works required to facilitate access for ground works/construction vehicles, including the provision (to basecourse level) of the passing places on Holly Mount Lane indicated on the approved plans (incorporating signage in accordance with Traffic Signs Regulations and General Directions 2016);
- If proposed, details of site hoarding/gate positions clear of the visibility splays onto Holly Mount Lane (Public Footpath No. 26, Tottington) indicated on the approved plans;
- The provision, where necessary, of temporary pedestrian facilities/protection measures;
- A scheme of appropriate warning/construction traffic speed signage in the vicinity of the site and its access;
- Confirmation of hours of operation (taking into consideration school start and finish times), demolition, delivery & construction vehicle sizes that can be accommodated on the Public Right of Way that serves the site and number of vehicle movements;
- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site and/or measures to control/manage delivery vehicle manoeuvres;
- Parking on site or on land within the applicant's control of operatives' and demolition/construction vehicles, together with storage on site of demolition/construction materials;
- Measures to ensure that all mud and other loose materials are not spread onto the adjacent Public Right of Way/adopted highways as a result of the groundworks operations or carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials. All highway remedial works identified as a result of the dilapidation survey shall be implemented to the written satisfaction of the Local Planning Authority prior to the development hereby approved being brought into

use.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

11. The vehicular and pedestrian access arrangements, provision of the passing places on Holly Mount Lane/Public Footpath No. 26, Tottington (incorporating signage in accordance with Traffic Signs Regulations and General Directions 2016), turning facilities and bin storage/collection arrangements for the new dwellings within the curtilage of the site indicated on the approved plans shall be provided before the development is first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.
Reason. In the interests of highway safety and to minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, HT2/4 - Car Parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.
12. The visibility splays indicated on the approved plans shall be implemented to the written satisfaction of the Local Planning Authority before the development is brought into use and subsequently maintained free of obstruction above the height of 0.6m.
Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development and HT6/2 - Pedestrian/Vehicular Conflict.
13. The car parking for the new dwellings indicated on the approved plans, shall be surfaced and made available for use to the written satisfaction of the Local Planning Authority prior to the development hereby approved being occupied and thereafter maintained at all times.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
14. Details/Samples of the materials to be used in the external elevations, and details of the surfacing materials for the hardstanding areas together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity and H2/1 - The Form of New Residential Development.
15. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 and Class A of Part 2 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.
16. The mobile home shall be removed from site prior to the first occupation of new

dwelling Type 4 hereby approved.

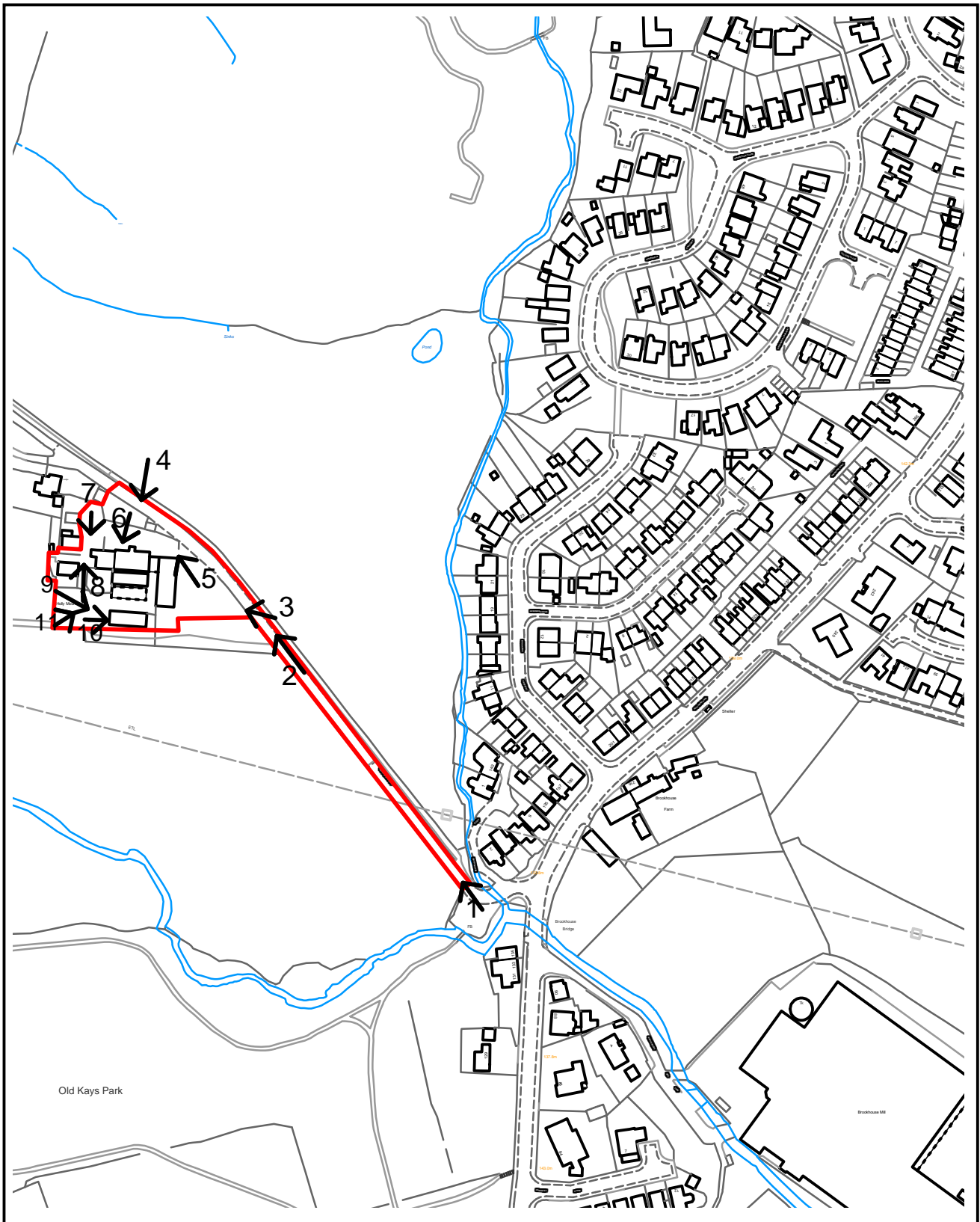
Reason. In the interests of visual amenity and to protect the openness of the Green Belt pursuant to Bury Unitary Development Plan Policies EN1/1 - Visual Amenity and OL1/2 - New Buildings in the Green Belt and chapter 13 - Protecting Green Belt land of the NPPF.

17. Prior to first occupation of the development hereby approved, a sprinkler/misting system to meet the requirements of BS 9251:2014 or other subsequent standard that meets the requirements of Greater Manchester Fire and Rescue Service and deemed suitable to overcome the site's emergency access deficiencies, shall be installed in the dwelling hereby approved and shall thereafter be maintained at all times.

Reason. In the interests of fire safety and to ensure the safe and satisfactory development of the site and for its future occupiers pursuant to Bury Unitary Development Plan Policies H1/2 - Further Housing Development, H2/2 - The Layout of New Residential Development and EN1/2 - Townscape and Built Design.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 68505

ADDRESS: Hollymount Farm, Hollymount Lane
Tottington, Bury, BL8 4HP

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

68505

Photo 1



Photo 2



68505

Photo 3



Photo 4



68505

Photo 5



Photo 6



68505

Photo 7



Photo 8



68505

Photo 9



Photo 10



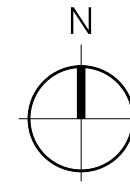
68505

Photo 11



Photo 12



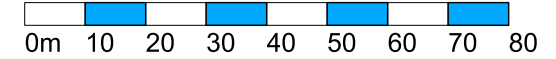


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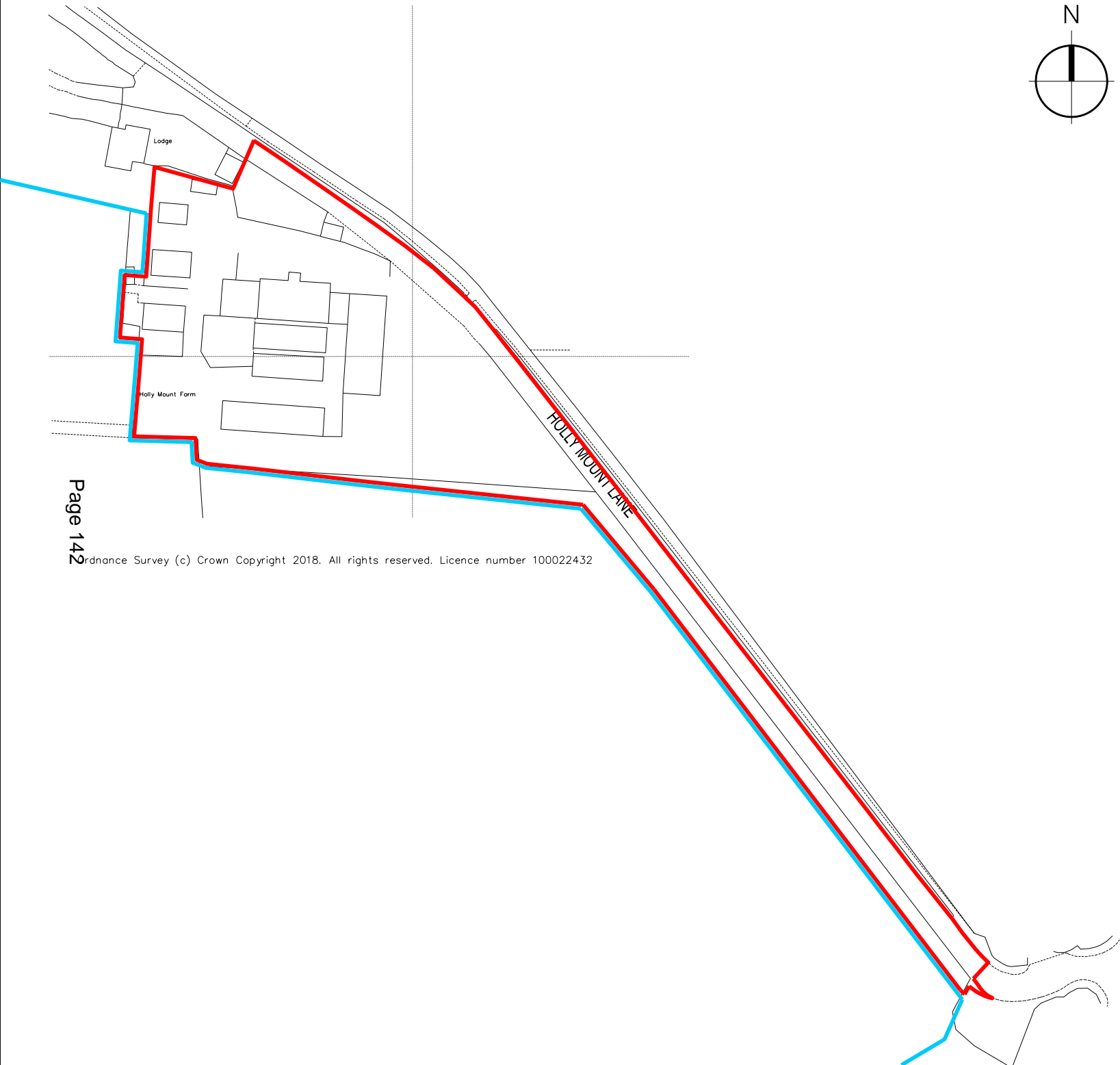
SITE SPECIFIC HAZARDS

IN ACCORDANCE WITH THE REQUIREMENTS OF THE CDM REGULATIONS 2015 THE FOLLOWING SIGNIFICANT RESIDUAL HAZARDS HAVE NOT BEEN DESIGNED OUT OF THIS PROJECT AND MUST BE TAKEN INTO CONSIDERATION BY CONTRACTORS PLANNING TO UNDERTAKE THE WORKS SHOWN ON THIS DRAWING:

SCALE 1:1250



- site boundary -area 7332 sqm
- other land in the ownership of the applicant



Page 142

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REV.F 221129	BOUNDARY UPDATED	RSL
REV.E 220426	BOUNDARY UPDATED	RSL
REV.D 220413	BOUNDARY UPDATED	RSL
REV.C 220413	BOUNDARY UPDATED	RSL

REV.	DATE	NOTES	INIT.
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CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM, TOTTINGTON
-

DRAWING TITLE
SITE LOCATION PLAN

STATUS
PLANNING

DATE	DRAWN	SCALE @ A4
SEPT 21		1:1250

PROJECT NUMBER	PLAN/ BUILD	TYPE	NUMBER	REVISION LETTER
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DRAWING NO.	108	PLN	L	01	F
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix: Colour	C

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EXISTING BUILDINGS 1048.8 SQM (19.3%)

EXISTING CONTAINERS 263.4 SQM (4.9%)

EXISTING HARDSTANDING 3267.7 SQM (60.2%)

EXISTING GRASSED AREAS 848.3 SQM (15.6%)

TOTAL 5428.2 SQM

REV. A 220728 KEY UPDATED RSL
REV. DATE NOTES INT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
EXISTING SITE AREAS

STATUS
PLANNING

DATE
MAR 22

DRAWN
SSS

SCALE @ A1
1:200

PROJECT
NUMBER

PLAN /
BUILD

TYPE

NUMBER

REVISION
LETTER

DRAWING NO.
107

PLN L 05 A

Site Location Plans L GA Plans P Elevations E
Sections S Details D Protic Colour C

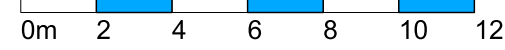
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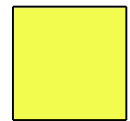
SCALE 1:200



N



KEY



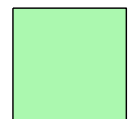
5 NO DWELLINGS 739 SQM (1 EXISTING) 13.702% OF TOTAL



TERRACOTTA SUDS PAVEMENTS HARD STANDING 489.1 SQM 9.07% OF TOTAL



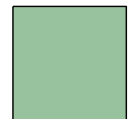
NATURAL STONE FLAGS inc. BUFF PAVIOR BORDER & STEPS HARD STANDING 954 SQM 17.52% OF TOTAL



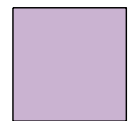
CAR PORTS 185.3 SQM 3.44% OF TOTAL



LANDSCAPED AREA 1705.6 SQM 31.624% OF TOTAL



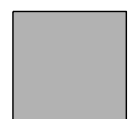
ECOLOGY / SUDS AREA 931.1 SQM 17.264% OF TOTAL



BIN/ CYCLE/ GARDEN STORES 65.3 SQM 1.211% OF TOTAL



TARMAC ACCESS ROAD 234 SQM 4.34% OF TOTAL



TARMAC AREA INC. PAVEMENT 89.8 SQM 1.67% OF TOTAL

TOTAL 5393.3 SQM PLUS GARDEN WALLS & FENCES (BELOW 2M)

DEVELOPED AREA 2756.6 SQM 51.11% OF TOTAL



REV. A 22/11/20 CAR PORT TYPE NOTE CORRECTED RSL
REV. DATE NOTES WIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED SITE AREAS
OPTION 3

STATUS
PLANNING

DATE NOV 22 DRAWN RSL SCALE @ A1 1:200

PROJECT NUMBER PLAN / BUILD TYPE NUMBER REVISION LETTER

DRAWING NO.

108 PLN L 16 A
Site Location Plans L GA Plans P Elevations E
Sections S Details D Profs. Colour C

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SITE SPECIFIC FACTORS

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SCALE 1:500

0m 5 10 15 20 25 30

LEGEND

- ADJACENT DWELLINGS
- EXISTING DWELLING
- EXISTING FARMHOUSE
- EXISTING STONE BARN
- EXISTING CONCRETE BLOCK AND STEEL BARN
- EXISTING STORAGE UNITS
- EXISTING FREEZER UNIT
- EXISTING TREES
- SITE BOUNDARY
- OTHER LAND IN APPLICANTS OWNERSHIP
- 26 TOT PROW

REVISIONS

NO.	DATE	REVISION	BY
01	22/04/22	ISSUED FOR PERMIT	RS
02	22/04/22	PROJECT MODIFIED	RS
03	22/04/22	PROJECT MODIFIED	RS
04	22/04/22	PROJECT MODIFIED	RS
05	22/04/22	PROJECT MODIFIED	RS

CLIENT
BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
SITE CONSTRAINTS

STATUS
PLANNING

DATE
APR 22

SCALE
1:500

PROJECT
108 PLN L 04 E

REVISIONS

NO.	DATE	REVISION	BY
01	22/04/22	ISSUED FOR PERMIT	RS
02	22/04/22	PROJECT MODIFIED	RS
03	22/04/22	PROJECT MODIFIED	RS
04	22/04/22	PROJECT MODIFIED	RS
05	22/04/22	PROJECT MODIFIED	RS

HEALTHCARE PARTNERSHIP
HEALTHCARE PARTNERSHIP LTD
108 PLN L 04 E
T. 01203 888 444

CONTACT
C. 01203 888 444
www.healthcarepartnership.co.uk

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SCALE 1:200
0m 2 4 6 8 10 12

STRUCTURE REMOVED
STRUCTURE RETAINED



REV. DATE NOTES INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED SITE PLAN
OVERLAID ONTO EXISTING
option 3

STATUS
PLANNING

DATE NOV 22 DRAWN RSL SCALE @ A1 1:200

PROJECT NUMBER PLAN / BUILD TYPE NUMBER REVISION LETTER

DRAWING NO.

108 PLN L 17 -

Site Location Plans L GA Plans P Elevations E
Sections S Details D Profs Colour C

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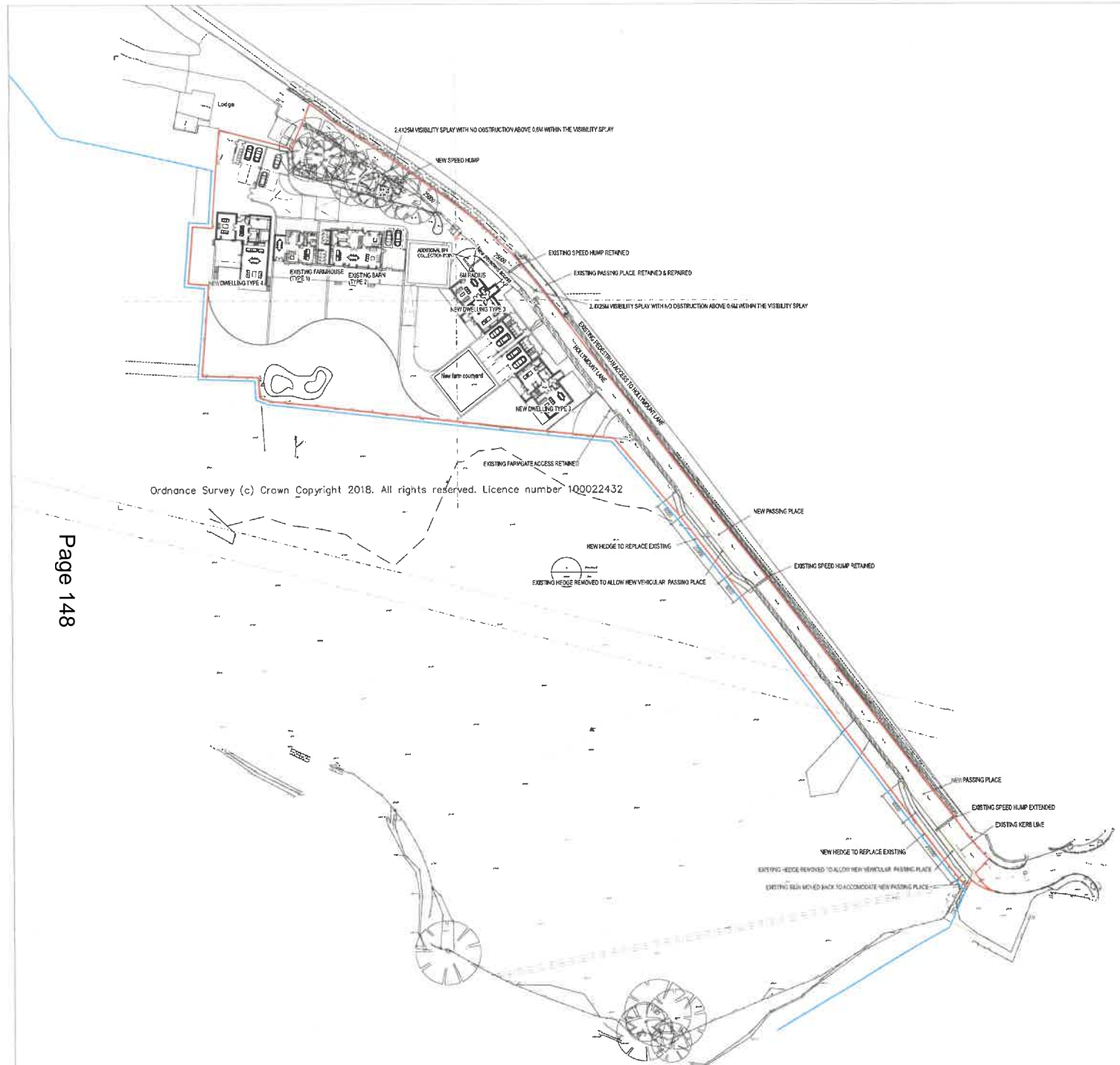
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SCALE 1:500
0m 5 10 15 20 25 30



REV: 22/11/18 14/11/2018 UPDATED RSL

CLIENT PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

PROPOSED ACCESS PLAN
option 3

STATUS

PLANNING

DATE NOV 22 2018 RSL SCALE 1:500

PROJECT 108 PLN L 15 A

REVISIONS

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SCALE 1:100



KEY

- STRUCTURE REMOVED
- RECLAIMED STONE
- NATURAL SLATE
- NATURAL DRESSED STONE
- DRY STONE WALL



NORTH (FRONT) ELEVATION



EAST (SIDE) ELEVATION

REV. A	22/10/12	REVISED TYPE 9 ADDED	RSL
REV.	DATE	NOTES	INIT.

CLIENT / PROJECT
J. BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED FARMHOUSE (TYPE 1)
EXTENSION ELEVATIONS 1 OF 2

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
APR 22	RSL	1:100
PROJECT NUMBER	PLAN / BUILD	TYPE
108	PLN	E
DRAWING NO.	NUMBER	REVISION LETTER
05	A	
Site Location Plans	L	GA Plans
Sections	S	Details
	P	Elevations
	D	Prefix: Colour
		C

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SITE SPECIFIC HAZARDS

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SCALE 1:100

0m 1 2 3 4 5 6

KEY

STRUCTURE REMOVED

RECLAIMED STONE

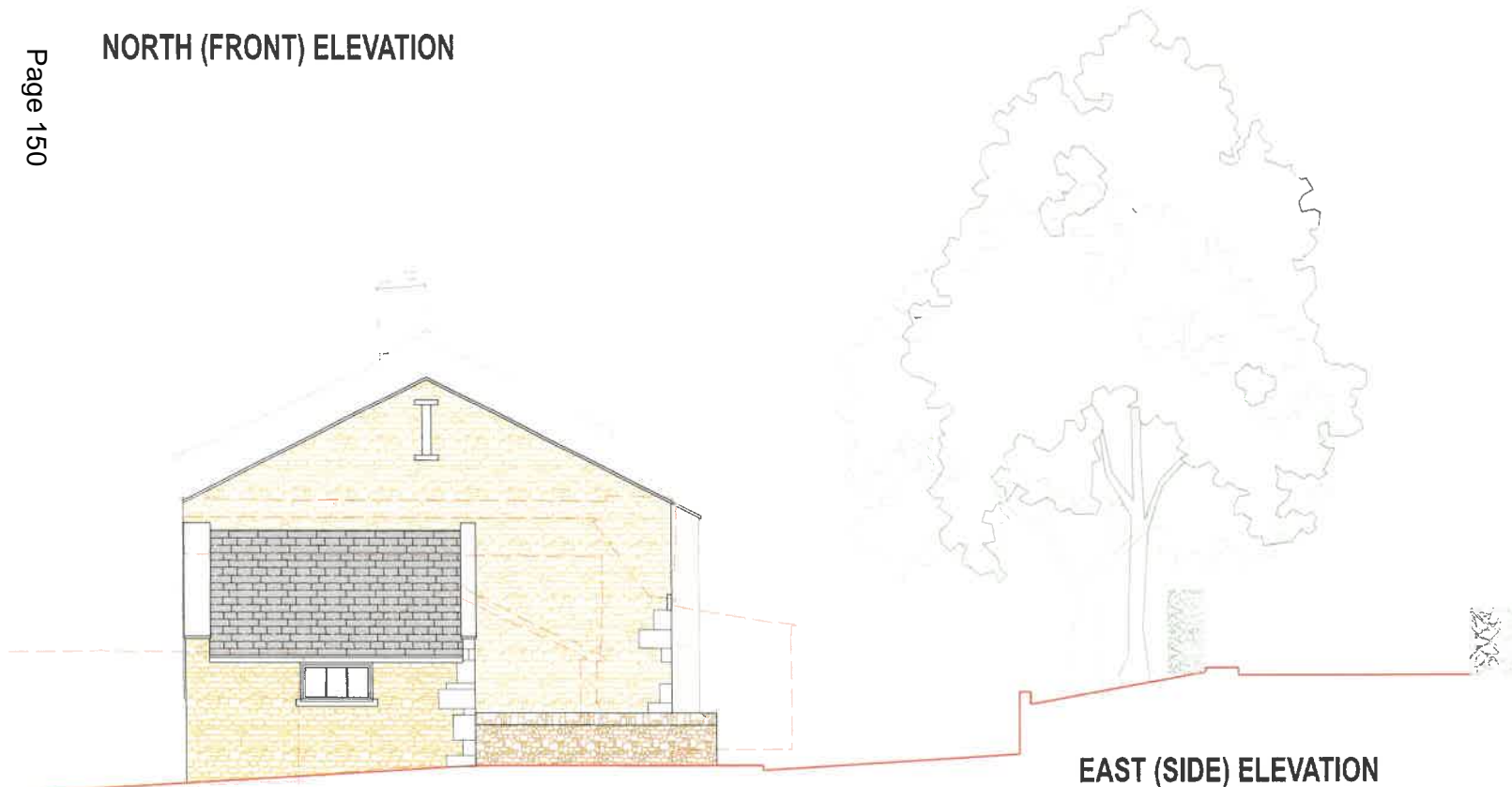
NATURAL SLATE

NATURAL DRESSED STONE

DRY STONE WALL

NORTH (FRONT) ELEVATION

Page 150



EAST (SIDE) ELEVATION

REV. A 221012 GARAGE OMITTED, CAR PORT ADDED RSL

REV. DATE NOTES INIT.

CLIENT / PROJECT

J. BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE

PROPOSED BARN CONVERSION
(TYPE 2) ELEVATIONS 1 OF 2

STATUS PLANNING

DATE APR 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
108	PLN	E	03	A

DRAWING NO.

Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Profile, Colour	C

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SCALE 1:100



- STRUCTURE REMOVED
- EXISTING STRUCTURE
- NEW STRUCTURE

FLOOR AREA 153 SQM (1647 SQFT) GIA
PLUS CAR PORT

REV.C 2210114 SITE LAYOUT UPDATED
REV.B 220522 PLANNING ISSUE
REV.A 220331 UPDATED TO CLIENT COMMENTS

REV. DATE NOTES INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED FARMHOUSE PLANS
TYPE 1

STATUS
PLANNING

DATE MAR 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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107	PLN	P	03	C
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix: Colour	C

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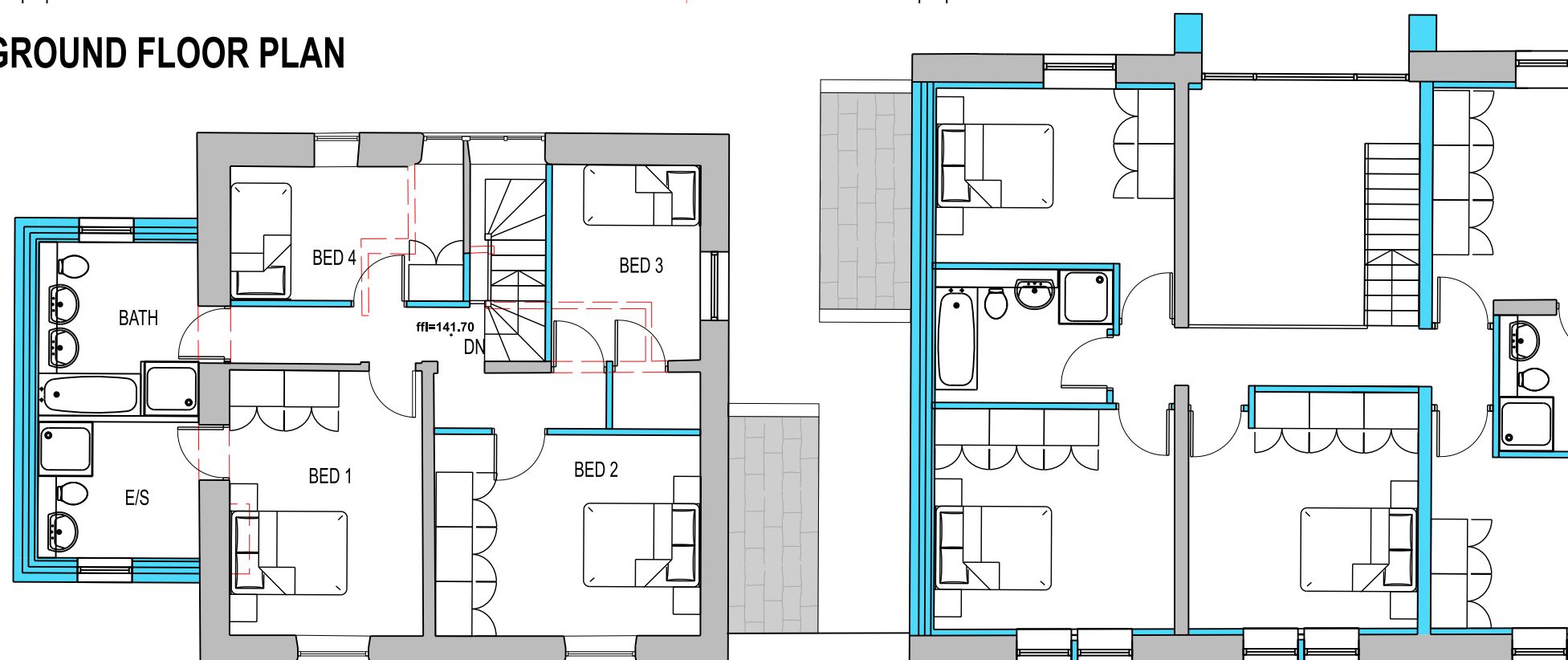
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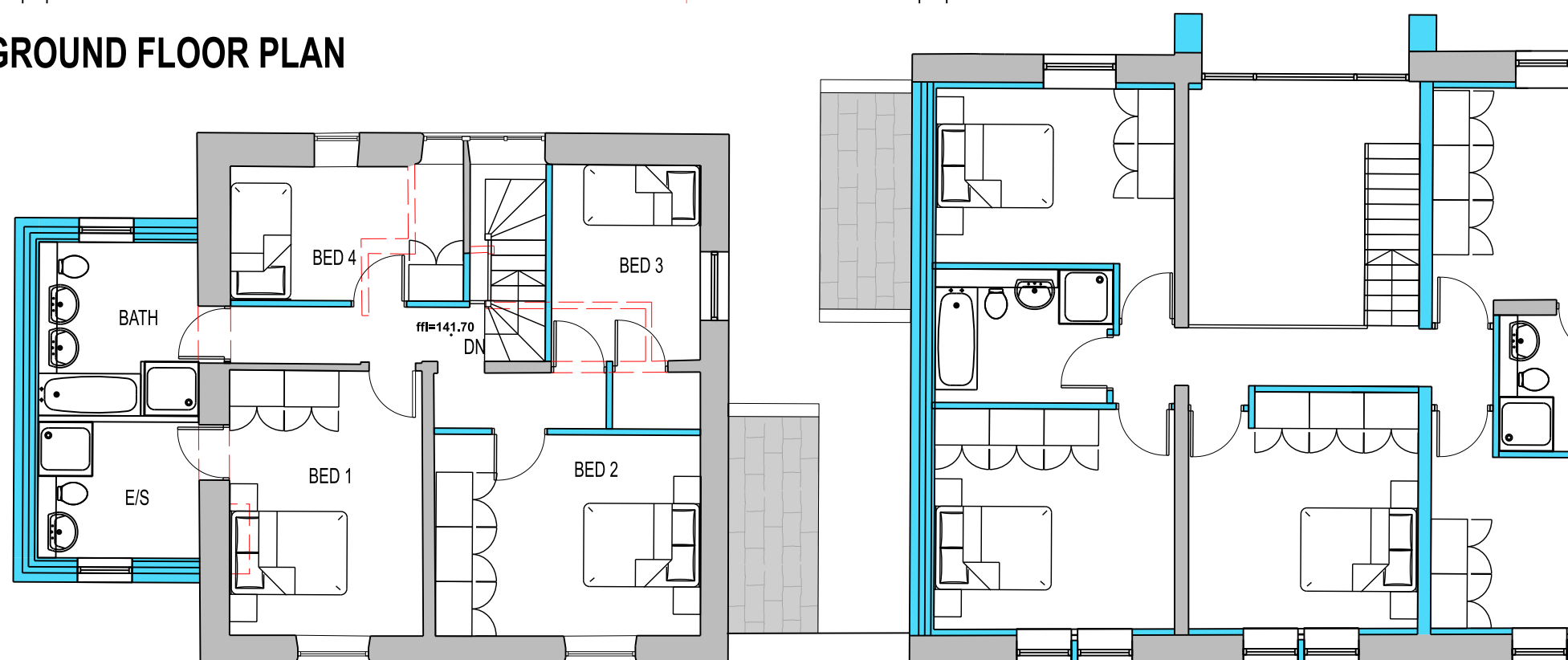
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PROPOSED GROUND FLOOR PLAN



PROPOSED FIRST FLOOR PLAN





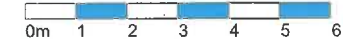
SOUTH (REAR) ELEVATION

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SCALE 1:100



KEY

STRUCTURE REMOVED

RECLAIMED STONE

NATURAL SLATE

NATURAL DRESSED STONE

DRY STONE WALL



WEST (SIDE) ELEVATION

REV. A 221014 PROJECT NO AMENDED RSL
REV. DATE NOTES INIT.

CLIENT / PROJECT
J. BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED BARN CONVERSION
(TYPE 2) ELEVATIONS 2 OF 2

STATUS
PLANNING

DATE APR 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER PLAN / BUILD TYPE NUMBER REVISION LETTER

DRAWING NO. 107 PLN E 04 A

Site Location Plans L GA Plans P Elevations E
Sections S Details D Profits: Colour C

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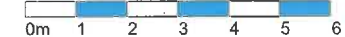
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SITE SPECIFIC HAZARDS

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SCALE 1:100



KEY

- STRUCTURE REMOVED
- RECLAIMED STONE
- NATURAL SLATE
- NATURAL DRESSED STONE
- DRY STONE WALL



WEST (SIDE) ELEVATION

REV. A	221014	PROJECT NO AMENDED	RSL
REV.	DATE	NOTES	INIT.

CLIENT / PROJECT
J. BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED BARN CONVERSION
(TYPE 2) ELEVATIONS 2 OF 2

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
APR 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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DRAWING NO.	107	PLN	E	04	A
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Profile Colour	C

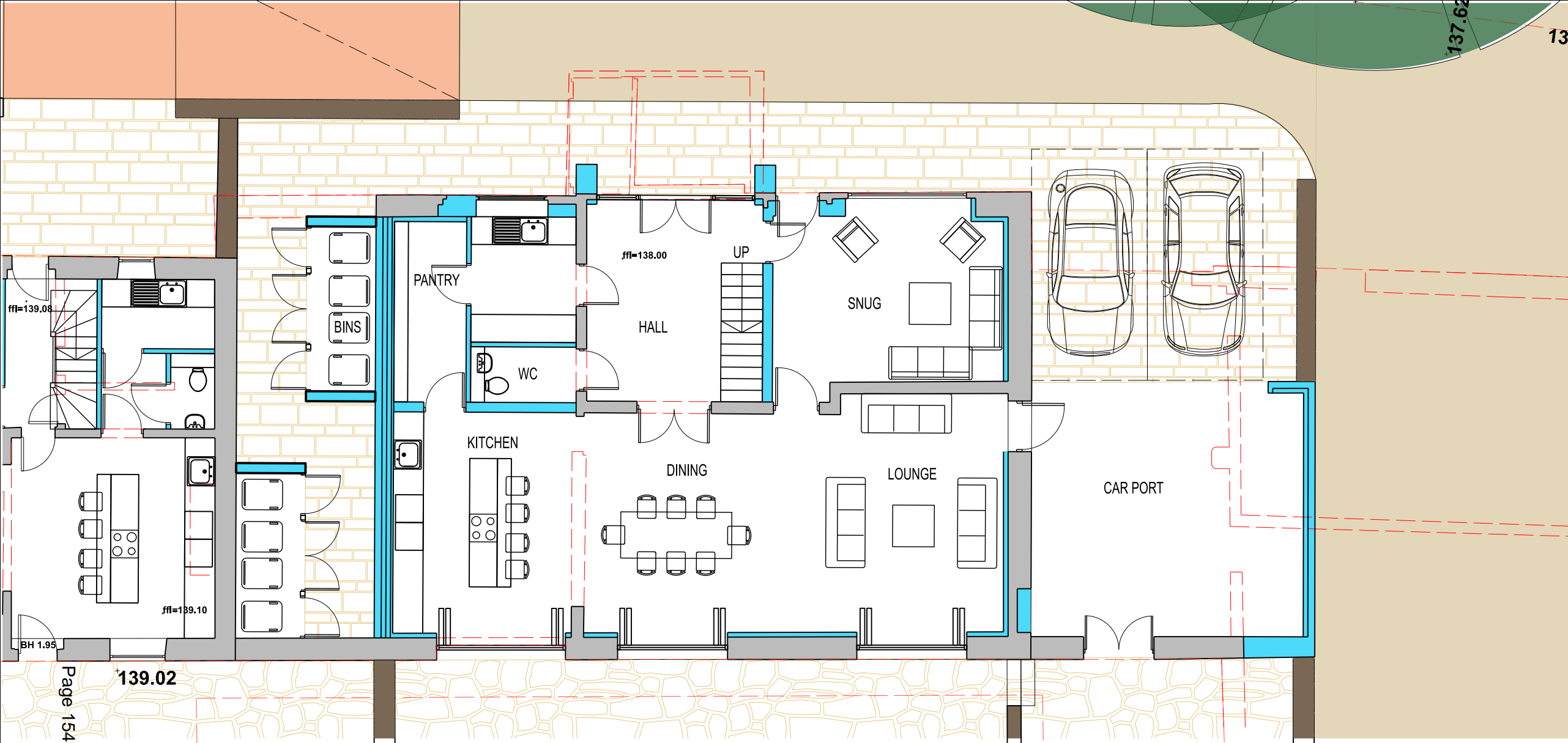
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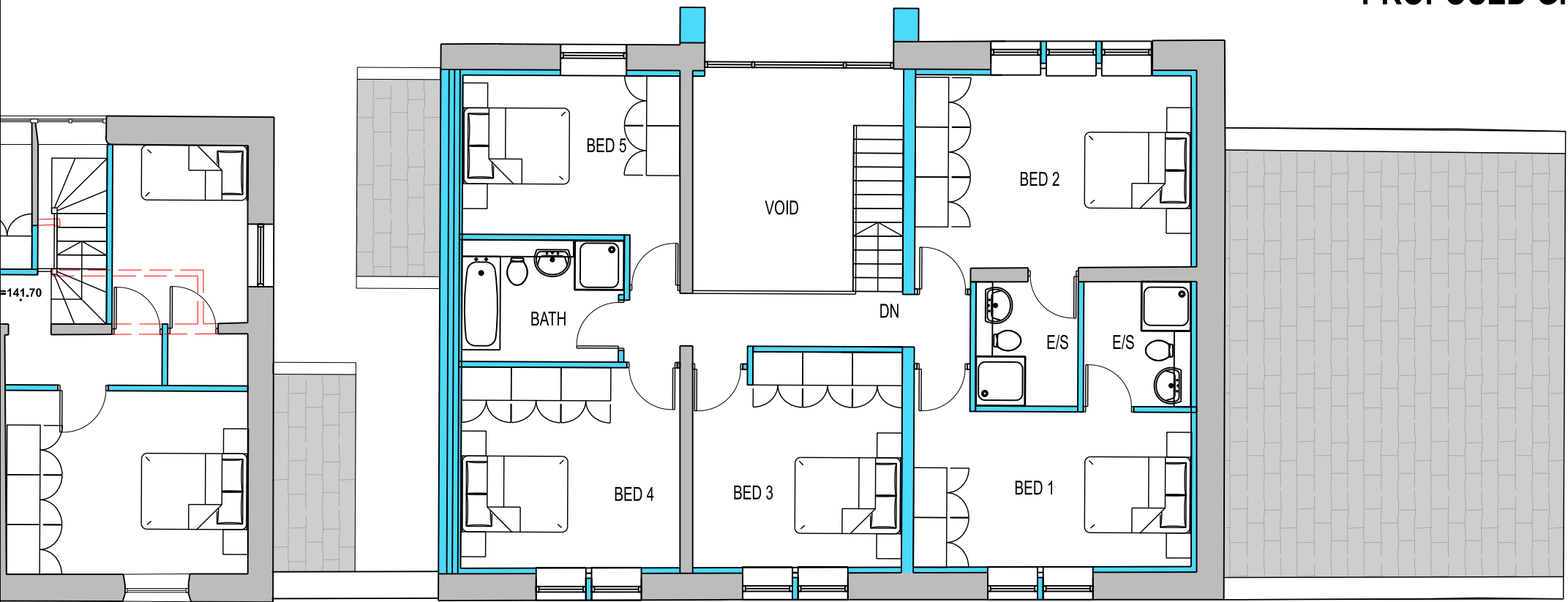
T. 08453 900 444

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www.hawthorn-estates.co.uk





PROPOSED GROUND FLOOR PLAN



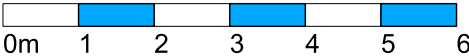
PROPOSED FIRST FLOOR PLAN

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SCALE 1:100



- STRUCTURE REMOVED
- EXISTING STRUCTURE
- NEW STRUCTURE

FLOOR AREA 224.4 SQM (2415 SQFT) GIA
PLUS CAR PORT

REV.C 221014	SITE LAYOUT UPDATED	RSL
REV.B 220331	PLANNING ISSUE	RSL
REV.A 220522	UPDATED TO CLIENT COMMENTS	RSL
REV. DATE	NOTES	INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED BARN PLANS
TYPE 2

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
MAR 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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DRAWING NO.	107	PLN	P	02	C
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix: Colour	C

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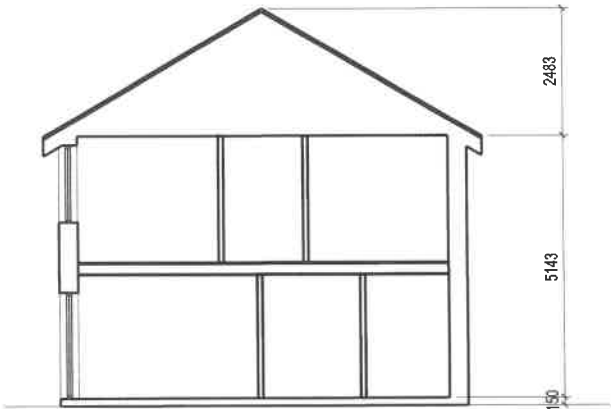


NORTH EAST (FRONT) ELEVATION



Timber framed open sided with stone dwarf wall, car port

SIDE ELEVATION



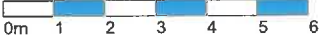
SECTION

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SCALE 1:100



KEY

- NATURAL SPLIT FACED STONE
- NATURAL SLATE
- NATURAL DRESSED STONE
- DRY STONE WALL

REV. DATE NOTES INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW FARMHOUSE
TYPE 3 ELEVATIONS 1 OF 2

STATUS
PLANNING

DATE NOV 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
108	PLN	E	21	-

DRAWING NO.

Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Profile Colour	C

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SOUTH WEST(REAR) ELEVATION



SIDE ELEVATION

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SCALE 1:100



KEY

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-  NATURAL SLATE
-  NATURAL DRESSED STONE
-  DRY STONE WALL

REV.	DATE	NOTES	INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW FARMHOUSE
TYPE 3 ELEVATIONS 2 OF 2

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
NOV 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
108	PLN	E	22	-

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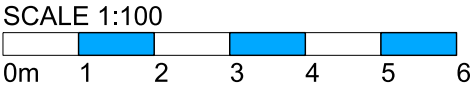
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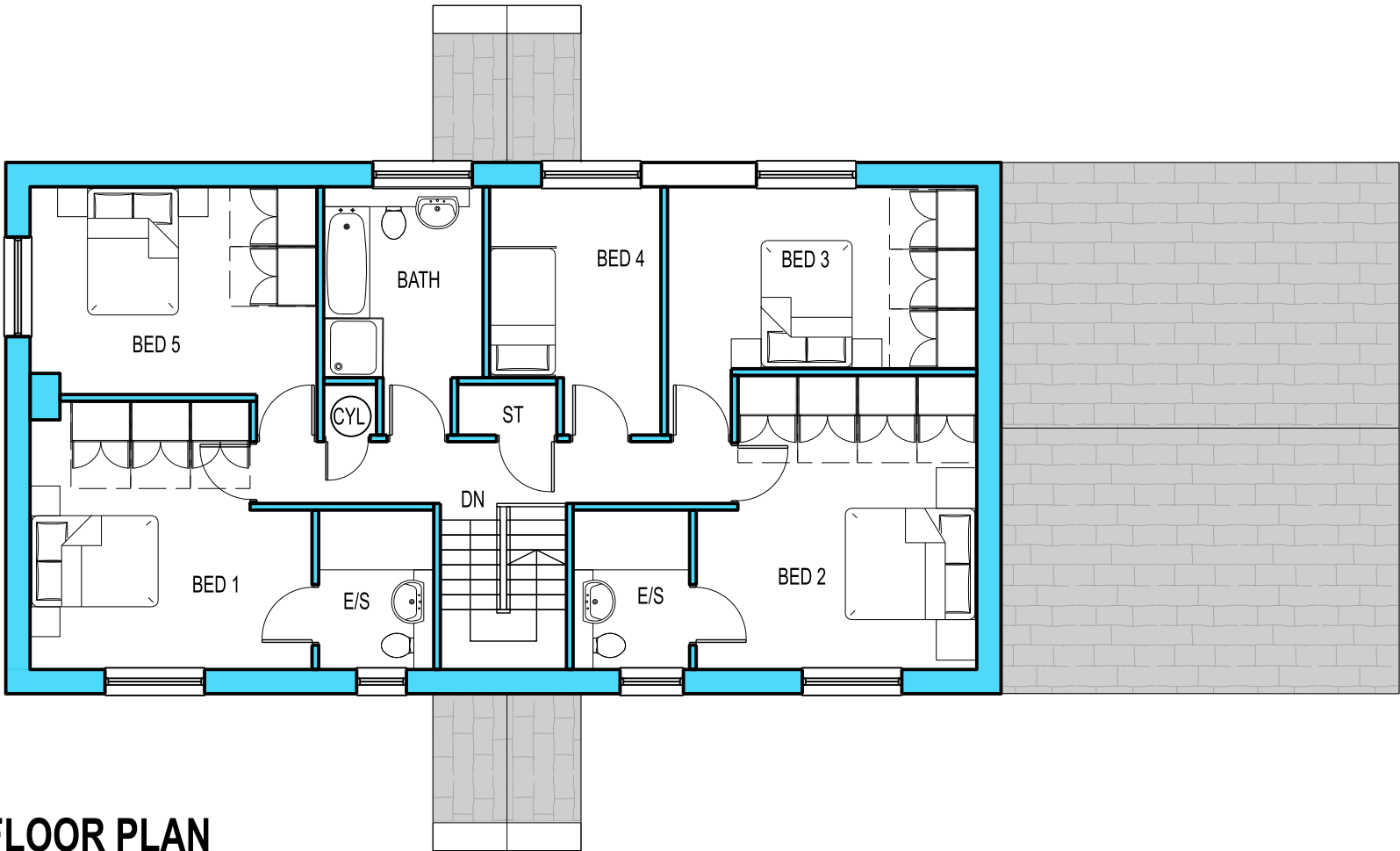
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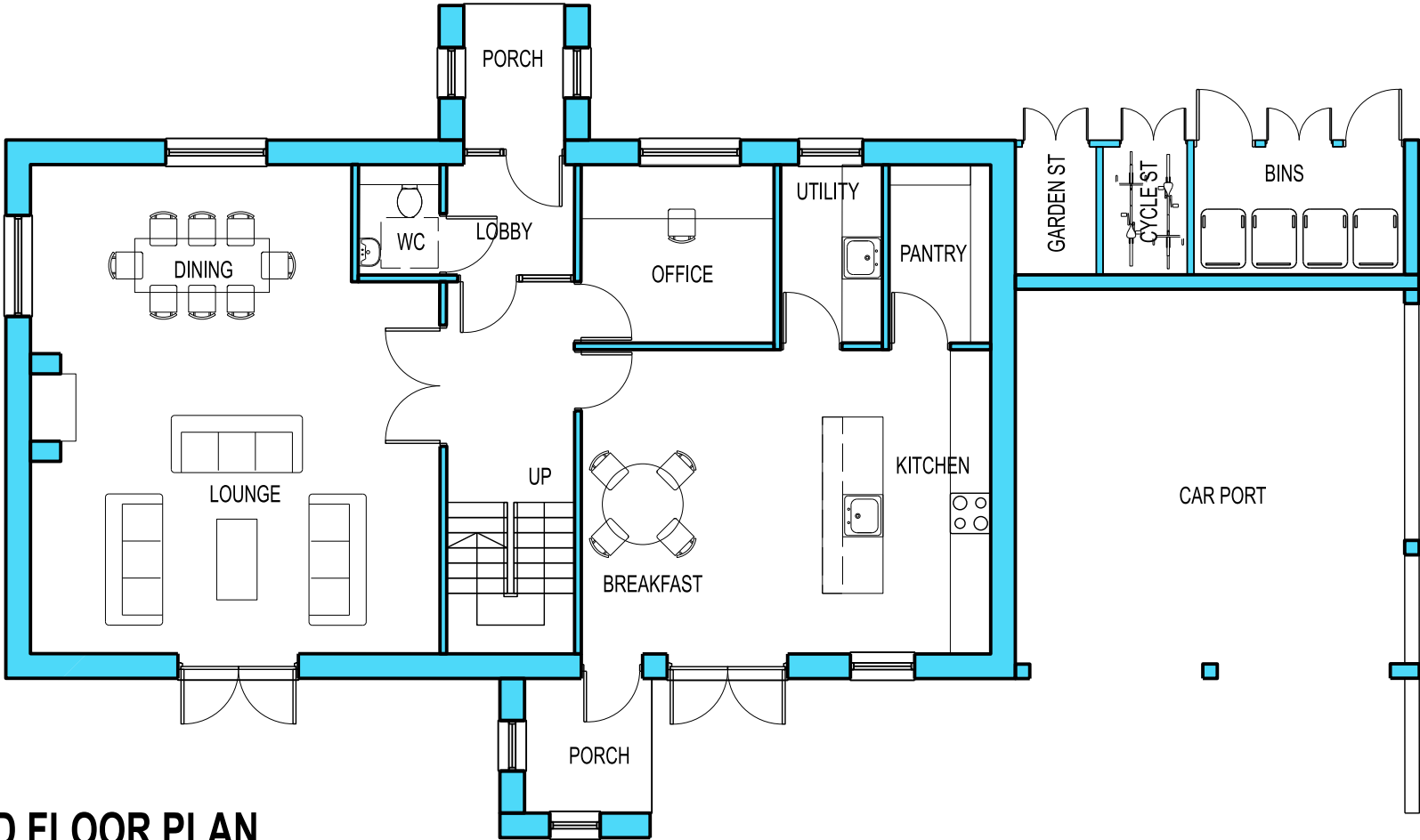
- STRUCTURE REMOVED
- NEW STRUCTURE

FLOOR AREA 207.4 SQM (2232 SQFT) GIA
PLUS CAR PORT AND BIN / CYCLE GARDEN
ST AND OPEN FRONT AND BACK PORCHES

PROPOSED FIRST FLOOR PLAN



PROPOSED GROUND FLOOR PLAN



REV.	DATE	NOTES	INIT.
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CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW FARMHOUSE
TYPE 3

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
NOV 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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DRAWING NO.
108 PLN P 09 -

Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix: Colour	C

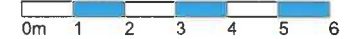
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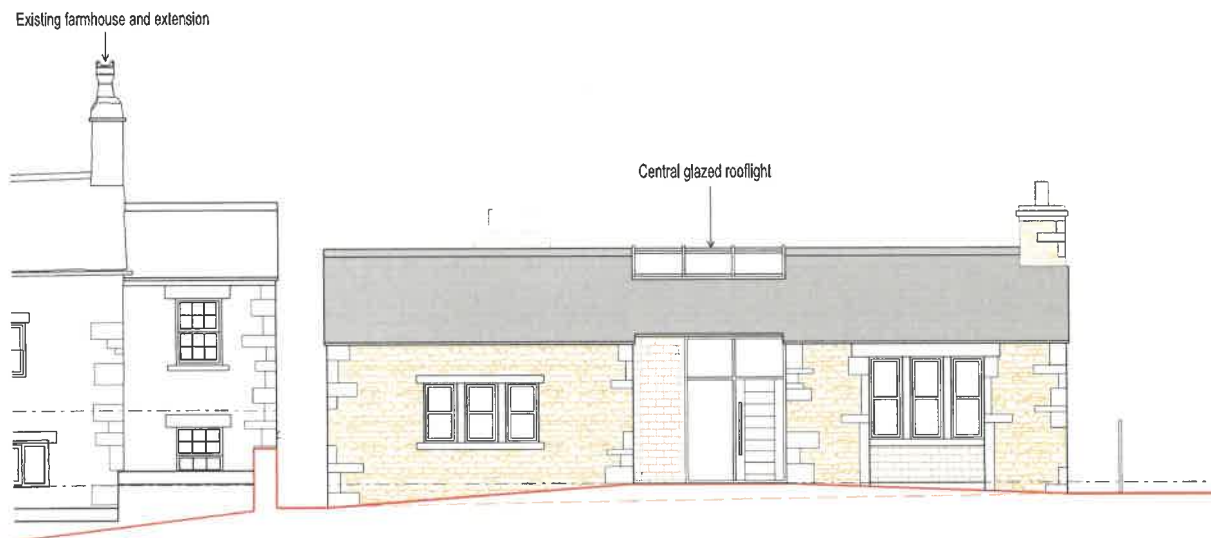


KEY

- EXISTING GROUND LEVEL
- NATURAL SPLIT FACED STONE
- NATURAL SLATE
- NATURAL DRESSED STONE
- LIGHT CREAM RENDER
- DRY STONE WALL



WEST (SIDE) ELEVATION



NORTH (FRONT) ELEVATION

REV. A 221108	TYPE NO AMENDED	RSL
REV. DATE	NOTES	INIT.

CLIENT / PROJECT
J. BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW DWELLING
TYPE 4 ELEVATIONS 1 OF 2

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
OCT 22	RSL	1:100
PROJECT NUMBER	PLAN / BUILD	TYPE
	NUMBER	REVISION LETTER

DRAWING NO.	108	PLN	E	17	A
Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Profile Colour	C

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SCALE 1:100

0m 1 2 3 4 5 6

Lead link roof to avoid maintenance issue with valley gutters

EAST (SIDE) ELEVATION

KEY

- EXISTING GROUND LEVEL
- NATURAL SPLIT FACED STONE
- NATURAL SLATE
- NATURAL DRESSED STONE
- LIGHT CREAM RENDER
- DRY STONE WALL

REV. A 221107 TYPE NO AMEDED RSL
REV. DATE NOTES INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW DWELLING
TYPE 4 ELEVATIONS 2 OF 2

STATUS
PLANNING

DATE OCT 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
108	PLN	E	18	A

DRAWING NO.

Site Location Plans	L	GA Plans	P	Elevations	E
Sections		Details		Prefor. Colour	C

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SOUTH (REAR) ELEVATION

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SCALE 1:100



KEY

- EXISTING GROUND LEVEL
-  NATURAL SPLIT FACED STONE
-  NATURAL SLATE
-  NATURAL DRESSED STONE
-  LIGHT CREAM RENDER
-  DRY STONE WALL

REV. A 221107	TYPE NO AMENDED	RSL
REV.	DATE	NOTES
		INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED NEW DWELLING
TYPE 4 SECTIONAL ELEVATION
-

STATUS
PLANNING

DATE	DRAWN	SCALE @ A3
OCT 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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DRAWING NO.	108	PLN	E	19	A
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix Colour	C

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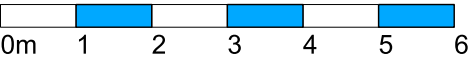
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SITE SPECIFIC HAZARDS

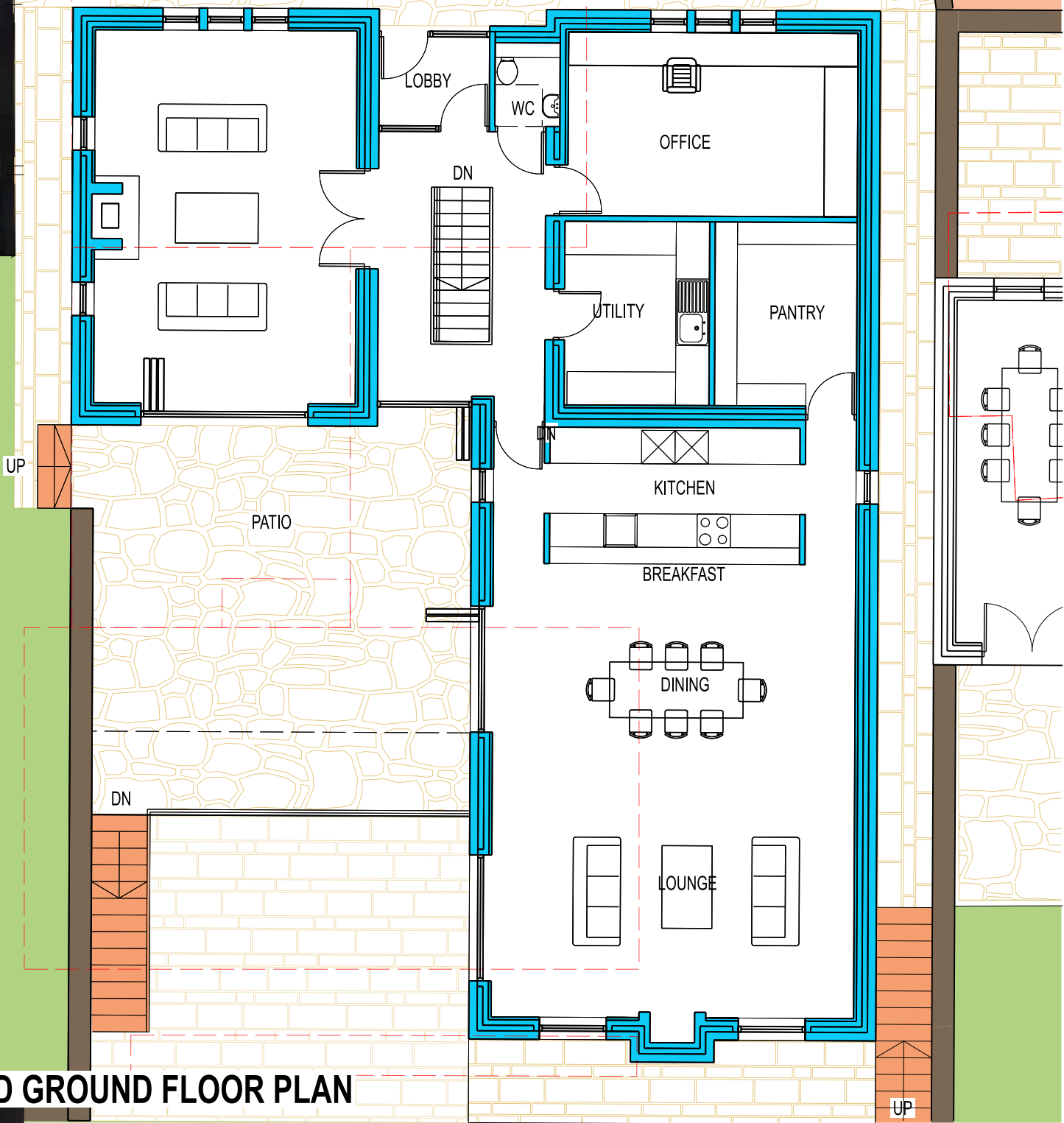
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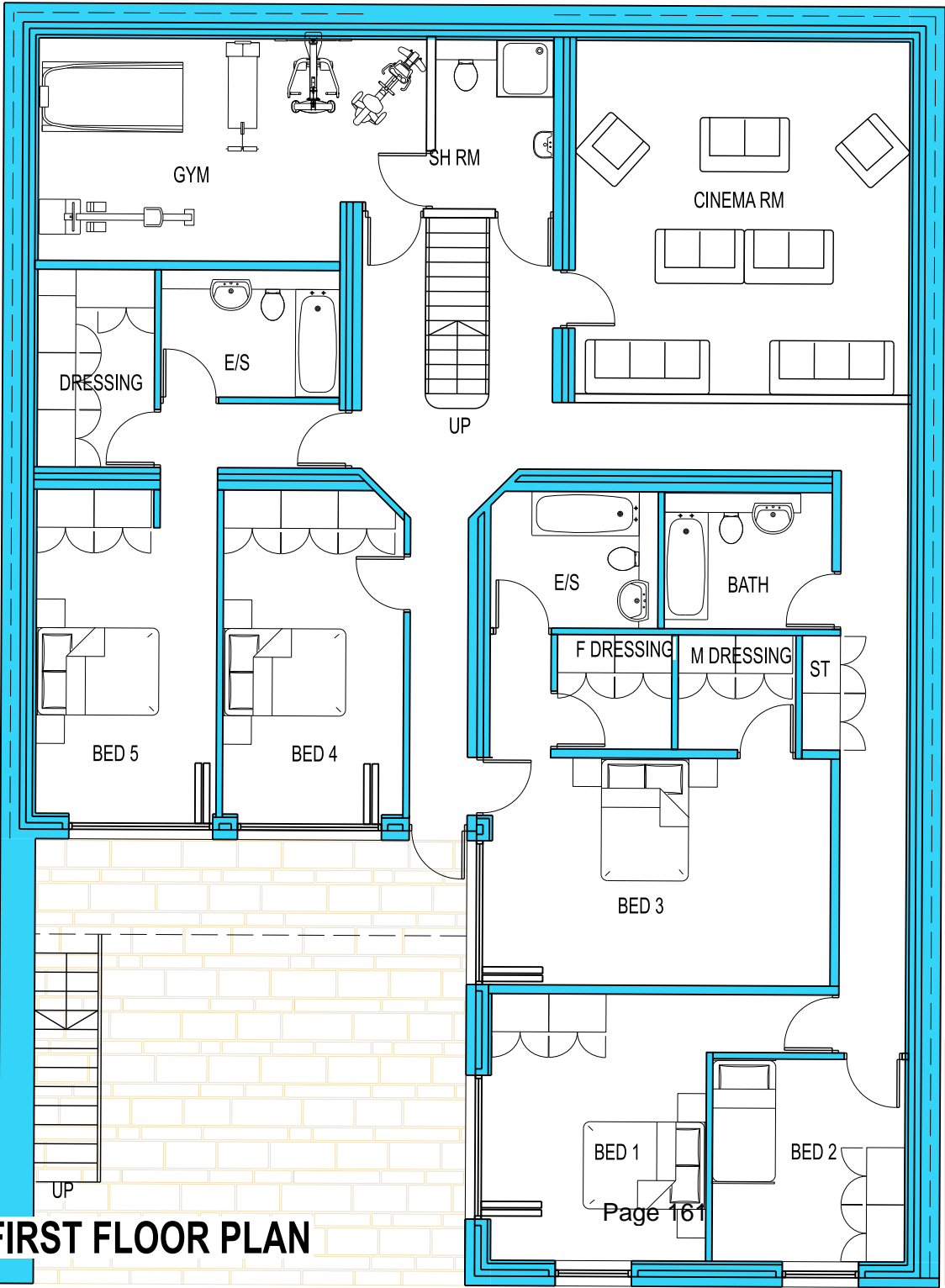


- STRUCTURE REMOVED
- NEW STRUCTURE

FLOOR AREA 375.5 SQM (4042 SQFT) GIA PLUS DOUBLE GARAGE AND BIN ST



PROPOSED GROUND FLOOR PLAN



PROPOSED FIRST FLOOR PLAN

REV. A	221107	TYPE NO AMENDED	RSL
REV.	DATE	NOTES	INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED BYRE
TYPE 4 NEW DWELLING

STATUS
PLANNING

DATE	DRAWN	SCALE @ A2
OCT 22	RSL	1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
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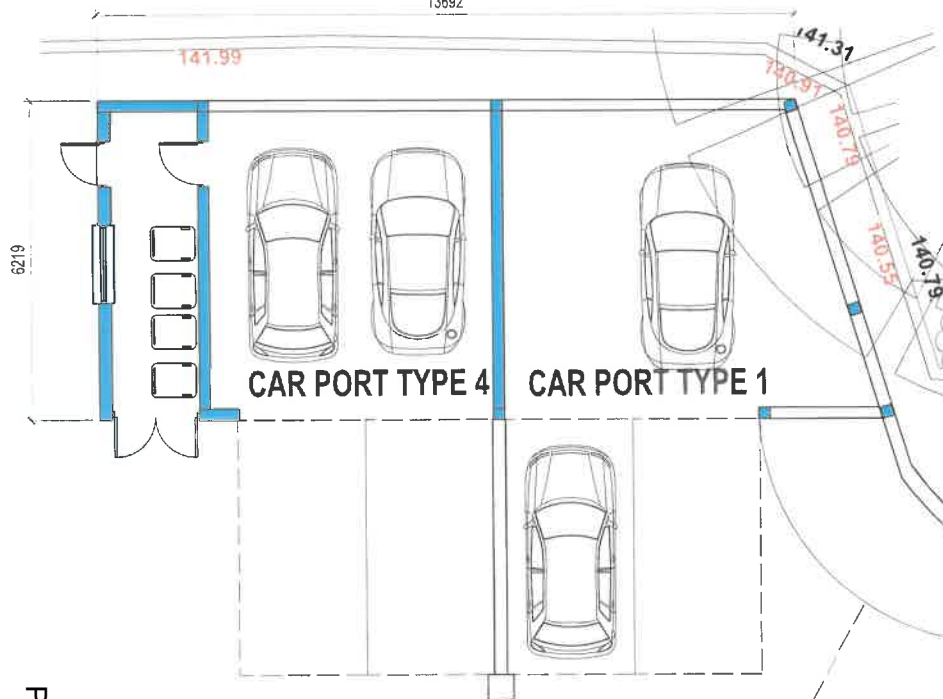
DRAWING NO.	108	PLN	P	08	A
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Site Location Plans	L	GA Plans	P	Elevations	E
Sections	S	Details	D	Prefix: Colour	C

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CAR PORT PLAN

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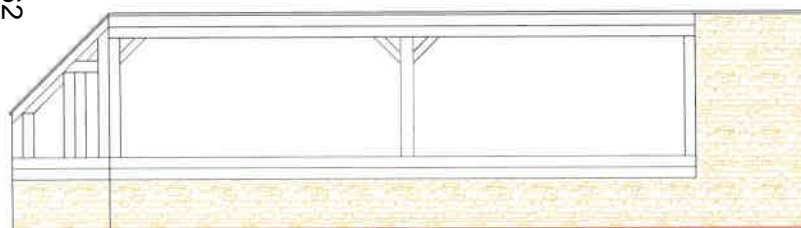
KEY

EXISTING GROUND LEVEL

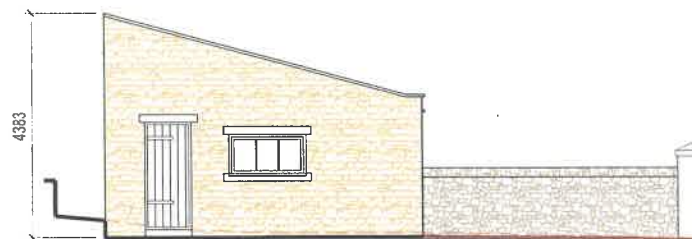
NATURAL SPLIT FACED STONE

NATURAL SLATE

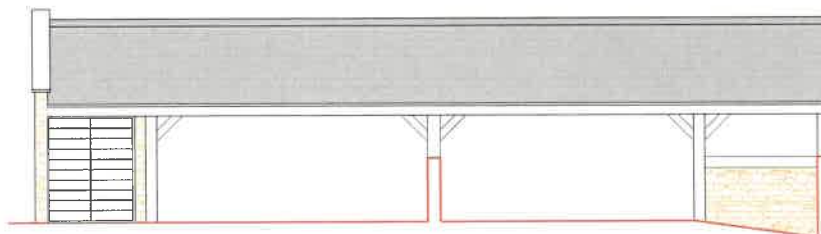
DRY STONE WALL



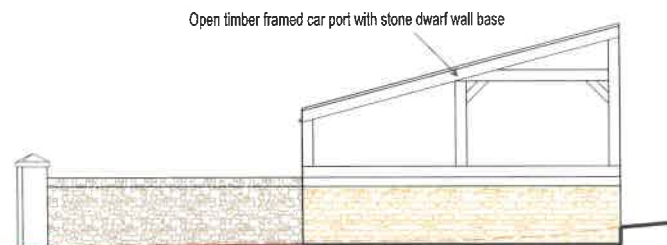
NORTH (REAR) ELEVATION



WEST (SIDE) ELEVATION



SOUTH (FRONT) ELEVATION



EAST (SIDE) ELEVATION

REV.B 221110 CAR PORT AMENDED TO LPA COMMENTS RSL
REV.A 221107 NOTES UPDATED RSL
REV. DATE NOTES INIT.

CLIENT / PROJECT
J.BUCKLEY PROPERTIES
HOLLYMOUNT FARM
GREENMOUNT

DRAWING TITLE
PROPOSED CAR PORTS
FOR TYPE 1 & TYPE 4
PLANS AND ELEVATIONS

STATUS
PLANNING

DATE OCT 22 DRAWN RSL SCALE @ A3 1:100

PROJECT NUMBER	PLAN / BUILD	TYPE	NUMBER	REVISION LETTER
108	PLN	E	20	B

Site Location Plans	L	GA Plans	P	Elevations	E
Section	S	Details	D	Profile, Colour	C

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Ward: Whitefield + Unsworth - Pilkington Park

Item 6

Applicant: Mr Keith Berry

Location: Philips High School, Higher Lane, Whitefield, Manchester, M45 7PH

Proposal: Increase existing car park from 802 m2, 43 bays to provide additional 1.354 m2 to create a further 42 car parking bays plus 2 disabled bays and 2 electric car charging stations; New automated gate 2m high to the entrance road to create a two way system; LED lighting

Application Ref: 68615/Full

Target Date: 13/10/2022

Recommendation: Approve with Conditions

Description

The application relates to the car park at Philips High School which is accessed off Park Lane in Whitefield. There are residential properties to the north on the opposite side of Park Lane with Higher Lane running along the eastern boundary of the site. The school and its grounds are located within the Green Belt.

The site is split into two halves with the main school buildings, sports halls and car park located in the northern part of the site and playing fields and sports pitches to the south.

The car park is accessed by one entrance/exit point off the internal road and provides 43 spaces. Directly to the south east of the car park is a piece of grassed land which is separated from the school playing fields by a line of trees and timber knee high fence. It is this piece of grassed land which is the subject of this planning application.

It is proposed to extend the existing car park (by 802 sqm) and parking provision for the school on this grassed area to create an additional 42 spaces (85 in total) plus 2 disabled spaces and 2 EV chargepoints, with a new access point to create a one way system into and out of the car park. The existing knee high fence which forms the boundary to the school playing fields would be retained and it is proposed to add additional trees and vegetation within the existing line of trees. Access to the playing fields for maintenance and grass cutting purposes would be provided by a 2m high gate. 4m high LED light poles would installed around the car park boundary.

Need for the development

Like many schools in the Borough, over more recent times, Philips High have progressively experienced a significant increase in pupil intake and staffing levels (up to 57% since 2004) which has thereon added to parking pressures which have forced staff and visitors to either park elsewhere within the school grounds which restricts circulation and movement within the site or by using Park Lane and surrounding streets which can be an issue for local residents.

The application site is often used as an informal 'overflow' car park when the school is used out of hours and at weekends for sports events and matches and in inclement weather can churn up the grassed surface.

The school states that the additional car park will alleviate the parking pressures within the school and surrounding streets and providing the one way system will improve circulation, traffic flows and safety.

Relevant Planning History

44832 - Formation & extension of new play area - Approve with Conditions 21/12/2005
 51348 - Erection of sports hall building including interlinking single storey lean to corridor and single storey lean to storage - Approve with Conditions 02/09/2009
 52502 - Extension to existing sports hall to provide reception, office and classroom - Withdrawn - Invalid 27/04/2010
 55191 - Non material amendment following granting of planning permission 51348 for corridor to be shortened back to fire escape; Hall to decrease in size from 40m x 35m to 38m x 35m; changes to levels but ridge height not changed. - Approve 12/06/2012
 67085 - Provision of infill, barrel vaulted canopy with open sides to a indented area space - Approve with Conditions 29/09/2021

Publicity

Letters sent on 19/8/22 to 41 properties.

One objection received with the following comments-

- Eighty plus cars with the additional stuff you want to allow to be built as a car park directly opposite on Park Lane and in front of our house which we chose to live in some ten years ago, which had the most beautiful views over to the golf course, shattered. We already tolerate the ugly green basketball building, for which we had no notice for.
- Not just for the fact above, but the two way traffic constantly, and much worse at weekend, in and out of that car park and onto Park Lane will naturally be horrendous, unsafe for the public, schoolchildren and very dangerous.
- Park Lane is not a main road and will not be able to cope with the traffic in and out of the "car park". It is the main thoroughfare for the majority of cars in Whitefield area to drive down to reach Higher Lane. There are no zebra crossings on Park Lane as it is, for the pedestrians, mothers walking babies, and general public.
- This added, to what could be total chaos when residents of Parkhill Drive, opposite the school and "car park", wish to drive to and from their homes and try and slip into Park Lane amongst all the oncoming traffic, or the reverse to get back to their homes.

The objector has been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection subject to conditions

Borough Engineer - Drainage Section - No response received

Environmental Health - Pollution Control - No response received

United Utilities - No objection

Sport England - No objection subject to conditions

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EN1/2	Townscape and Built Design
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
CF1/1	Location of New Community Facilities
CF2	Education Land and Buildings
OL1/1	Designation of Green Belt
OL1/5	Mineral Extraction and Other Development in the Green Belt

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning

considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Green Belt Policies -

The site is located in the Green Belt and therefore consideration needs to be given to the acceptability of the principle of the proposed development.

Chapter 13 - Protecting Green Belt Land, Paragraphs 147 and 148 state that inappropriate development in the Green Belt is by definition, harmful and should not be approved except in Very Special Circumstances (VSC). Planning Authorities should ensure that substantial weight is given to any harm in the Green Belt. VSC will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Paragraph 150 states that certain forms of development are not inappropriate in the Green Belt providing they preserve its openness and do not conflict with the purposes of including land within it.

One of these exceptions is sub-section b) for engineering operations which is considered relevant as the proposed development would comprise groundworks and resurfacing of land.

Green Belt Principle - The proposed development would comprise groundworks to re-surface the grassed area to create a hardstanding area. The site is located directly adjacent to the existing car park and contained within the nucleus of the main school development with the backdrop of the existing sports hall and ball hall to the rear. To the south west, the site is separated from the school playing fields by a defined boundary of a knee high fence and line of trees and vegetation.

The proposed development would not encroach the Green Belt beyond the footprint of the existing school buildings and would be screened by planting and vegetation from wider range views from the south west.

The school have also provided a case for the very special circumstances for the need of the proposed development as follows:

- Essential parking needs for the school, dual use sports facilities and therefore betterment to the wider community in terms of relieving parking pressures in the area;
- Reduction of the use of the school's playground areas and other areas of the school site which would improve pedestrian access areas for the pupils, staff and visitors;
- The development would not result in additional traffic to the school; the proposals are to better the current parking problems;
- The development would be low level surface works and would not encroach the Green Belt beyond existing buildings within the school site;
- Improved traffic circulating within the school site by the provision of the one way route to and from the car park.

Given the position of the car park within the school site and that it would be contained within an already defined boundary and it is considered the proposed development would not have a harmful impact on the openness of the Green Belt and as such would be considered appropriate development in accordance with the principles of the NPPF.

Land Use Principle - Sport England - As the development would utilise a grassed piece of land within the school grounds which could potentially have use as playing fields or previously used as playing field in the last 5 years, Sport England have been consulted as a statutory requirement.

Sport England have responded as follows.

The existing car parking area is separated from the playing field by an existing hedge. The proposal would not extend into the playing field areas or sports pitches. The plans show

excavated material being disposed of within the school grounds and along the boundary between the car park and the playing field where there could be a risk that this would reduce the area available to pitches and pitch sports (and their safety margins) if placed on the opposite site of the hedge closest to the playing field.

Assessment of the Proposed Development against Sport England Playing Field Policy and the NPPF

Sport England's Playing Fields Policy and Guidance provides clarity and advice on how Sport England assesses planning applications affecting playing fields. The development proposed is for an extension to the car park alongside existing car parking, it would not extend into the grassed playing field used for pitch sports and is on a part of the site not used for pitches, and therefore assessment against policy Exception E3 would be pertinent in this case.

To comply with Exception E3 the proposed development must only affect land incapable of forming part of a playing pitch and does not:

- reduce the size of any playing pitch;
- result in the inability to use any playing pitch (including the maintenance of adequate safety margins and run-off areas);
- reduce the sporting capacity of the playing field to accommodate playing pitches or the capability to rotate or reposition playing pitches to maintain their quality;
- result in the loss of other sporting provision or ancillary facilities on the site; or
- prejudice the use of any part of a playing field and any of its playing pitches.

In this case the development would not be on land that is or has been marked as pitches in the past; and provided that the disposal of spoil does not encroach onto the grassed playing field where it could affect pitches or their safety margins Sport England consider that the proposal would meet Exception E3. This would include the location of any storage of materials or contractors compound which must be kept off the playing field.

The school have also provided a written confirmation that the site has never been used as a playing field by the school.

Historic aerial photographs dating back to 2009 also confirm the site had not been marked out as a playing pitch.

Conclusion

In making the above assessment, Sport England do not wish to raise an objection subject to planning conditions:

- (a) preventing disposal of spoil on the playing field; or if on site, agreeing in advance of any commencement of development (after consultation with Sport England) precisely how much, how and where any spoil disposal would be carried out; and
- (b) agreeing the position of any wider works, or material storage, etc and contractors compound before commencing the development.

Conditions have been recommended for a Construction Traffic Management Plan, control of disposal of excavated material and details of the location and disposal of excavated material.

It is also therefore considered that the proposed development would therefore comply with paragraph 99 of the National Planning Policy Framework (NPPF).

Layout and impact on surrounding area - The car park is currently accessed via a single entry/exit point which is taken off the internal school road. The application proposes to create a one way system whereby traffic would enter the car park from the existing access point and exit from a newly created opening from the proposed car park which would improve vehicular movement within the school grounds and control traffic flows to and from

Park Lane. The development would also provide safer pedestrian movement for pupils, staff and visitors.

The proposed car park would be delineated for an additional 42 parking spaces, 2 disabled access spaces and 2 EV chargepoints.

The additional parking capacity would not result in an increase in traffic to the school, the intention being to alleviate the school's current parking pressures and parking on Park Lane and nearby streets. The proposed development would be a betterment above the current parking situation for residents.

A gate from the new car park to the playing fields would be installed to enable access to the playing fields for maintenance purposes and grass cutting and the line of trees and vegetation which currently forms the boundary between the site and the playing fields would be enhanced with additional planting.

The school and the sports pitches and remainder of the school site would be unaffected by the proposed development.

The Highway Section have raised no objection to the proposed development subject to conditions.

It is therefore considered that the proposed development would provide a betterment in terms of parking provision, access and safety and as such would comply with EN1/2, CF1/1, HT2/4 and HT2/6.

Impact on residential amenity - The proposed car park would be contained within the school grounds and there would be a distance of over 40m from the development to the nearest houses on Park Lane. The development would comprise ground and surfacing works and there would be no built structures. With the intervening road and screening from trees and vegetation, it is considered the layout, scale and position of the car park would not significantly impact on the outlook from the nearest houses opposite the site. Those properties on Park Lane to the west of the site would be more than 200m away and remain unaffected by the siting of the car park.

In terms of traffic generation, whilst the car park extension would increase capacity within the site, it would better the current situation for local residents by taking cars off the nearby streets. As the school is also used as an out of hours sporting facility at weekends and evenings, there would be less traffic or on-street parking by those using the venue.

It is proposed to install 4m high LED lighting columns around the perimeter of the car park. The lamps would be directed downwards with minimal light spillage and given the distance away from the nearest dwellings, light pollution would not be an issue.

The proposal would not therefore have any unacceptable impacts upon neighbouring amenity and would comply with CF1/1 and HT2/4.

Drainage - It is proposed to form a new drainage channel along the north eastern side of the existing car park and provide drainage channels within the new car park to funnel surface water run-off so that it does not flow onto the highway.

United Utilities have been consulted and raise no objection to the proposed development, subject to the consideration and investigation of sustainable options in accordance with the hierarchy of drainage and the NPPG which would be included as a condition.

Response to objections.

- The issues raised with regards to traffic generation, parking and impact on visual amenity have been covered in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development

Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings - Existing - Car parking layout, site details and ground levels KB 18314 01 Existing; Proposed existing and proposed plan - Car parking layout levels KB 22301 02 SITE; Additional vegetation application planting areas KB/03/PLANTING and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Where during any works on site, unforeseen contamination is suspected or found, or contamination is caused, works on the site shall cease and the Local Planning Authority shall be notified immediately. The developer shall then produce a risk assessment and submit remediation proposals, if required, for approval to the Local Planning Authority. On approval of the remediation strategy, the development shall then be carried out in accordance with the approved details and process including any required timescales.
Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
4. There shall be no disposal of excavated material or arisings from the development on the playing field.
Reason. To protect the playing fields from damage, loss or availability of use and to accord with chapter 15 - Conserving and enhancing the natural environment of the NPPF.
5. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
 - Photographic dilapidation survey of the site access in the event that subsequent remedial works are required following construction of the development;
 - Access point for construction traffic from the adopted highway;
 - Site hoardings (if proposed);
 - Hours of operation and number of vehicle movements;
 - A scheme of appropriate warning/construction traffic signage in the vicinity of the site and its access;
 - Parking on site (or on land under the applicant's control) of operatives' and construction vehicles;
 - Measures to mitigate any loss of existing parking facilities during construction of the development;
 - Storage on site of works/contractors' compound (including any buildings,

moveable structures, works, plant, machinery, construction materials, including any requisite phasing of the development to accommodate this;

- Measures to ensure that all mud and other loose materials are not spread onto the adjacent adopted highways as a result of the groundworks operations or carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations;
- A scheme for the removal of the works/contractors' compound and the restoration of the land on which it is situated.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period.

The areas identified shall not be used for any other purposes other than the parking of vehicles and storage of construction materials. All highway remedial works identified as a result of the dilapidation survey shall be implemented to the written satisfaction of the Local Planning Authority prior to the development hereby approved being occupied.

Reason. To protect playing fields/sports facilities from damage, loss or availability of use and to maintain the integrity of the adopted highway, mitigate the impact of the construction traffic generated by the proposed development on the adjacent adopted highways, ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict and the principles of the NPPF.

6. The car parking, one way working arrangements and pedestrian facilities indicated on the approved plans shall be surfaced, demarcated and made available for use and thereafter maintained at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

7. No development shall commence unless and until surface water drainage proposals have been submitted to and approved in writing by the Local Planning Authority. The scheme should be in accordance with the submitted Surface Water Sustainable Drainage Assessment and must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented prior to first occupation and thereafter maintained.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

8. The additional planting shown to be provided on the approved plans shall include native tree and shrub planting and shall thereafter be implemented not later than 12 months from the date the development is first used or within the first available tree planting season, and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of

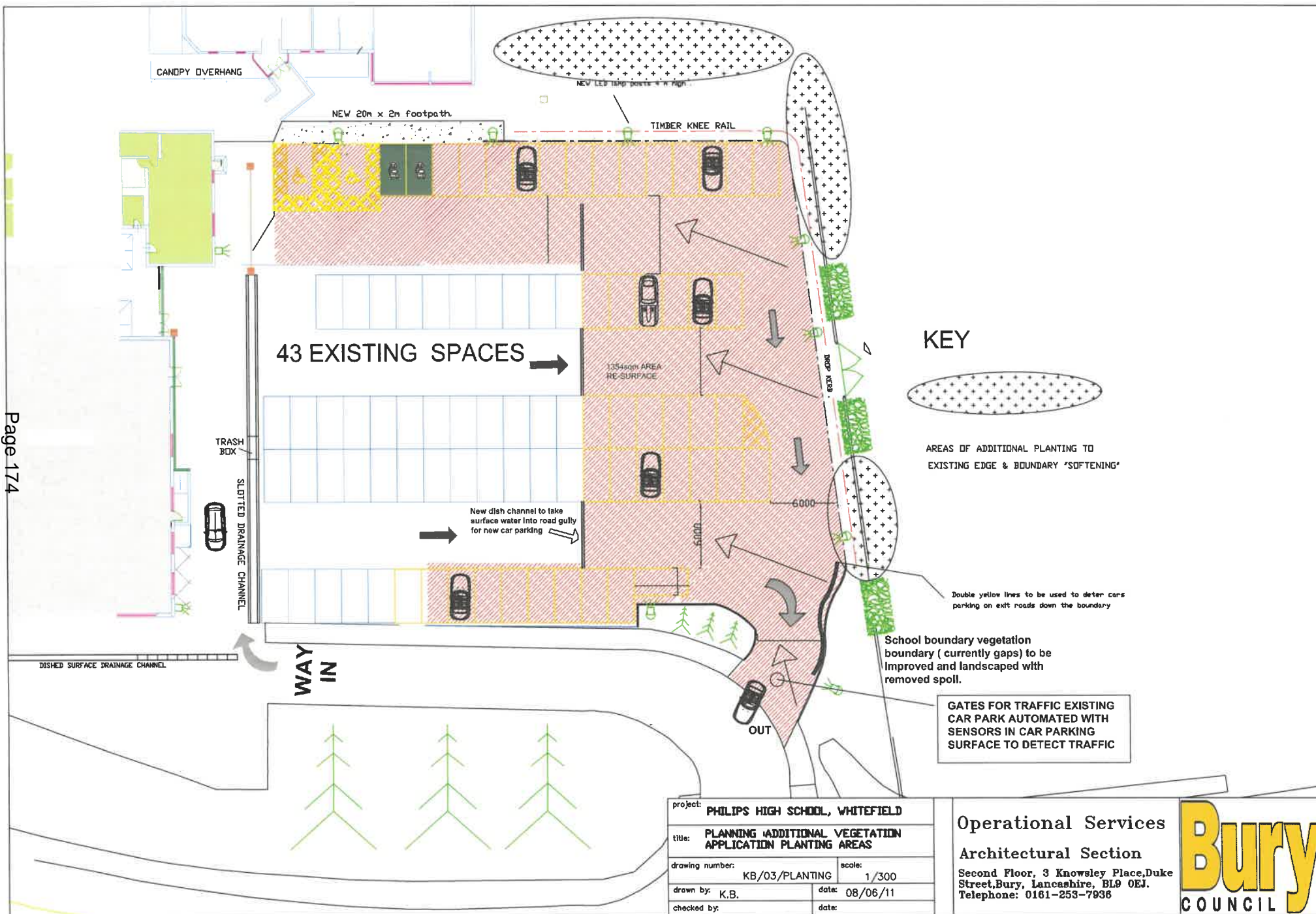
visual amenity pursuant to Policies EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

9. The two EV chargepoints hereby approved shall be provided prior to the first use of the development hereby approved and thereafter maintained.

Reason. In accordance with the NPPF, to encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**





Ward: Radcliffe - North and Ainsworth

Item 7

Applicant: Mr Phillip Johnston

Location: Land to rear of 104 Bury New Road, Radcliffe, Bolton, BL2 6QB

Proposal: Removal of existing outbuildings; formation of hardstanding; construction of stable block with tack room / feed store; waste storage container and erection of fencing, gates and stile

Application Ref: 68778/Full

Target Date: 07/11/2022

Recommendation: Approve with Conditions

Description

The site relates to land which is located behind a row of terraced properties, Nos 100-104 which front onto Bury New Road. To the north east is a garage and car sales business and to the south is open land and fields. The site is within the Green Belt.

The applicant states the current use of the land has historically been used as allotments and for the keeping of pigs, chickens and pigeon farming and there are 4 outbuildings on the site which have become dilapidated and in a poor state. The land has also become overgrown by vegetation and grass. The site is bounded by timber fencing.

The site is accessed via an unadopted single lane from Bury New Road which is between the side gable of No 104 and the adjacent garage/car sales business and leads to an area of hardstanding which is used to park and manoeuvre vehicles. The hardstanding extends to the rear of 104 and is also used for parking, at the end of which is a gated access to the rear gardens of the adjacent dwelling.

The access lane to the site from Bury New Road carries a Public Right of Way (No 21CC) which continues along the north eastern side of the site and field beyond, accessed via a stile.

The application seeks the removal of the existing buildings and proposes the construction of a stable block with tack room and feed store and waste storage facility. It is also proposed to provide an area within the site for parking and turning of vehicles. New boundary fences would be erected around the site.

The applicant states the stables would be let for the personal use of 2 individuals only.

The site would essentially be split into two parts. The proposed stables block would be formed in a 'U' shape in the western part of the site for up to 8 horses. It is proposed to lease the stables for the personal use of 2 individuals, each with access to 4 no. stables with separate tack and feed stores within the build. The stables would be constructed of timber with composite roof panels.

A container for waste collection would be located adjacent to the stable block.

The stable block would be separated from the eastern part of the site by a timber fence. This remaining part of the site would be used as a parking and turning area for trailers which would be formed of a porous surfacing such as grasscrete.

A timber post and rail fence would enclose the southern boundary of the site and new timber fence and gates would be formed to the parking area at the site entrance.

The application is a resubmission of a scheme for a development for the construction of 2

no stables blocks for 8 stables in total, which was refused for 5 reasons:

- Intensification of use of the access with sub-standard vehicular and pedestrian provision,
- Impact on Green Belt due to siting and scale of the development,
- Failure to demonstrate Very Special Circumstances due to impacts of scale and siting,
- Siting and extent of boundary treatment and hardstanding,
- Impact on residential amenity,
- Insufficient information submitted to assess ecological impact

Relevant Planning History

67301 - Removal of existing outbuildings; formation of hardstanding; construction of 2 no. blocks of stables with tack room / feed store; waste storage container and erection of fencing, gates and stile - Refused 5/1/22

Publicity

Neighbours notified by letter on 15/9/22

Site notice posted 30/9/22

Press advert 22/9/22.

Three objections received with the following issues:

- Existing floorspace: The applicants state that the existing gross internal floorspace is 140sqm but this is incorrect and at the most it would be only 45sqm. The existing buildings have never had planning application approved and they were only temporary sheds.
- Opening Hours: The applicants have answered NO, when it should states YES, because of there will be regular access and noise disturbance to surrounding properties.
- Industrial and Commercial Machinery: On the application form it asks about industrial and commercial machinery and the applicants have responded NO to both, however, on review of the plans, the applicant has stated that they will be using tractors.
- Existing Use: There is a further question about 'existing use' and the description provided is incorrect because they have stated that previous use of the land included, pigs and chicken farming and this is factually incorrect. In the time we have lived near there (13 years), it has never been used for that purpose!
- Site Vacant: There is a further question asking if the site is vacant and the response is NO, again, this is incorrect as the site has not been used for 2 years and is now overgrown and unkempt.
- Contamination: there is a question in respect of contamination and the response is NO, again incorrect as the applicants are intending to use some of the site for car parking, so therefore the response should be YES.
- Vehicle Parking: the applicant has stated that the vehicle type is car, but there will also be vans, horseboxes and towing vehicles. Again, this is incorrect information.
- Sewage: on the planning application form, the applicants have stated that they do not know how they will deal with drainage and it will not connect to the existing drainage system. I can't see any further details about the drainage system and it appears that it will be left to soak into the ground ? - the drainage system around 100 Bury Road is very weak at the best of times and having this development adding to it will cause many issues (noise, smells nuisance, contamination)
- Waste storage and collection: It is proposed that the waste management system is to be at end of a garden - the proposal is to be 6 m which will cause a nuisance (horse flies and other airborne pests) and we will be unable to allow the young children to play in the garden.
- Trade Effluent: The applicants have answered No on the planning application. This is a commercial build so there will be trade waste.

In review of SPD 10:

- The development is too close to neighbours - I can't see a 30m buffer.
- Are the stables essential and genuinely required? The landowners have stables 60 m across the road on their own property.

- In our view, this is intended to be a commercial enterprise, as evidenced by the commercial enterprise they presently run on their land but use a different access way as opposed to forcing a new access where one does not exist.
- Bridleways - this articles states the need for access for a good access to a bridle access network;
- It states on the plans, that the proposed access is a public walkway. There are no bridleways near where the applicants wish to place the stables and would need to take the horses onto a busy 'A' road. The applicants have another point of access further down the road on Bury New Road - should this not be where the stables are built given there is access already in place?
- The access the applicants intend to use is a private pathway, that has extremely dangerous ingress and egress between a car pitch business and a dwelling (104). This road is an 'A' road and has 40,000 cars using it daily. The entrance is usually blocked with cars from the garage and visiting customers. The area on the plans where the intended entrance gates are to be situated is where neighbours in adjoining houses park their cars/vans and digger equipment. Any vehicles trying to use the entrance would also have to use both lanes of the main road to gain access or leave the area creating very a dangerous situation on the main road especially as people already drive in excessive speeds in excess of 60-80mph.
- Lighting - There is nothing in the plans about lighting which will be required. This area has no access to water or electricity so the applicants would have to use a generator to light the stables as stables are normally attended at out of hours as well as during the day. There will be noise and air pollution from a generator running.
- The existing buildings referred to within the application are extremely smaller that the application implies. These are temporary buildings with no foundations, and are sitting on old beer kegs. They were used by the neighbours for a hobby. No planning application has ever been approved.
- In conclusion, we are against the stables being built next to any of the dwellings (100, 102 104), they should be built further away from the houses (minimum 30+ meters) to prevent a detrimental effect of our living standards for ourselves and our children from horse flies/rats and vermin/stench and noise/ light pollution from the commercial stables. Any stable access should be further down where they currently enter when undertaking there other commercial ventures.
- Unsuitable development in the Green Belt
- Question the accuracy of the red edge site plan
- I don't think the access road is suitable for the flow of traffic that will be created by the new stables.
- Rodents, particularly mice and rats, are already a problem. My concern is that these new stables and feed storage will only worsen the problem, considering the proximity to my property.
- Not only this, the smell of waste storage container I'm sure will not be pleasant. I want to be able to open my windows and doors without having to smell manure.

Those who have objected have been informed of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection subject to conditions.

Public Rights of Way Officer - No objection.

Environmental Health - Contaminated Land - No objection subject to conditions

Greater Manchester Ecology Unit - No objection subject to condition/informative

Environment Agency - No response received

United Utilities (Water and waste) - No response received.

Pre-start Conditions - The applicant/agent has agreed with pre-start conditions

Unitary Development Plan and Policies

NPPF National Planning Policy Framework

EN1/1 Visual Amenity

EN1/2 Townscape and Built Design

EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN7/3	Water Pollution
OL1	Green Belt
OL4/7	Development Involving Horses
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD10	Planning for Equestrian Development
SPD11	Parking Standards in Bury

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Chapter 13 - Protecting Green Belt Land, Paragraphs 147 and 148 state that inappropriate development in the Green Belt is by definition, harmful and should not be approved except in Very Special Circumstances (VSC). Planning Authorities should ensure that substantial weight is given to any harm in the Green Belt. VSC will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Paragraph 149 states that the construction of new buildings is inappropriate in the Green Belt. One of the exceptions listed is for (b) the provision of appropriate facilities (in connection with the existing land use or a change of use) for outdoor sport, outdoor recreation, cemeteries and burial grounds and allotments; as long as the facilities preserve the openness of the Green belt and do not conflict with the purposes of including land within it.

Policy OL4/7 - Development Involving Horses states the keeping of horses for recreational purposes or as part of commercially based equestrian activities will be considered acceptable where it would not have an adverse effect on the appearance of the rural areas. In particular, high standards of design, construction and maintenance will be expected as part of any development proposals.

The scale and size of buildings will be important, only being considered acceptable where design and siting would not have an unduly detrimental effect on the surrounding environment.

SPD10 - Planning for Equestrian Development provides further policy guidance for equestrian facilities with consideration and advice including the siting, design scale, and associated parking and access.

Green Belt principle - The development would involve the erection of stables in the Green Belt and therefore in principle, the proposed development would meet the criteria of para 149 bullet point (b) of the NPPF for the provision of facilities for outdoor sport and recreation.

The test is whether the development would have a greater impact on openness. This is considered below.

Siting and scale

The existing site comprises four outbuildings and storage sheds enclosed within a fenced boundary. The proposed development would comprise a single block in a 'U' shaped form,

concentrated within one area of the site and for the most part, the proposed stable block would be contained within the existing site boundary. The part of the block linear to the southern site boundary would extend 1.4m beyond the existing site boundary but this incursion into the Green Belt is considered marginal and not to a significant degree which would cause adverse impacts or harm to the openness of the Green Belt.

The remaining site area would provide the parking and turning facilities for the two users which includes a space to park a trailer should the need arise. This element of the development would encroach no further than the existing site boundary. The parking area would be surfaced in a porous or grasscrete material which would also minimise visual impacts to the area. A certain amount of hardstanding/surfacing to serve a stable development is generally accepted and in this instance, the proposed area is considered sufficient enough but not overly or unacceptably excessive for the scale of development proposed.

SPD 10 states at para 4.2 that generally speaking, applications for 3 to 4 small stables and one storage area for tack/hay/animal feed in one given location may be acceptable, subject to detail and capacity of the land and number of horses involved.

The application proposes a development for 8 stables. In this case, the applicant has provided written confirmation that the stables would be for the private use for 2 individuals only and would not be for commercial purposes.

In turn, a less extensive parking and storage area would be required and the proposals would provide for one car space and one trailer space for each user.

As such, the scale of development and number of stables to serve two individual users only would be acceptable and comply with SPD10. The use of the stables for personal use for two individuals can reasonably be secured by condition to which the applicant has agreed. It is therefore considered the proposed development would not cause significant harm to the openness and special character of the Green Belt and therefore would be appropriate development and comply with the principles of para 149 (b) of the NPPF. Very Special Circumstances would not be required.

Design and appearance

The stables would be single storey and formed in a 'U' shape and as such only relatively short lengths of the build would be visible from public views.

It would be constructed of concrete block and timber cladding and with a composite clad roof and as such materials would be appropriate for the character of development and within the rural surrounding area.

In terms of boundary treatment, SPD10 advises that post and rail fencing is the most commonly used type of enclosure which is a more sensitive solution and less harmful to openness and Green Belt character. It is proposed to erect a 1.5m post and rail fence to the field boundary to the south which is considered an appropriate and less visually intrusive solution and which are commonly found in Green Belt locations. To the site and entrance from the access lane, a 2.1m high timber boundary fence and gate would be installed and which is considered acceptable enclosure to the development entrance.

As such, it is considered the proposed development would comply with the principles of para 149 (b) of the NPPF, UDP Policy OL4/7 and SPD10 and would therefore be appropriate development and as such Very Special Circumstances would not be required.

Highways issues

The access to the site would be via the existing single wide lane and Public Right of Way (PRoW) which leads from Bury New Road and along the side gable of No 104.

As confirmed by the applicant, the stables would be for the use of 2 individuals only and as such, traffic generation and movement along the access lane would be relatively minimal. A turning area within the site boundary would be provided to facilitate the manoeuvre of

cars with a trailer which would enable cars to exit from the site in a forward gear. The site would provide a parking space for each of the users of the stables and a space for the parking of a trailer should the need arise and as such this would be sufficient to serve the two individual users of the site, which in turn would minimise development within the Green Belt.

It is therefore considered that the scale of the development would not be significant to be disruptive to traffic flows on Bury New Road or cause concerns in terms of impacts on highway safety or to pedestrian users of the PRow.

The area of hardstanding to the east of the site area would be retained for parking for local users/residents in the area.

The PRow continues past the eastern boundary of the site and thereon through the field which extends beyond the site to the south. The PRow and access to it would not be affected by the development.

The PRow officer has requested that the applicant be made aware that no public rights of way should be affected or damaged by the proposed development. An informative to the applicant would be included to this effect, and to also advise the applicant that the new stile should be of a design that creates the least obstruction possible for users of the public footpath.

The applicant would be made aware of the need to safeguard people using public rights of way and that no public rights of way are damaged, widths altered, gates erected across their width (unless authorised by Bury Council the Highway Authority) or obstructed by any other means either during or as a result of development. The applicant must ensure that no changes to land levels/surface water run-off affects a public right of way. If a temporary obstruction of public rights of way is unavoidable, no development must take place until a temporary closure order has been made by Bury Council and a suitable temporary alternative route(s) made available.

The Highway and PRow Section have raised no objections to the proposed development, subject to the inclusion of a condition to limit the use of the stables for 2 individuals only in order to control the scale of development and subsequent traffic movements to and from the site. A condition for the submission of a construction traffic management plan and further details of the design of the access and turning areas to serve the site would also be included.

It is therefore considered that the proposed development would be acceptable and would comply with OL4/7 - Development Involving Horses, HT2/4 - Car parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.

Impact on residential amenity - The nearest residential properties are to the north, Nos 100-104 Bury New Road.

Where it is considered necessary and depending on the circumstances of the site, SPD 10 suggests that a buffer of 30m between residential properties and stable development be considered.

Whilst the site would be on land to the rear of these houses, the proposed stable block itself would be behind a vacant plot of land, and such that the stables would be off-set approximately 10m to the south west and the rear boundary of No. 100 Bury New Road and more than 20m from this dwelling itself.

There is an intervening fence which runs along the northern boundary of the site as well as boundary treatment to the rear gardens of the neighbours plus domestic type sheds and trees and vegetation and as such a substantial buffer in terms of separation distances and screening between the site and the neighbours already exists.

The stable building would be orientated such that there would be no openings which would directly face the houses and as such disturbance and activity from the stables area would be limited.

The proposed parking and turning area for the stables would be located behind Nos 100-104. As the site would only be used by 2 individuals, traffic and noise/activity from cars arriving and leaving would be relatively minimal. The car park area would only be of a size necessary serve the development and provide a parking and trailer space for each user with an area to turn in order to exit the site in a forward gear. The parking area would also be enclosed by a substantial boundary fence and a gated access which would contain the developed area and screen views.

A box trailer would be provided for the storage of horse waste and manure which would typically be collected once or twice a week and used by the local farmer.

The scale of the proposed development would be significantly reduced comparative to the previous scheme and it is therefore considered that together with the separation distances, relationship and orientation of development and intervening boundary treatment there would not be a harmful impact on the adjacent neighbouring properties.

As such, the proposed development would comply with OL4/7, EN1/2 and SPD10.

Ecology

Bats

Greater Manchester Ecology Unit (GMEU) have been consulted and sought clarification of the state of the structures to be demolished to determine whether a bat assessment would be required. The applicant has provided information and photographs of the structures to be removed and GMEU have been satisfied that there would be a low risk for bat roosting potential. An informative to advise the applicant of their responsibilities should bats be found.

Nesting Birds

The site appears to include potential bird nesting habitat. All British birds nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife & Countryside Act 1981, as amended. GMEU recommend a condition along the following lines be applied to any permission.

No site clearance or demolition shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.

Invasive Species

Sites such as this are moderate risk for invasive species. GMEU have identified there may be evidence of himalayan balsam on the site.

A condition has been recommended for the submission of a method statement detailing eradication and/or control and/or avoidance measures for himalayan balsam for approval by the Local planning Authority.

Contributing to and Enhancing the Natural Environment

Section 174 of the NPPF 2021 states that the planning policies and decisions should contribute to and enhance the natural and local environment. The site appears to be of low ecological value but will result in a net increase of bare and developed ground. Some form of mitigation is recommended. GMEU would suggest some form of native boundary planting as mitigation. This could be a shelterbelt or native hedge. GMEU also recommend installation of bird boxes as mitigation for loss of nesting habitat. GMEU are amenable to options, for ecological mitigation to be incorporated outside the development site but within

the ownership of the applicant. The details can be conditioned.

Summary

The applicant has sought to address the reasons for the refusal of the previous application, and in doing so now proposes a relatively small scale stable development for personal usage. Siting, design and access issues have been suitably addressed in the application and the submission of information to address the ecology issues have been addressed to the satisfaction of GMEU.

It is therefore considered that the proposed development would be appropriate and compliant with the principles of the NPPF.

Response to objections

- The calculation of 140 sqm of floor space of the existing buildings provided by the applicant is correct.
- Opening hours are considered not to be relevant for a domestic and personal use. The development would not be of a significant scale to justify restriction of hours.
- Vehicles to the site would comprise the users personal car and trailer. Tractors are used to maintain the adjacent fields and do not form part of the application proposals or the application site.
- The site is currently an allotment with storage and shed buildings. The proposal for stables in the Green Belt is considered to be appropriate development in accordance with the principle of para 149 (b) of the NPPF.
- Drainage - A condition for the submission of drainage details would be included.
- A storage facility for waste would be provided next to the stables and collected regularly. This is also an Environmental Health issue should statutory nuisance arise.
- The use would be for individual and personal use only for two individuals which would be secured by condition. Breach of the condition would be subject to enforcement action.
- The existing access arrangements would be acceptable to serve the scale of the development.
- A condition for the submission of details of any lighting to be installed would be included.
- Any issues arising from vermin or rodents would be dealt with under separation legislation and the Environmental Protection Act .
- All other issues have been covered in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings - Revised site location plan 000 received 21/10/22; Existing site plan 002 rev A; Existing site plan 003 rev A, Proposed elevations 111 rev P1; Proposed roof plan 105 rev P1; Proposed elevations 110 rev P1; Proposed ground floor plan 104 rev P4; Proposed site plan 103 rev P5;

Proposed landscaping 106 rev P5 and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. Where during any works on site, unforeseen contamination is suspected or found, or contamination is caused, works on the site shall cease and the Local Planning Authority shall be notified immediately. The developer shall then produce a risk assessment and submit remediation proposals, if required, for approval to the Local Planning Authority. On approval of the remediation strategy, the development shall then be carried out in accordance with the approved details and process including any required timescales.
Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
4. Prior to any earthworks a method statement detailing eradication and/or control and/or avoidance measures for Himalayan Balsam or any other invasive species identified on site should be supplied to and agreed in writing to the LPA. The agreed method statement shall be adhered to and implemented in full.
Reason. The scheme does not provide full details of the actual extent of Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
5. No site clearance or demolition of buildings shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.
6. Prior to the above groundworks for the stable building, a landscaping scheme shall be submitted to, and approved by the Local Planning Authority. The contents of the plan could include native hedge planting and should include the provision of bird boxes as mitigation for loss of nesting habitat. The approved scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied/used or within the first available tree planting season,; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies EN1/2 - Townscape and Built Design, EN6/3 - Features of Ecological Value and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.
7. The stables hereby permitted shall be used for the domestic purposes only for no more than two individuals at any one time. They shall not be used in part or whole for any commercial livery, equestrian or other use.
Reason. In the interests of amenity having regard to the location of the site pursuant to Policies OL4/7 - Development Involving Horses and OL1/2 – New Buildings in the Green Belt of the Bury Unitary Development Plan.
8. Prior to the construction of any of the stables hereby approved, the 4 existing buildings shown on the existing site plan 002 rev A shall be permanently removed

from site.

Reason. In the interests of visual amenity pursuant to Bury Unitary Development Plan Policies OL1/2 - New Buildings in the Green Belt and EN1/1 - Visual Amenity.

9. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
- Photographic dilapidation survey of Public Footpath No. 21, Christ Church, Radcliffe leading to and abutting the site in the event that subsequent remedial works are required following construction of the development;
 - Access point(s) for construction traffic and all temporary works required to facilitate access for demolition/construction vehicles;
 - The provision, where necessary, of temporary pedestrian facilities/protection measures on Public Footpath No. 21, Christ Church, Radcliffe;
 - A scheme of appropriate warning/construction traffic speed signage in the vicinity of the site and its access;
 - Confirmation of hours of operation, delivery, construction and vehicle sizes that can be accommodated on the public right of way that serves the site, and number of vehicle movements;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site and/or measures to control/manage demolition/delivery vehicle manoeuvres;
 - Parking on site or on land within the applicant's control of operatives' and demolition/construction vehicles, together with storage on site of demolition/construction materials;
 - Measures to ensure that all mud and other loose materials are not spread onto the adjacent public right of way/adopted highways as a result of the groundworks operations or carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the demolition and construction periods and the measures shall be retained and facilities used for the intended purpose for the duration of the both periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials. All highway remedial works identified as a result of the dilapidation survey shall be implemented to the written satisfaction of the Local Planning Authority prior to the development hereby approved being brought into use.

Reason. Information not submitted at application stage. To maintain the integrity of the adopted highway, mitigate the impact of the demolition/construction traffic generated by the proposed development on the adjacent Public Right of Way, ensure adequate off street car parking provision and materials storage arrangements for the duration of the demolition and construction periods and ensure that the adjacent highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

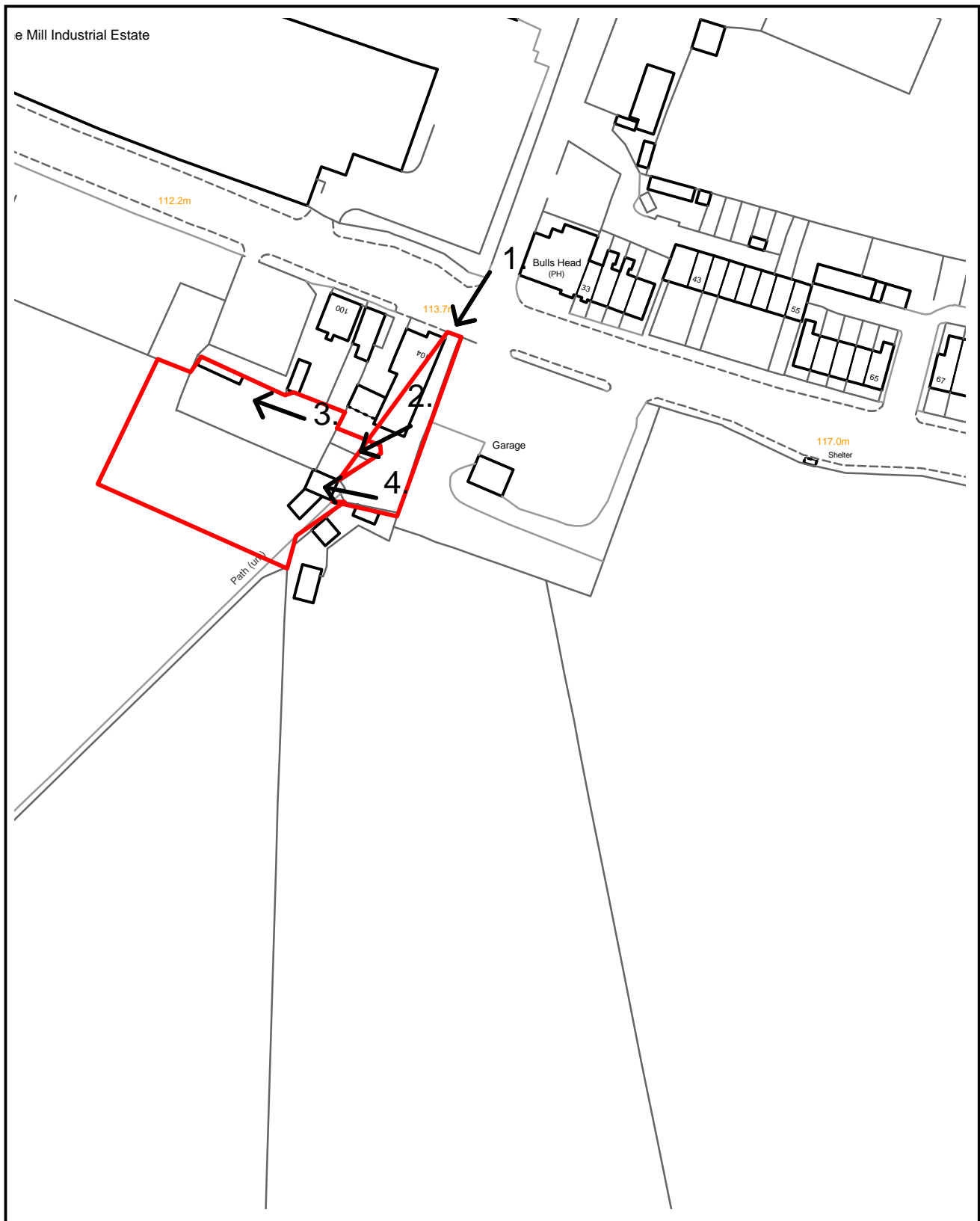
10. The vehicular access arrangements, turning facilities and provision of a new stile on the adjacent Public Footpath No. 21, Christ Church, Radcliffe, of a design to be agreed with the Highway Authority, indicated on the approved plans, shall be provided before the development is brought into use and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan

11. The car parking indicated on the approved plans, shall be surfaced, demarcated (as appropriate) and made available for use to the written satisfaction of the Local Planning Authority prior to the development hereby approved being brought into use and thereafter maintained at all times.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
12. The car park and turning area shown on the approved plans shall be surfaced in a grasscrete or similar surfacing and shall thereafter be maintained.
Reason. In the interests of visual amenity pursuant to Bury Unitary development Plan Policies EN1/1 - Visual Amenity and OL4/7 - Development Involving Horses.
13. No development shall commence unless and until surface water drainage proposals have been submitted to and approved in writing by the Local Planning Authority. The scheme should be in accordance with the submitted Surface Water Sustainable Drainage Assessment and must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented prior to first occupation and thereafter maintained.
Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact. To promote sustainable development and reduce flood risk pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
14. Prior to the installation of any lighting and associated lighting equipment on the site hereby approved, details shall be submitted to the Local Planning Authority for approval. Only the approved lighting and equipment shall be installed in association with the development.
Reason. To protect the amenities of nearby occupiers pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints - Item 7 - 68778



PLANNING APPLICATION LOCATION PLAN

APP. NO 68778

ADDRESS: Land to rear of 104 Bury New Road Radcliffe



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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68778 – Item 7 - Photos

Photo 1



Photo 2



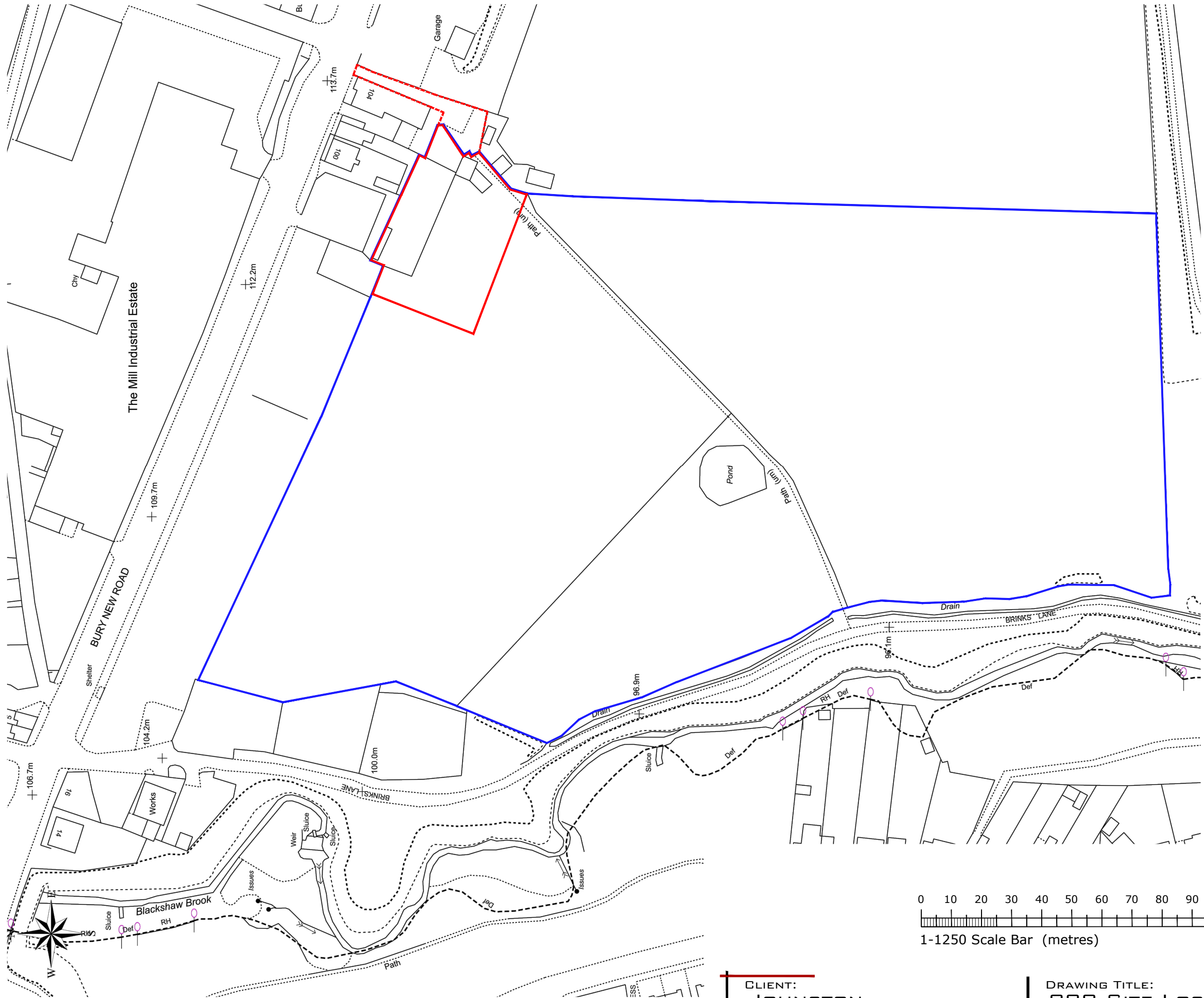
68778 – Item 7 - Photos

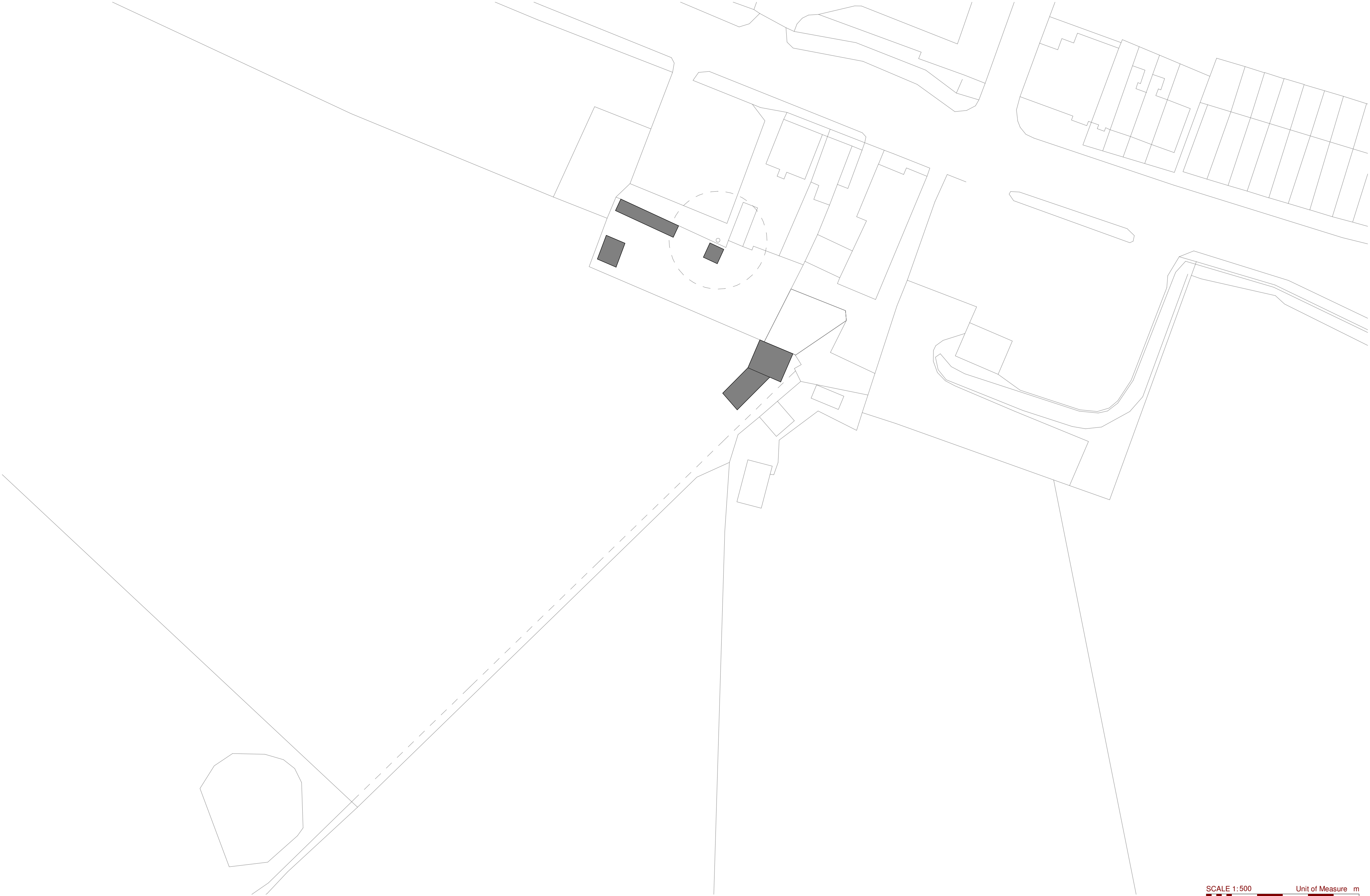
Photo 3



Photo 4







SCALE 1:500 Unit of Measure m
0 5 10 15 20 25

CLIENT
JOHNSTON

PROJECT NUMBER DRAWING SCALE:
21844 1 : 500@ A2

DRAWING NUMBER / TITLE
002 EXISTING SITE PLAN
PROJECT
LAND OFF 104 BURY NEW ROAD

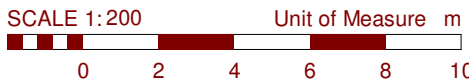
13/07/21 REV A - APPLICATION PACKAGE

RIBA 
Chartered Practice

WEBSITE: **WWW.SHWILKINSON.CO.UK**
EMAIL: **SCOTT@SHWILKINSON.CO.UK**
TEL: **01942 538 533**
MOB: **07809 373 066**

ADDRESS: **6 DALE LEE,
WESTHOUGHTON,
BOLTON,
BL5 3YE**

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Ward: Whitefield + Unsworth - Pilkington Park

Item 8

Applicant: Harrington House Estates Limited

Location: 79 Bury New Road, Whitefield, Manchester, M45 7EG

Proposal: Change of use from 6 bed HMO (Class C4) to 8 bed HMO (Sui Generis)

Application Ref: 68542/Full

Target Date: 16/09/2022

Recommendation: Approve with Conditions

This application was deferred at the meeting of the Planning Control Committee in November

in order to carry out a site visit.

Description

The application relates to a large red brick/slate end terraced property on the west side of Bury New Road, to the south of the Higher lane junction. The property was formerly a single house, it has undergone refurbishment and conversion to a 6 person House in Multiple Occupation (HMO) as permitted development. The site is the southernmost property within the terraced block of eight residential properties. It is noted that the property at the northern end of the block is also a HMO. To the west, across Bury New Road is the United Reformed Church and the block of flats known as Mayfair Gardens.

The main pedestrian entrance is from Bury New Road. Vehicular access to the unmade parking area at the side of the property is from the unmade/unadopted access road (Robin Lane) that runs around the back and side of the row of terraced properties (79-93 Bury New Road) and onto Bury New Road on the north side of the row. Robin Lane also allows access to the children's day nursery, situated to the rear of the site. The carriageway width along Robin Lane varies from around 5.5m, to the rear of the block and 8.5m to the side of No.93 at the northern end of the block. The width of the hardstanding immediately to the south of the property is 11.6m wide.

Bury New Road, to the front of the site, is a very busy stretch of road with parking restrictions and there are traffic lights directly in front of the property.

The proposed development involves:

- Change of use of the existing 6 person HMO into an 8 person HMO. The two additional bedrooms and kitchen/diner would be on the second floor, in the existing rooms within roofspace. The proposal also involves the creation of 11 parking spaces to the side of the property. It is proposed that 5 of these spaces would be for the residents of the HMO with the others available for neighbouring residents.

The supporting statement, submitted with the application, states that the HMO is licensed for 6 people and is currently occupied. It has confirmed that three of the residents have cars which is a 50% car ownership level. In addition to the supporting statement, a Highways Technical Note has been submitted, specifically addressing the issues relating to access and parking.

Relevant Planning History

64719 - Change of use from existing 6 bed HMO (Class C4) to 8 bed (8 person) house in multiple occupation (HMO) (Sui Generis) - 10/03/2020

61596 - Conversion and extensions to existing dwelling house to form 8 bed House in Multiple Occupation (HMO) - Refused 28/07/2017

64719 - Change of use from existing 6 bed HMO (Class C4) to 8 bed (8 person) house in

Publicity

Thirty four neighbours were notified by letter dated 25/07/2022.

Thirteen objections received from 11 properties and summarised below:

- The previous application was refused and the subsequent appeal dismissed. The concerns of the Planning Inspector are still relevant.
- The proposal will intensify the use of the property and lead to increased traffic generation and pressures on parking around the site.
- There is no control over how many cars a future tenant may have. Two additional units may mean 4 extra cars.
- There would be increase service/delivery vehicles.
- The unmade access road from Bury new Road is not suitable to carry more vehicles.
- With cars parked on Robin Lane, vehicles may not be able to turn and would have to reverse onto Bury New Road.
- The junction onto Bury New Road is very dangerous and more vehicles would be detrimental to highway safety.
- Vehicles along this part of Bury new Road travel at high speed, making turning very hazardous.
- There have been traffic incidents - the railings next to Besses Church on Bury Old Road at the approach to the junction, were knocked flat leaving a huge quantity of debris. This can only have been caused by a vehicle approaching the corner at a very high speed and I refer to my previous observations of vehicles going at circa 60 mph. In addition, a vehicle coming onto Bury New Road from Bury Old Road went into the railings outside number 91.
- Not all traffic accidents have resulted in hospitalization or being reported.

Representation received from Cllr Fitzgerald.

- Concerned that having 8 separate homes in this building could lead to poor and cramped housing.
- Not against HMOs and understand the issues around housing in the UK today, but remain concerned that squeezing another two homes into a building that is already occupied to a much higher level than most in Whitefield isn't going to solve the Housing crisis and just risks a race to the bottom on the quality of Housing.
- I do note that in general the landlord has improved the building in recent years with new windows, etc.
- Concerned about cars accessing the main road. The road and junction where the exit would be is quite unusual and dangerous.
- Cars merge into separate lanes from the motorway and Bury Old Road and across to either Higher Lane or to continue along Bury New Road.
- There are barriers there for a reason because of the number of incidents of cars going into houses and the Council spent a lot of money a few years ago on lights and a pedestrian crossing at Chestnut because of these issues.
- The loss of the turning circle and risk of residents needing to back out at this access point is extremely dangerous and I am pretty sure against the Highway code, i.e., you aren't supposed to back out onto a road.
- Reject this on the basis of the previous applications that have been rejected in Bury and the appeal that was rejected by the Planning Inspectorate. I have looked at the comments in the rejected appeal and it supports what I have outlined above: *'The lane appears to have a limited amount of parking and turning space. Based on the evidence before me, an increase in parking demand from occupiers and visitors could lead to circumstances where vehicles reverse onto Bury New Road to the detriment of highway safety in the road'*.
- There doesn't appear to be any regulatory changes just that the applicant has found someone who has said that the Planning Inspector was wrong.
- Has anything materially changed in the regulations as traffic continues to increase so I would think this would mean it is less likely the application should be granted?
- Over the period that the landlord has owned the property there have been repeated

attempts to increase the number of dwellings in the building and there isn't anything new that they can offer in this application other than hiring a consultant.

Those making representations have been notified of the Planning Control Committee meeting.

Statutory/Non-Statutory Consultations

Traffic Section - No objection.

Environmental Health - No objection.

Waste Management - No objection

Greater Manchester Police - No objection.

Pre-start Conditions - Applicant/Agent has agreed with pre-start conditions.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/4	Conversions
EN1/2	Townscape and Built Design
EN7	Pollution Control
HT2/4	Car Parking and New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework
SPD13	Conversion of Buildings to Houses in Multiple Occupation

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - The conversion of a house to a House in Multiple Occupation is acceptable in principle, highlighted by the fact that the property was previously converted to a 6-person HMO under 'permitted development' earlier this year.

This application seeks permission to allow accommodation for 2 further residents on the second floor and as such the main issue is what impact the impact of the additional accommodation would have in planning terms.

The National Planning Policy Framework (NPPF) sets out the governments objective of significantly boosting the supply of homes in all kinds of size and tenures. With regard to HMOs, the conversion of large houses into flats or bedsits can make an important contribution to the local housing stock and can utilise vacant or underused space more efficiently.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is within an existing HMO, located within the urban area and adjacent to

residential uses. The proposed development would not conflict with the surrounding land uses and would be located in a relatively sustainable location with access to public transport and services. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

However, it is recognised that the conversion of buildings to HMOs can, in some cases, raise issues such as parking, residential amenity and impact on character and appearance of an area.

Appeal Decision relating to 64719 (APP/T4210/W/19/3242673) - The previous appeal decision, dated 10th March 2020 is summarised below:

- The main issue is the effect of the proposal on highway safety in Bury New Road with particular reference to vehicular parking.
- Although Bury New Road is relatively straight, visibility is restricted to the south. Exiting from the access lane in a forward gear appears safe but any drivers reversing out of the lane would have poor visibility southwards and limited warning of approaching vehicles. Such manoeuvres would lead to unacceptable highway safety risks.
- No parking or turning layout for the lane was provided. From observations it did appear possible to park, turn and manoeuvre within the wider areas, which would allow vehicles to exit safely from the lane in a forward gear.
- Whilst there were still spaces available at the time of the site visit, third-party representations raised concerns regarding the level of parking, particularly in the evenings.
- The Council do not have any specific parking standards for HMO's. At present the property is occupied by 6 individuals and the appellant advises that 3 of the tenants have cars and in the area car ownership for HMO tenants is expected to be around 33%. It is therefore put to me that the proposal for 2 additional individuals would likely result in an additional car at the appeal site.
- Car ownership cannot normally be controlled through planning and in a scenario where all residents would have a car, it would seem unlikely, based on observations and the evidence that all vehicles could be parked along the lane in combination with cars associated with the other properties.
- With increased car ownership there is the potential for inappropriate parking that would impede the turning and manoeuvring of vehicles in the lane and could result in vehicles reversing out into the highway due to a lack of available space. Any increase in occupancy levels has the potential to incrementally breach the capacity for parking and turning within the lane. In the absence of substantive evidence to the contrary, this would not be conducive to securing good levels of highway safety.
- The proposed 8-bedroom HMO has the potential to generate a greater demand for car parking than both a family dwelling and the existing 6-bedroom HMO.
- The location of the site and its proximity to shops, facilities and public transport links, means there is potential for occupiers to not need their own cars and some residents may cycle. However, the site equally has good road links for potential occupiers that own a car.
- In conclusion, the lane appears to have a limited amount of parking and turning space. Based on the evidence available, an increase in parking demand from occupiers and visitors could lead to circumstances where vehicles reverse onto Bury New Road to the detriment of highway safety in the road.
- The proposal would be contrary to the requirements of policy HT2/4 and H2/4 of the Bury Unitary Development Plan (adopted 1997) and the Conversion of Buildings to Houses in Multiple Occupation Supplementary Planning Document (adopted 2007). These require, amongst other things, that applications make adequate provision for their car parking and servicing requirements. The proposal would also be contrary to paragraph 109 of the National Planning Policy Framework (the Framework) which seeks to ensure that development does not have an unacceptable impact on road safety.
- It is recognised that the appellant is seeking to make an effective use of an existing property, an objective which is encouraged by the Framework. The site is sustain ably

located with good access to services and facilities. The proposal would increase the diversity of housing supply, meeting a need. The appellant has invested in renovating the property, improving its appearance in the street scene. These are all positive factors in favour of the scheme. However, they do not outweigh the harm identified.

Design and layout/visual amenity - Policy H2/4 states that applications for the conversion of a building into a house of multiple occupation will be considered with regard to the following:

- the effect on the amenity of neighbouring properties through noise, visual intrusion, the position of entrances, impact of parking areas, extensions and fire escapes;
- the general character of the area and the existing concentration of flats;
- the amenity of occupants;
- the effect on the streetscene of any changes to the external appearance of the building;
- car parking and servicing requirements.

Given that the proposal involves only the addition of two bedsits within the existing building without any changes in design or external appearance of the building, there are no significant visual amenity issues arising. Indeed the refurbishment that has been undertaken at the property has significantly improved its appearance within the streetscape and as such would be in accordance with Policies EN1/2 and H2/4 of the Bury Unitary Development Plan.

Impact upon Residential Amenity - As existing windows, facing the road to the front and yard to the rear, would be utilised, there would be no significant overlooking beyond the existing situation.

With regard to noise outbreak, appropriate sound insulation to the party wall, required by building regulations, is in place. The proposed development would not have an adverse impact upon the residential amenity of the neighbouring properties and would be in accordance with UDP Policy.

Highways Technical Note - The application is accompanied by a 'Highways Technical Note' by Axis. This document sets out the justification for the proposal in terms of access and parking. The document is available to be read in full on the Councils website but the main points of the document are summarised:

- PIA data has been analysed using Crashmap which indicates that there is no evidence of an existing highway safety issue within the general vicinity of the site.
- The site benefits from a high level of accessibility by the main non-car modes of transport, being within walking distance of a number of amenities, within cycling distance of various settlements via high quality routes and having good access to bus and tram services.
- In accordance with the parking arrangements associated with the existing site, it is proposed that car parking associated with the two additional HMO bedrooms will be accommodated along Robin Lane, which can be accessed via Bury New Road.
- A review of 2011 Census data on local car ownership levels, and the level of car occupancy at the existing site and of other similar sites contained within the TRICS Database indicates that the proposed development is anticipated to generate an additional car parking demand of 2 spaces in the worst case scenario.
- Based on the results of a parking beat survey that was undertaken along Robin Lane, it has been demonstrated that there is a minimum spare capacity for 9 cars to park along Robin Lane which is more than sufficient to accommodate the anticipated additional parking demand of the proposed development, in addition to occasional spikes in activity that might occur.
- Furthermore, it has also been demonstrated that this level of parking can be accommodated along Robin Lane whilst also allowing a large car to safely and satisfactorily access, manoeuvre and egress Robin Lane in a forward gear.
- The proposed development is forecast to result in an additional 3 two-way trips per day, on average, with no additional trips forecast to take place during the weekday AM

- and PM peak hours.
- Paragraph 111 of the National Planning Policy Framework states that: “Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe”.
- Based on the evidence presented in this TN, no unacceptable impact would be created by the development and the application is therefore commended for approval.

Highway and Parking - The previous planning application for the change of use to an 8 person HMO was refused in November 2019 on the following grounds:
The lack of dedicated off-road parking provision is likely to lead to vehicles parking and carrying out manoeuvres on the highway to the serious detriment of the free flow of traffic and road safety. The proposal is therefore unacceptable and would be contrary to the NPPF and UDP Policies HT2/4 Car Parking and New Development and H2/4 Conversions and supplementary guidance document SPD13 Conversion of Buildings to Houses in Multiple Occupation.

Since the previous dismissal of the appeal in 2020, the applicant has acquired an additional land strip of land within the existing informal parking area running parallel to the southern boundary of the property and within the red edged site. Within this strip of land, it is proposed to mark out 11 parking spaces at ninety degrees to the gable/side wall. These spaces would be available for use by the residents of the HMO and any surplus spaces, by local residents.

There are no specific car parking standards for HMO's in SPD11 Parking Standards in Bury, but SPD 13 The Conversion of Buildings to Houses in Multiple Occupation gives some general advice. It states that parking and road safety issues will be important considerations when assessing a planning application and any proposal that is considered to have a detrimental impact on highway safety or harm to amenity will not be permitted. Car parking provision should meet the requirements of the likely occupants and, where possible, should be provided off-street.

In terms of car ownership the supporting statement, submitted with the current application indicates that, of the 6 current residents, 3 have cars. On this basis, it is possible that there could be a further car generated by the additional two persons. Whilst we would not be able to reasonably control car ownership, it would be reasonable to suggest that, in accordance with general guidance, there would be a need for one additional space. However, even if car ownership was greater, the 11 spaces indicated within the site, would be considered more than adequate to accommodate additional vehicles.

The revised site plan indicates that along with the parking area to the side of the property, there would be room for vehicles to turn at the end of Robin Lane so they could exit onto Bury New Road in forward gear.

Notwithstanding the issues specifically relating to parking, the site is considered to be very sustainable with excellent public transport links. Bury New Road and nearby Bury Old Road are main roads through the Borough with bus routes and there is a metrolink station at Besses O' th' Barn, 300m away to the east. There is also a cycle store located in the rear yard.

It is considered that the revised proposals and the points made in the Highways Technical Note, submitted with the application, go a significant way to assuage the concerns with regard to highway safety raised by the Council and the Planning Inspector. The additional parking area that would be formally demarcated at the side of the property is sufficient to accommodate parking for residents of the HMO and neighbours and it has been demonstrated that there would be appropriate turning on Robin Lane to allow vehicles to exist onto Bury New Road in forward gear.

The Council's Traffic Team have no objection to the proposal. Given its sustainable

location, the proposal is acceptable and would comply to the NPPF and UDP Policies HT2/4 Car Parking and New Development and H2/4 Conversions and associated SPD11 Parking Standards in Bury and SPD13 Conversion of Buildings to Houses in Multiple Occupation.

December 2022 Report Update - The applicant's statement with supporting photographs, addressing objections raised can be viewed via the planning file on-line.

The applicant has also provided a website link to videos, indicating how vehicles can access the parking area via Robin Lane, turn and exit back onto Bury New Road. This link is available via the planning application file on the Council website.

Ownership - The application includes Certificate A which states that the applicant owns all land within the red edged site. The agent has also confirmed ownership of the site by the applicant.

Objections.

The issues raised by objectors have been addressed in the main report. Given the current car ownership levels within the HMO, the availability of car parking and the reasonable expectation of there being a turning area available on Robin Lane, it is considered that vehicles would be able to exit safely from Robin Lane onto Bury New Road in forward gear. the traffic generated by the two additional residents would be so great as to cause undue parking problems and be detrimental to highway safety. It is considered that the noise and disturbance issues raised by the neighbour have been addressed by work carried out to the party walls

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

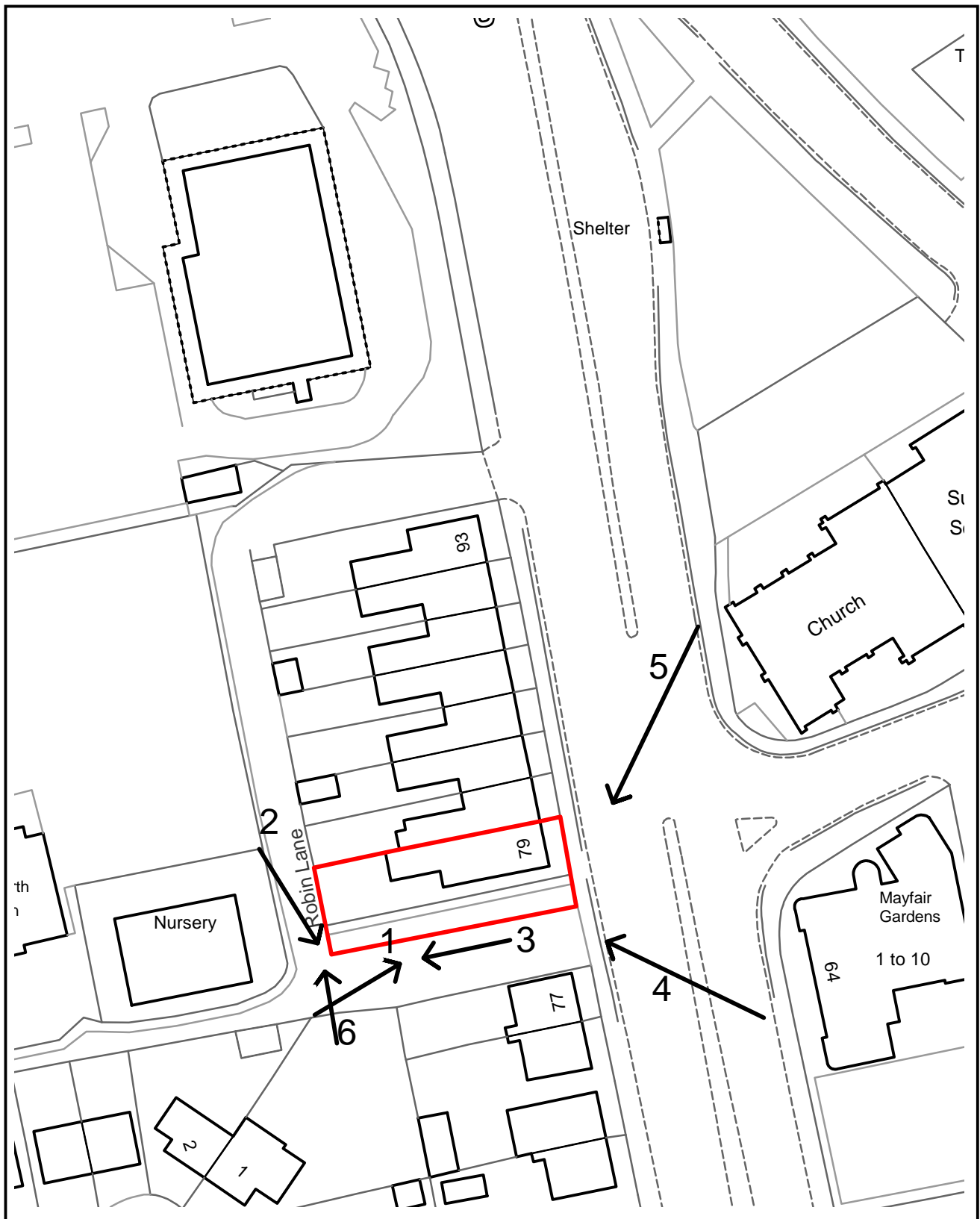
1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered Revised Site Plan P01, 079/02, 03, 04 and 05 and Site Plan with Swept Path analysis, and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The car parking indicated on the approved plans shall be demarcated and made permanently available for use prior to the additional residential units hereby approved being brought into use.
Reason. To ensure adequate off-street car parking provision in the interests of road safety pursuant to policy HT2/4 Car Parking and New Development of the Bury Unitary Development Plan.
4. Prior to the use hereby approved commencing, details of the proposed 'low level barrier' as indicated on the site plan, shall be submitted to the Local Planning

Authority for approval. The barrier shall be positioned to ensure that pedestrian access onto Bury New Road adjacent to No.79 and access to the adjacent footway chamber is maintained. The barrier shall be completed prior to the additional residential units hereby approved being brought into use and retained thereafter.

Reason. To prevent encroachment of parked cars onto the public footway in the interests of road safety pursuant to policy HT2/4 Car Parking and New Development of the Bury Unitary Development Plan.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 68542

ADDRESS: 79 Bury New Road, Whitefield



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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68542

Photo 1



Photo 2



68542

Photo 3



Photo 4



68542

Photo 5

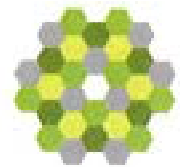


Photo 6

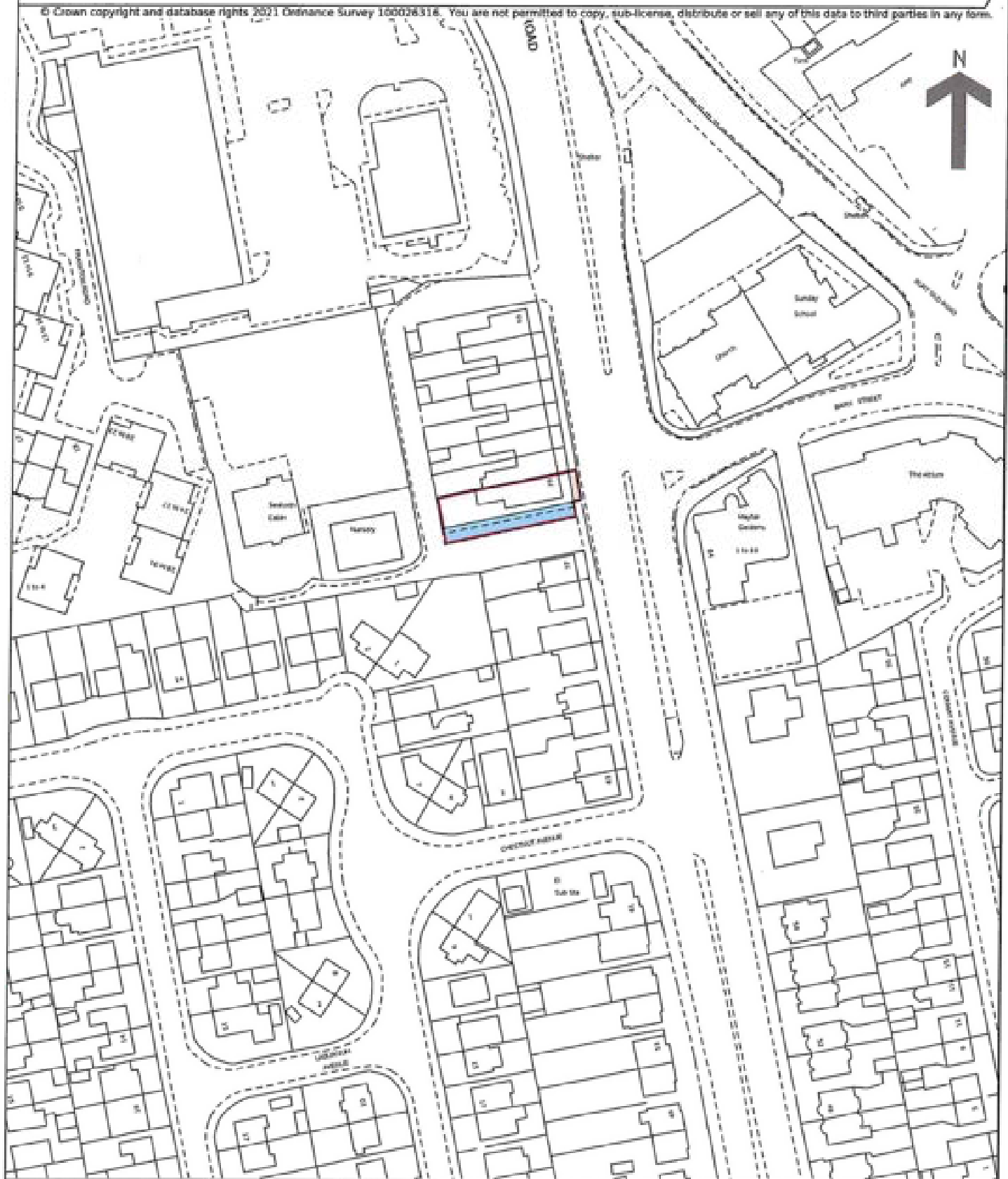


HM Land Registry
Official copy of
title plan

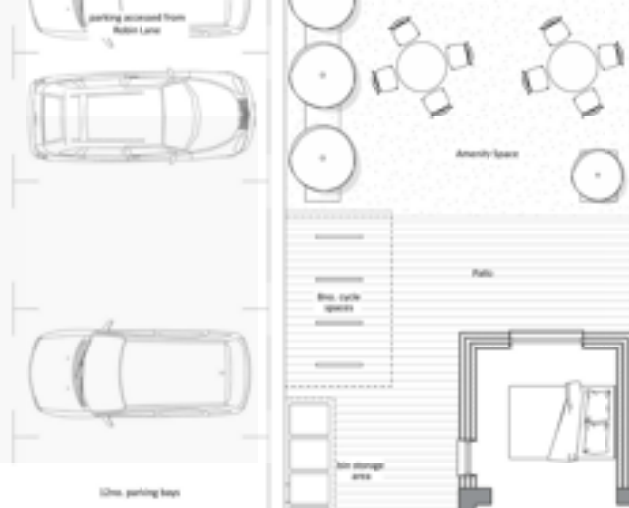
Title number **GM393369**
Ordnance Survey map reference **SD8005SE**
Scale **1:1250**
Administrative area **Greater Manchester :**
Bury



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NOTE:
ALL DIMENSIONS ARE TO BE CHECKED
ON SITE PRIOR TO THE START OF WORKS.
IF ANY DISCREPANCIES ARE TO BE FOUND
THEN THIS OFFICE IS TO BE NOTIFIED AS
SOON AS POSSIBLE.



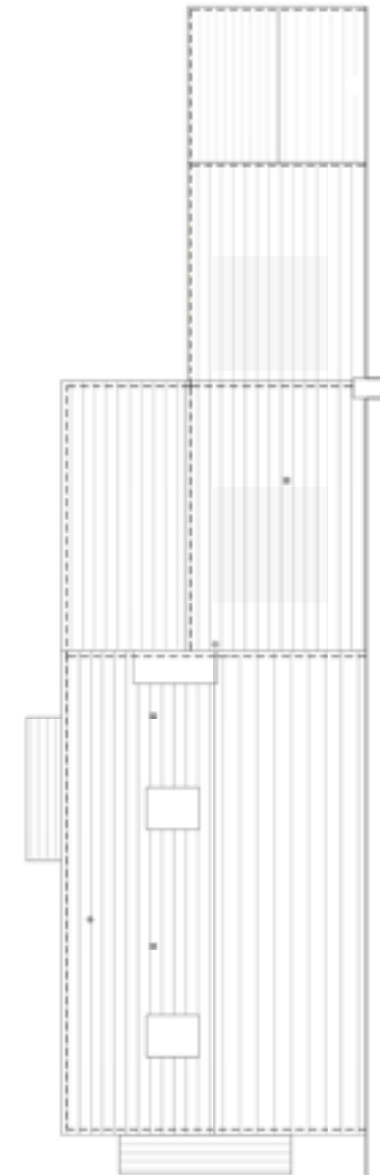
GROUND FLOOR



FIRST FLOOR



SECOND FLOOR



ROOF PLAN

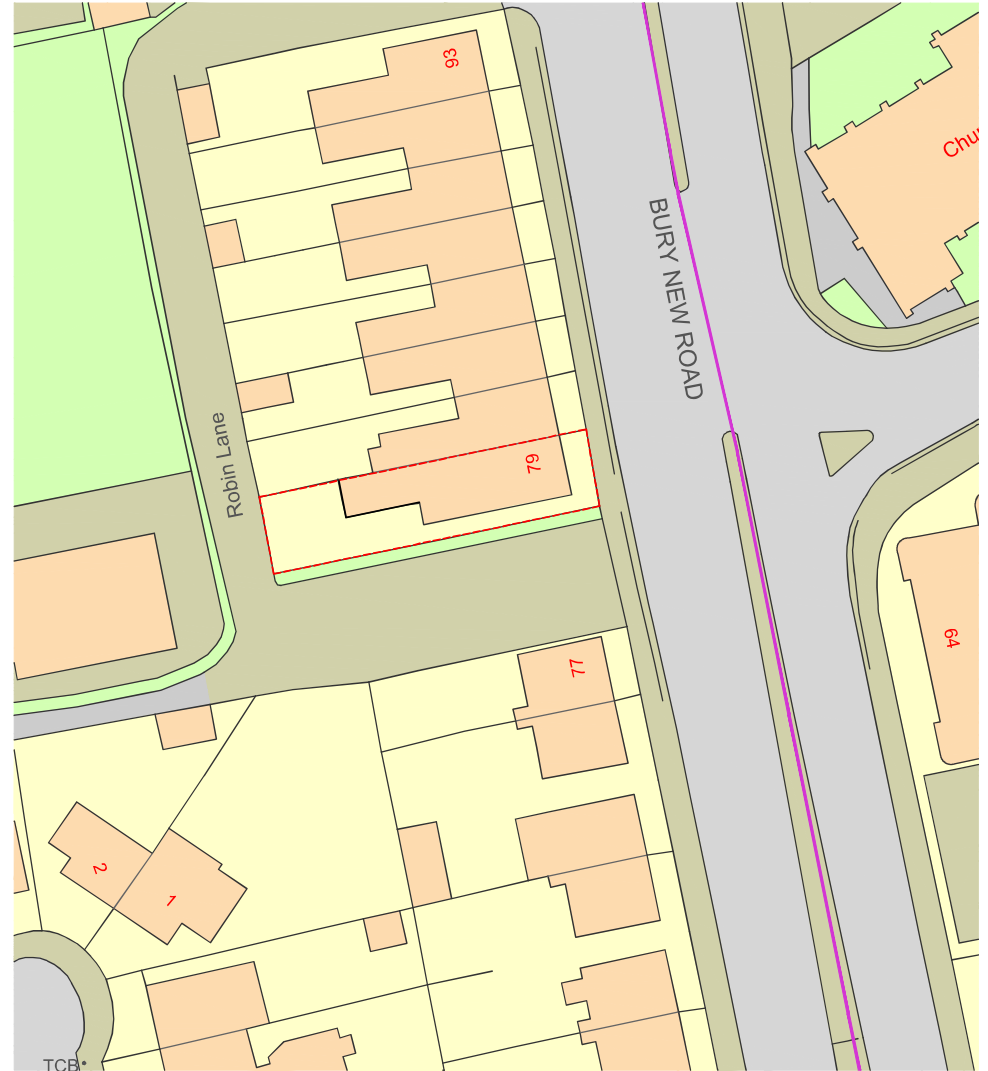
1m 0 1 2 3m



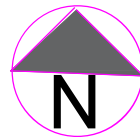
Author	Tom	Designer
Rev	001/01	Planning and/or other information
Project Proposed Extension & Change of Use		
Architect Ref: 101		
Site Proposed Plan	Client & Address	
Scale 1:500	Drawing Number	
Date 10/01/12	Drawing Number	



Location Plan
1:1250



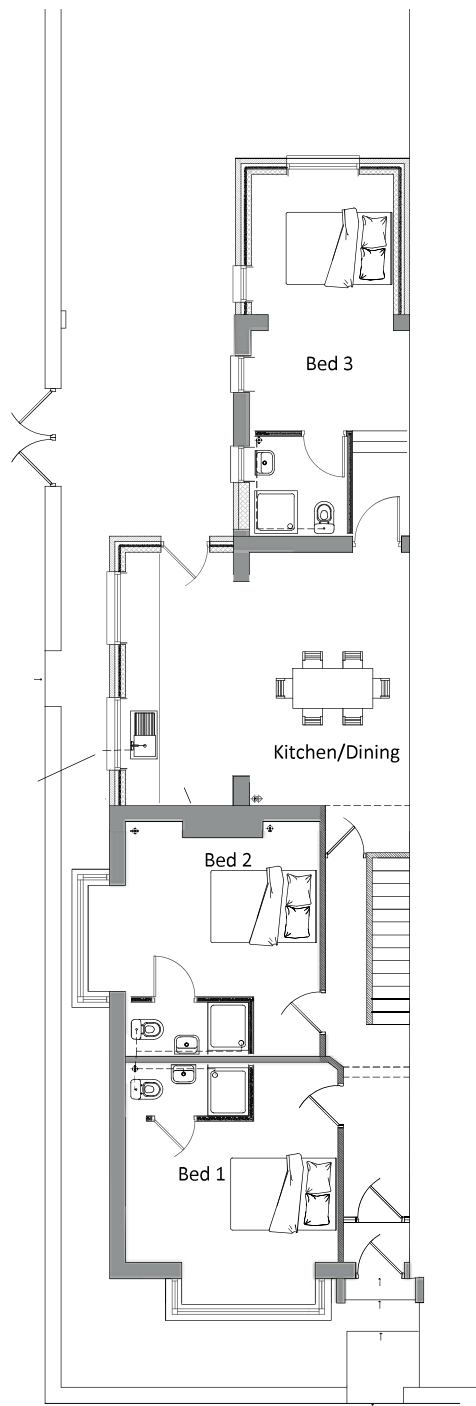
Site Plan
1:500



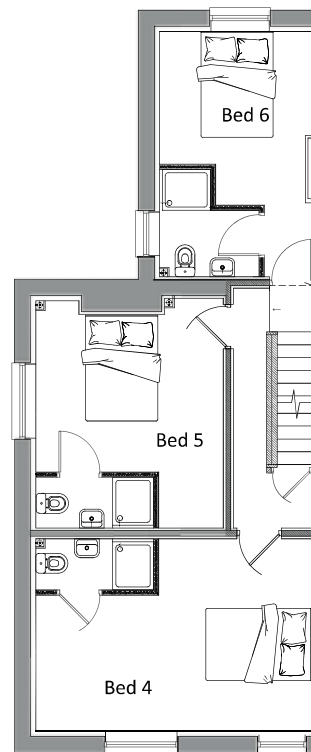
NOTE
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Revision	Date	Description

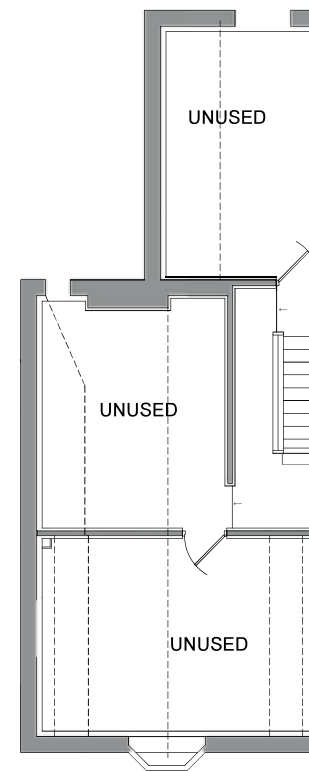
Project 79 Bury New Road Whitefield M45 TEG		
Title Location/ Site Plan	Client N Mackey	
Scale 1:1250@A3	Drawing Number 079-01	
Date SEPT 19		



GROUND FLOOR



FIRST FLOOR



SECOND FLOOR

NOTE
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SOON AS POSSIBLE

Revision	Date	Description

Project 79 Bury New Road Whitefield M45 7EG		
Title Existing Plans		Client N Mackey
Scale 1:50@A1		Drawing Number 079-02
Date SEP 19		

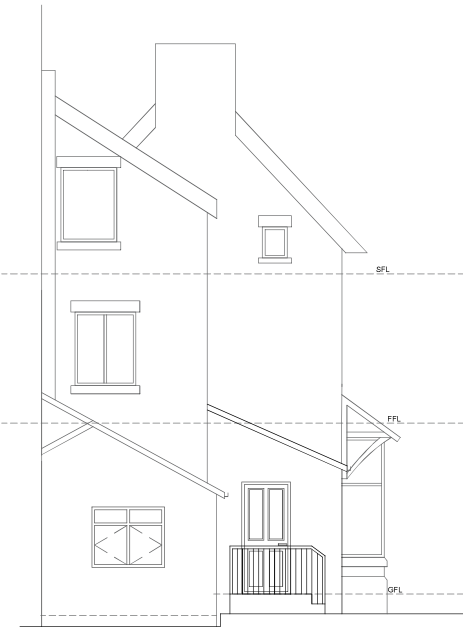




SIDE ELEVATION



FRONT ELEVATION



REAR ELEVATION

NOTE
ALL DIMENSIONS ARE TO BE CHECKED
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SOON AS POSSIBLE



Revision	Date	Description

Project	
79 Dury New Road Whitefield M45 7EG	
Title Elevations	Client R Mackey
Scale 1:50@A1	Drawing Number 070-05
Date SEPT 19	



SIDE ELEVATION



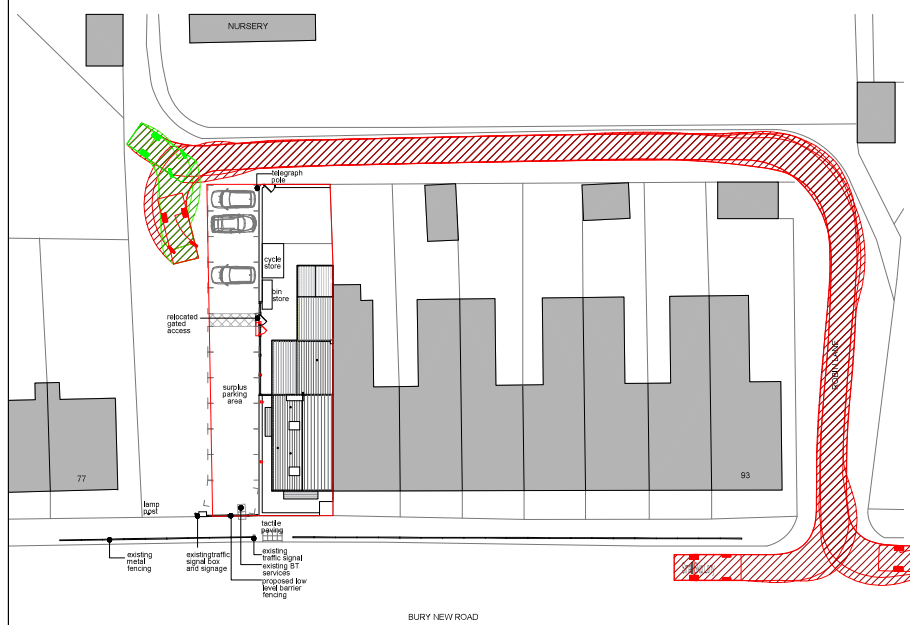
FRONT ELEVATION

REAR ELEVATION

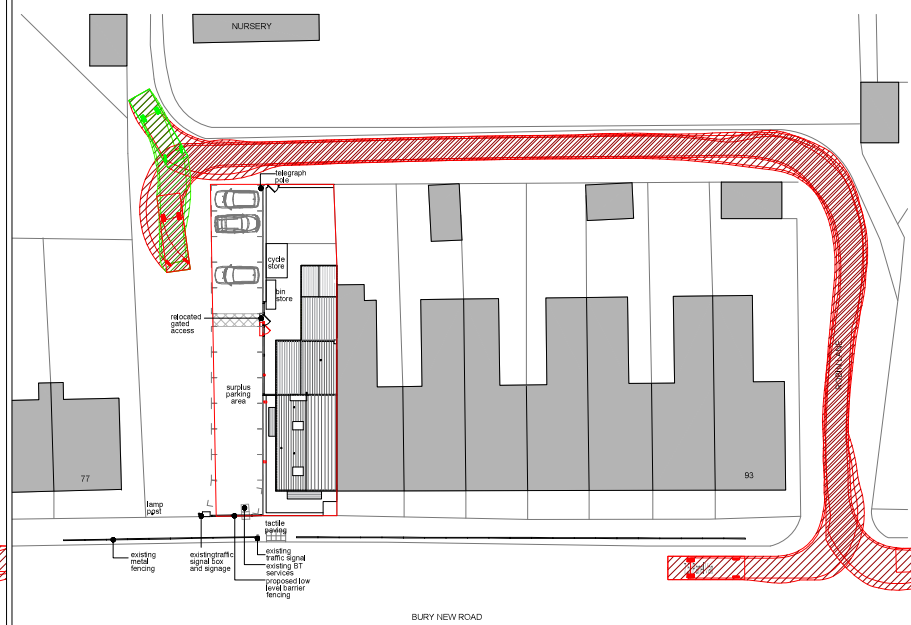
NOTE
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Revision	Date	Description
Project		
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Title EXISTING ELEVATIONS		Client H Mackey
Scale 1:50@A1	Drawing Number 070-03	
Date SEPT 19		

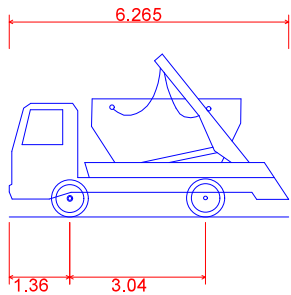


SWEPT PATH ANALYSIS - SKIP LORRY



SWEPT PATH ANALYSIS - 7.5T VAN

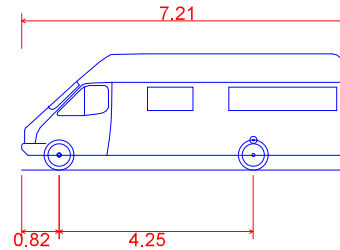
Page 211



Small Skip Lorry
Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Max Track Width
Lock to lock time
Kerb to Kerb Turning Radius

6.265m
2.390m
3.650m
0.396m
2.435m
6.00s
6.340m

PROFILE OF VEHICLE USED IN SWEPT PATH ANALYSIS - SKIP LORRY



7.5t Panel Van
Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock to lock time
Kerb to Kerb Turning Radius

7.210m
2.192m
2.544m
0.316m
1.865m
4.00s
7.400m

PROFILE OF VEHICLE USED IN SWEPT PATH ANALYSIS - 7.5T VAN

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Revision History	Date

Client: HARRINGTON HOUSE ESTATES LIMITED	axis
Project: PROPOSED EXTENSION & CHANGE OF USE, WHITEFIELD, BURY	
Drawing title: SWEPT PATH ANALYSIS	
Date: 20-10-2022	Drawn by: AD
Drawing number: 3186-01-ATRO2	Status: rev
Scale: 1:250@A3	
Planning environment design	

NURSERY

Notes:
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 All dimensions are in millimetres unless otherwise noted.
 All dimensions to be checked on site before proceeding with work.
 All omissions and discrepancies to be reported in writing to Project 3 Architects Ltd.
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KEY

— site boundary

P01 12/10/22 Planning ED
 Rev: Date: Description: By:

5 m 0 5 10 m



PROJECT3
 architects

4 back griffin street
 altrincham
 cheshire
 WA14 1DY

0161 233 0575
 mail@project3architects.com
 www.project3architects.com

Project:
Proposed Extension & Change of Use
Whitfield, M4 7EG
 Title:
Proposed Site Plan

Status: **PLANNING** Scale @ ISO A3: 1:250
 Drawing Number: 0742-P3A-XX-XX-DR-A-00101 Revision: P01

77

93

ROBIN LANE

BURY NEW ROAD

telegraph pole
 cycle store
 bin store
 relocated gated access
 surplus parking area
 lamp post
 existing metal fencing
 existing traffic signal box and signage
 tactile paving
 existing traffic signal
 existing BT services
 proposed low level barrier fencing

REPORT FOR NOTING

Agenda Item 5

Bury
COUNCIL

**Agenda
Item**

5

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	13 December 2022
SUBJECT:	DELEGATED DECISIONS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	The report lists: Recent delegated planning decisions since the last PCC
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A
Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS**DIRECTOR:**

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Planning Control Committee of the delegated planning decisions made by the officers of the Council.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-None**Contact Details:-**

David Marno, Head of Development Management
Planning Services, Department for Resources and Regulation
3 Knowsley Place
Bury BL9 0EJ

Tel: 0161 253 5291

Email: d.marno@bury.gov.uk

Planning applications decided using Delegated Powers

Between 31/10/2022 and 04/12/2022



Ward: **Bury East**

Application No.:	68639	App. Type:	FUL	01/11/2022	Approve with Conditions
Location:	40 Topping Fold Road, Bury, BL9 7NG				
Proposal:	Change of use of first and second floors from Office/Storage (Class E) to 1no. Apartment (Class C3)				
Application No.:	68724	App. Type:	ADV	16/11/2022	Approve with Conditions
Location:	237 Wash Lane, Bury, BL9 7DH				
Proposal:	Retention of 1no. non-illuminated top sign and 1no. non-illuminated logo panel				
Application No.:	68757	App. Type:	FUL	28/11/2022	Approve with Conditions
Location:	8 The Rock, Bury, BL9 0NT				
Proposal:	Change of use from education centre (Class F1) to licensed dessert café/restaurant (Class Eb)				
Application No.:	68776	App. Type:	ADV	11/11/2022	Approve with Conditions
Location:	103 The Rock, Bury, BL9 0NB				
Proposal:	1 No. externally illuminated fascia sign				
Application No.:	68845	App. Type:	FUL	22/11/2022	Approve with Conditions
Location:	33 Knowsley Street, Bury, BL9 0ST				
Proposal:	Change of use from office (Class E) to residential (Class C3)				
Application No.:	68853	App. Type:	FUL	17/11/2022	Approve with Conditions
Location:	St Johns House, 155-163 The Rock, Bury, BL9 0ND				
Proposal:	Refurbishment to ground, first, second and third floor office space including ground floor windows and new entrance				
Application No.:	68857	App. Type:	GPDE	11/11/2022	Prior Approval Required & Granted - Ext
Location:	34 Irwell Street, Bury, BL9 0HE				
Proposal:	Prior approval for proposed single storey rear extension				
Application No.:	68862	App. Type:	FUL	22/11/2022	Approve with Conditions
Location:	2 Bridgefield Mews, Bridgefield Drive, Bury, BL9 7PE				
Proposal:	Single storey extension at rear with raised patio, glazed ballustrade and steps to garden level.				
Application No.:	68942	App. Type:	FUL	22/11/2022	Approve with Conditions
Location:	Trinity Street car park, Trinity Street, Bury, BL9 0BT				
Proposal:	Installation of 2no. rapid Taxi/PHV Electric Vehicle chargers and relocate and upgrade the existing fast charger on site.				

Application No.: 68943 **App. Type:** FUL 22/11/2022 Approve with Conditions
Location: Foundry Street Car Park, Foundry Street, Bury, BL9 7AZ
Proposal: Installation of 2no. Rapid Taxi Electric Vehicle chargers and upgrade current fast charger on site

Application No.: 68966 **App. Type:** ADV 30/11/2022 Approve with Conditions
Location: Unit 27, 2 Central Street, Bury, BL9 0JN
Proposal: 1 no. internally illuminated fascia sign; 1 no. vinyl graphic on existing projected sign; 3 no. Vinyl graphic signs applied to existing external windows; 6 no. vinyl window decals applied to existing external windows

Ward: **Bury East - Moorside**

Application No.: 68685 **App. Type:** P3JPA 16/11/2022 Prior Approval Required and Refused
Location: Unit 2, Mossfield House, Chesham Fold Road, Bury, BL9 6JZ
Proposal: Prior approval for proposed change of use of existing offices (Class E) to 7 no. flats (Class C3)

Application No.: 68870 **App. Type:** ADV 01/12/2022 Approve with Conditions
Location: MKM Building Supplies, 2 Chamberhall Business Park, Chamberhall Green, Bury, BL9 0AP
Proposal: 13 no. non-illuminated fascia signs and 5 no. internally illuminated fascia signs

Application No.: 68905 **App. Type:** LDCP 22/11/2022 Lawful Development
Location: 296 Walmersley Road, Bury, BL9 6NH
Proposal: Lawful development certificate for proposed demolition of existing garage and construction of new outbuilding

Application No.: 68909 **App. Type:** FUL 14/11/2022 Approve with Conditions
Location: 302 Hornby Street, Bury, BL9 5DS
Proposal: Two/single storey rear extension, Hip to gable roof extension at side with loft conversion and rear dormer; New windows to side (north) elevation.

Ward: **Bury East - Redvales**

Application No.: 68591 **App. Type:** FUL 02/11/2022 Approve with Conditions
Location: Bury C of E High School, Haslam Brow, Bury, BL9 0TS
Proposal: Creation of additional vehicle parking area to provide 29 additional spaces and erection of 2.4m - 3m high fencing

Application No.: 68720 **App. Type:** FUL 07/11/2022 Approve with Conditions
Location: 8 Somerset Drive, Bury, BL9 9DG
Proposal: First Floor Rear Extension

Application No.: 68729 **App. Type:** FUL 11/11/2022 Refused
Location: 2 Orchid Drive, Bury, BL9 9EL
Proposal: Front porch; Two storey side extension; Single storey rear extension

Application No.:	68738	App. Type:	FUL	24/11/2022	Approve with Conditions
Location:	Cellnet Building, Dumers Lane, Bury, BL9 9QL				
Proposal:	Change of use of from call centre office (Class E) to a flexible use comprising of a warehouse distribution centre (Class B2/B8) with ancillary offices; Creation of new site access/entrance with sliding security gates, service yard and vehicle/cycle parking; External alterations including new loading/delivery doors to north elevation				

Application No.:	68801	App. Type:	ADV	10/11/2022	Approve with Conditions
Location:	Bury College, Market Street, Bury, BL9 0BG				
Proposal:	3 No. internally illuminated fascia signs and 1 no. free standing internally illuminated double sided totem sign; Letters sign above entrance				

Application No.:	68848	App. Type:	FUL	30/11/2022	Approve with Conditions
Location:	12 Cardigan Drive, Bury, BL9 9LG				
Proposal:	Part single/Part two storey rear extension				

Ward: **Bury West - Elton**

Application No.:	68752	App. Type:	FUL	17/11/2022	Approve with Conditions
Location:	20 Sheringham Drive, Bury, BL8 1HG				
Proposal:	First floor side extension				

Application No.:	68914	App. Type:	FUL	28/11/2022	Approve with Conditions
Location:	3 Hayling Close, Bury, BL8 1GS				
Proposal:	Two storey extension at side and rear elevational changes				

Application No.:	68951	App. Type:	FUL	01/12/2022	Approve with Conditions
Location:	10 Arundel Close, Bury, BL8 1YB				
Proposal:	Two storey side/front extension; Single storey rear extension and external alterations				

Ward: **Bury West - West**

Application No.:	68606	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	165 Bolton Road, Bury, BL8 2NW				
Proposal:	Alterations to shop front/installation of roller shutters and subdivision of 1 no. shop into 2 no. retail units; Two/single storey extension at rear and alterations to form 2 no. ground floor flats and 1 no. additional first floor flat				

Application No.:	68671	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	RRG KIA Service Centre, Wellington Street, Bolton Road, Bury, BL8 2AX				
Proposal:	Alterations to existing windows/doors; Installation of new first floor window openings to front elevation to form first floor office and Remodel internal existing offices/reception area				

Application No.:	68672	App. Type:	ADV	08/11/2022	Approve with Conditions
Location:	RRG KIA Service Centre, Wellington Street, Bolton Road, Bury, BL8 2AX				
Proposal:	1 No. internally illuminated fascia sign				

Application No.:	68828	App. Type:	FUL	18/11/2022	Approve with Conditions
Location:	51 Kensington Drive, Bury, BL8 2DE				
Proposal:	Demolition of rear conservatory and Erection of rear/side single storey extension				

Application No.:	68919	App. Type:	FUL	30/11/2022	Approve with Conditions
Location:	573 Bolton Road, Bury, BL8 2DW				
Proposal:	Render of existing property				

Ward: **North Manor**

Application No.:	68563	App. Type:	FUL	07/11/2022	Approve with Conditions
Location:	8 Leamington Avenue, Bury, BL9 5LD				
Proposal:	Single storey extension at rear				

Application No.:	68660	App. Type:	FUL	22/11/2022	Approve with Conditions
Location:	239 Summerseat Lane, Ramsbottom, Bury, BL0 9UL				
Proposal:	Conversion of garage to garden room and the addition of new windows/doors to front and side elevations				

Application No.:	68687	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	2 Westgate Avenue, Ramsbottom, Bury, BL0 9SS				
Proposal:	Two storey extension at side with front porch; Part single/Part two storey extension at rear; Formation of parking area at front with new vehicular access and erection of gate/fence				

Application No.:	68719	App. Type:	LDCP	02/11/2022	Lawful Development
Location:	Wood Mill, Wood Road Lane, Summerseat, Bury, BL9 5QA				
Proposal:	Lawful development certificate for proposed building for swimming pool and gym				

Application No.:	68783	App. Type:	FUL	14/11/2022	Approve with Conditions
Location:	9 Swan Road, Ramsbottom, Bury, BL8 4ES				
Proposal:	Single storey garage extension at side with new driveway/raised garden areas at front and formation of rear patio with steps to garden level				

Application No.:	68789	App. Type:	FUL	31/10/2022	Approve with Conditions
Location:	10 Crag Lane, Summerseat, Ramsbottom, Bury, BL9 5NE				
Proposal:	2 no. single storey front extensions; 3 no. rear dormers				

Application No.:	68795	App. Type:	FUL	02/11/2022	Approve with Conditions
Location:	39 Greenheys Crescent, Tottington, Bury, BL8 4QD				
Proposal:	Single storey extension/porch at front; Conversion of existing conservatory to single storey rear extension; New window opening to side elevation and render to external elevations				

Application No.:	68814	App. Type:	FUL	07/11/2022	Refused
Location:	21 Newcombe Road, Ramsbottom, Bury, BL0 9UU				
Proposal:	Detached single storey garage to front of dwelling and Extended driveway				
Application No.:	68816	App. Type:	FUL	10/11/2022	Approve with Conditions
Location:	21 Brierfield Drive, Bury, BL9 5JJ				
Proposal:	Single storey front extension				
Application No.:	68826	App. Type:	FUL	07/11/2022	Approve with Conditions
Location:	431 Holcombe Road, Tottington, Bury, BL8 4HB				
Proposal:	Single storey side extension; First floor side dormer extensions				
Application No.:	68860	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	10 Tor Avenue, Tottington, Bury, BL8 4HG				
Proposal:	First floor balcony over existing flat roof at rear				
Application No.:	68863	App. Type:	FUL	24/11/2022	Approve with Conditions
Location:	9 Belvedere Avenue, Tottington, Bury, BL8 4ED				
Proposal:	Single storey rear extension				
Application No.:	68896	App. Type:	FUL	28/11/2022	Approve with Conditions
Location:	33 Oak Avenue, Ramsbottom, Bury, BL0 9UY				
Proposal:	Single storey rear extension				
Application No.:	68906	App. Type:	FUL	15/11/2022	Approve with Conditions
Location:	The Bungalow, Wood Road Lane, Bury, BL9 5QA				
Proposal:	Single storey outbuilding for garage and storage				
Application No.:	68912	App. Type:	LDCEP	23/11/2022	Lawful Development
Location:	43 Vernon Road, Tottington, Bury, BL8 4DD				
Proposal:	Proposed lawful development certificate for bi fold doors to replace existing window on front elevation of residential property				
Application No.:	68955	App. Type:	FUL	17/11/2022	Approve with Conditions
Location:	40 Heath Avenue, Ramsbottom, Bury, BL0 9UN				
Proposal:	Two storey side extension, Single storey rear extension connecting to outbuilding and New roof to front porch.				

Ward: **Prestwich - Holyrood**

Application No.:	68656	App. Type:	FUL	07/11/2022	Approve with Conditions
Location:	26 Daneshill, Prestwich, Manchester, M25 2QL				
Proposal:	Two storey side extension				

Application No.:	68663	App. Type:	FUL	02/11/2022	Approve with Conditions
Location:	343 Heywood Road, Prestwich, Manchester, M25 2RN				
Proposal:	Two storey side extension; Single storey rear extension; Loft conversion with rear dormer				

Application No.:	68711	App. Type:	FUL	31/10/2022	Approve with Conditions
Location:	19 Langley Avenue, Prestwich, Manchester, M25 3EB				
Proposal:	Two storey side extension				

Application No.:	68793	App. Type:	FUL	02/11/2022	Approve with Conditions
Location:	16 Barnard Avenue, Whitefield, Manchester, M45 6TY				
Proposal:	Two/single storey extension at side/rear				

Application No.:	68806	App. Type:	FUL	02/11/2022	Approve with Conditions
Location:	3 Elm Grove, Prestwich, Manchester, M25 3DN				
Proposal:	Single Storey Rear Extension				

Application No.:	68822	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	23 Guest Road, Prestwich, Manchester, M25 3DJ				
Proposal:	Demolition of existing single storey rear extension and replace with new single storey rear extension				

Application No.:	68877	App. Type:	FUL	24/11/2022	Approve with Conditions
Location:	9 Carver Avenue, Prestwich, Manchester, M25 1GA				
Proposal:	Single storey rear extension; First floor rear extension				

Application No.:	68964	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	10 Nursery Road, Prestwich, Manchester, M25 3EN				
Proposal:	Hip to gable roof extension at the side with loft conversion and rear dormer; Raise roof ridge height and new pitched roof to existing front bay window and alterations to first floor windows at the front.				

Ward: **Prestwich - Sedgley**

Application No.:	68580	App. Type:	FUL	17/11/2022	Approve with Conditions
Location:	147 Rectory Lane, Prestwich, Manchester, M25 1DH				
Proposal:	Single storey rear/side extension; Alterations to rear tiered garden to make patio space				

Application No.:	68650	App. Type:	FUL	15/11/2022	Approve with Conditions
Location:	8 Dellcot Close, Prestwich, Manchester, M25 0GX				
Proposal:	Raise roof ridge height/hip to gable roof extension with loft conversion and dormers to front/rear to form second floor living accommodation; Single storey extension at side/rear and single storey extension at side				

Application No.:	68690	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	74 Sheepfoot Lane, Prestwich, Manchester, M25 0DT				
Proposal:	2 No. two storey side extensions, single storey rear extension, front porch and loft conversion with rear dormer to form second floor; Boundary wall/railings to front and side.				
Application No.:	68725	App. Type:	FUL	02/11/2022	Approve with Conditions
Location:	4 Castle Hill Road, Prestwich, Manchester, M25 0FR				
Proposal:	Detached garage and storage building with walk-on flat roof and glazed balustrades to perimeter. New vehicular dropped crossing with gated vehicle entrance and associated landscaping				
Application No.:	68763	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	5 Jesmond Avenue, Prestwich, Manchester, M25 9NG				
Proposal:	Single storey rear extension				
Application No.:	68804	App. Type:	FUL	18/11/2022	Approve with Conditions
Location:	21 Sheepfoot Lane, Prestwich, Manchester, M25 0BN				
Proposal:	Variation of condition no. 2 (approved plans) of planning permission 67324 for external changes including an increase in width of rear basement extension, changes to fenestration detailing, balustrade and brick colour (to grey brick), enlargement of roof light and addition of a tile clad canopy with associated internal alterations				
Application No.:	68852	App. Type:	FUL	15/11/2022	Approve with Conditions
Location:	37 Bury Old Road, Prestwich, Manchester, M25 0EY				
Proposal:	Single storey rear extension				
Application No.:	68866	App. Type:	LDCP	07/11/2022	Lawful Development
Location:	89 Bury Old Road, Prestwich, Manchester, M25 0FQ				
Proposal:	Lawful development certificate for proposed conversion of patio area within existing building walls into an en suite bathroom				
Application No.:	68875	App. Type:	FUL	08/11/2022	Approve with Conditions
Location:	79 Windsor Road, Prestwich, Manchester, M25 0DB				
Proposal:	Variation of condition no. 2 of planning permission 68043 - Amend proposed layout of the ground and first floor plans (so the existing occupants on the ground floor of 79 Windsor Road will expand into the first floor above and the existing first floor occupants will take the ground and first floor of the new development)				
Application No.:	68916	App. Type:	TEL	30/11/2022	Prior Approval Required and Granted
Location:	Pavement outside St Mary's Park, Prestwich, M25 1AA				
Proposal:	Prior approval for proposed 5G telecommunications installation: 17.0m High Phase 8 Street pole and additional equipment cabinets				
Application No.:	69009	App. Type:	GPDE	29/11/2022	Prior Approval Not Required - Extension
Location:	129 Albert Avenue, Prestwich, Manchester, M25 0HF				
Proposal:	Prior approval for proposed single storey rear extension				

Application No.: 69039 **App. Type:** FUL 30/11/2022 Approve with Conditions
Location: 21 Firbank, Ogden Street, Prestwich, Manchester, M25 1JL
Proposal: Garage conversion & addition of side doorset and window

Ward: **Prestwich - St Mary's**

Application No.: 68488 **App. Type:** FUL 14/11/2022 Refused
Location: Lady Margaret House, 51 St Anns Road, Prestwich, Manchester, M25 9LF
Proposal: Proposed new detached garage at side with reinstated vehicular access

Application No.: 68676 **App. Type:** FUL 17/11/2022 Approve with Conditions
Location: 37 Church Lane, Prestwich, Manchester, M25 1AN
Proposal: Conversion of loft and storage area to create additional living space; Alteration to door/windows on front elevation

Application No.: 68762 **App. Type:** FUL 01/12/2022 Approve with Conditions
Location: Eastholme, St Anns Road, Prestwich, Manchester, M25 9LD
Proposal: Part single/Part two storey extension at rear; Extended driveway

Application No.: 68771 **App. Type:** FUL 08/11/2022 Approve with Conditions
Location: 79 Prestwich Hills, Prestwich, Manchester, M25 9PY
Proposal: Single storey side extension

Application No.: 68787 **App. Type:** FUL 31/10/2022 Approve with Conditions
Location: 69 Kersal Road, Prestwich, M25 9SN
Proposal: Two/single storey side extension and single storey extension at side/rear with first floor balcony/glass balustrade; Conversion of garage to living accommodation; New garage; Widening of existing driveway

Application No.: 68957 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 13 Butterstile Lane, Prestwich, Manchester, M25 9PW
Proposal: Garage conversion; Loft conversion; First floor rear juliette balcony; Widening of existing vehicular access and addition of gate

Ward: **Radcliffe - East**

Application No.: 68713 **App. Type:** FUL 30/11/2022 Approve with Conditions
Location: 28 Bury And Bolton Road, Radcliffe, Manchester, M26 4LD
Proposal: Front porch and canopy

Application No.: 68760 **App. Type:** FUL 16/11/2022 Approve with Conditions
Location: 38 Bury And Bolton Road, Radcliffe, Manchester, M26 4LD
Proposal: Single storey rear extension; Raised patio with balcony

Application No.: 68823 **App. Type:** FUL 07/11/2022 Approve with Conditions
Location: 13 Wilton Gardens, Radcliffe, Manchester, M26 2UP
Proposal: Single storey side extension

Ward: **Radcliffe - North and Ainsworth**

Application No.: 67787 **App. Type:** FUL 01/11/2022 Approve with Conditions
Location: Land adjacent to 9 Taylors Lane, Radcliffe, Bolton, BL2 6QS
Proposal: Conversion of existing stable block to form 1 no. dwelling with associated garage and stable block

Application No.: 68665 **App. Type:** FUL 10/11/2022 Approve with Conditions
Location: 5 Knowsley View, The Haven, Knowsley Road, Radcliffe, BL2 5PT
Proposal: Redevelopment of a site known as 'The Haven' for 1 no. private residential dwelling, with associated garden curtilage and parking

Application No.: 68669 **App. Type:** FUL 31/10/2022 Approve with Conditions
Location: 197 Bury New Road, Radcliffe, Bolton, BL2 6QQ
Proposal: Single storey side extension

Application No.: 68837 **App. Type:** FUL 31/10/2022 Approve with Conditions
Location: 60 Launceston Road, Radcliffe, Manchester, M26 3UN
Proposal: Single storey extension at front and side

Application No.: 68978 **App. Type:** FUL 01/12/2022 Approve with Conditions
Location: Land to rear of 41 Bury Old Road & west of Arthur Lane, Ainsworth, Bolton, BL2 5PF
Proposal: Variation of conditions 2 (approved plans) & 3 (materials) of planning permission 65373 - Reduce amount of glazing, internal changes and external brick finish

Ward: **Radcliffe - West**

Application No.: 68927 **App. Type:** LDCLP 23/11/2022 Lawful Development
Location: Footpath opposite Hawthorn Hotel, 141-143 Stand Lane, Radcliffe, Manchester, M26 1JR
Proposal: Lawful development certificate for proposed addition of antennae to an existing 15m pole structure that supports fibre cable

Ward: **Ramsbottom + Tottington - Tottington**

Application No.: 68811 **App. Type:** FUL 01/11/2022 Approve with Conditions
Location: 2 Hazel Avenue, Tottington, Bury, BL8 3EQ
Proposal: Conversion of existing conservatory into single storey rear extension

Application No.: 68832 **App. Type:** FUL 11/11/2022 Approve with Conditions
Location: 61 Bradshaw Road, Walshaw, Tottington, Bury, BL8 3PN
Proposal: Variation of condition 2 following approval of planning permission ref. 66152 - Addition of dormer extension to rear

Application No.: 68855 **App. Type:** FUL 15/11/2022 Approve with Conditions
Location: 100 Moorside Road, Tottington, Bury, BL8 3HR
Proposal: Single storey side extension and Loft conversion with front and rear dormers

Application No.: 68910 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 10 Rosewood Avenue, Tottington, Bury, BL8 3HG
Proposal: Extension to existing garage

Ward: **Ramsbottom and Tottington - Ramsbottom**

Application No.: 68241 **App. Type:** FUL 04/11/2022 Approve with Conditions
Location: Higher House, Moor Road, Ramsbottom, Bury, BL8 4NX
Proposal: Extension, alteration and remodelling of outbuildings to form a family annexe

Application No.: 68564 **App. Type:** FUL 22/11/2022 Approve with Conditions
Location: Bank Top Farm, Moorbottom Road, Ramsbottom, Bury, BL8 4NP
Proposal: Single storey extension at front/side with first floor balcony with glazed balustrade and glazed external staircase; Installation of 3 no. dormer windows to existing roof at front; Change existing ground floor front window opening to patio doors and colour of all windows/doors to anthracite grey

Application No.: 68773 **App. Type:** PIAPA 02/11/2022 Prior Approval Required and Granted
Location: First Floor, 45 Bridge Street, Ramsbottom, BL0 9AD
Proposal: Prior approval for change of use from Commercial/Retail (Class E) to 1no. Flat (Class C3)

Application No.: 68798 **App. Type:** FUL 24/11/2022 Approve with Conditions
Location: 93 Bury New Road, Ramsbottom, Bury, BL0 0BZ
Proposal: Two storey/single storey rear extension

Application No.: 68836 **App. Type:** FUL 17/11/2022 Approve with Conditions
Location: 5 Young Street, Ramsbottom, Bury, BL0 9HS
Proposal: Raised balcony with glazed balustrade/external stairs at rear

Application No.: 68864 **App. Type:** FUL 25/11/2022 Approve with Conditions
Location: Hey House, Holcombe Old Road, Ramsbottom, Bury, BL0 4NS
Proposal: First floor extension above existing garage with external staircase

Application No.: 68876 **App. Type:** FUL 17/11/2022 Approve with Conditions
Location: Bast House Farm, Manchester Road, Ramsbottom, Bury, BL9 5LZ
Proposal: First floor extension above attached garage with dormer window at rear; Amended door and window openings

Application No.: 68913 **App. Type:** FUL 30/11/2022 Approve with Conditions
Location: 11 Spring Close, Ramsbottom, Bury, BL0 9SQ
Proposal: Two storey side extension; Single storey rear extension; Formation of parking space, bin storage area at side and resiting of steps to raised garden at rear

Ward: **Whitefield + Unsworth - Besses**

Application No.: 68561 **App. Type:** FUL 11/11/2022 Approve with Conditions
Location: 110 Thatch Leach Lane, Whitefield, Manchester, M45 6FW
Proposal: Two storey/single storey rear extension

Application No.: 68841 **App. Type:** FUL 22/11/2022 Approve with Conditions
Location: 3 Prestfield Road, Whitefield, Manchester, M45 6BD
Proposal: Single storey rear extension

Application No.: 68849 **App. Type:** FUL 02/12/2022 Approve with Conditions
Location: 275 Parr Lane, Bury, BL9 8PJ
Proposal: Installation of a dropped kerb and associated landscaping of driveway

Application No.: 68901 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 191 Parr Lane, Bury, BL9 8JW
Proposal: Single storey rear extension; first floor side extension and new first floor window at front

Application No.: 68958 **App. Type:** TEL 07/11/2022 Prior Approval Required and Refused
Location: Footpath opposite junction with Ashby Grove, Thatch Leach Lane, Whitefield, M45 6FN
Proposal: Proposed 5G telecoms installation: H3G 17m street pole and additional equipment cabinets.

Ward: **Whitefield + Unsworth - Pilkington Park**

Application No.: 68731 **App. Type:** FUL 08/11/2022 Approve with Conditions
Location: 26 West View Grove, Radcliffe, Manchester, M45 7NQ
Proposal: Single storey side/rear extension

Application No.: 68736 **App. Type:** FUL 10/11/2022 Approve with Conditions
Location: 34 Hillingdon Road, Whitefield, Manchester, M45 7QN
Proposal: Demolition of existing dwelling and erection of 1 no. replacement dwelling with associated works

Application No.: 68756 **App. Type:** FUL 16/11/2022 Approve with Conditions
Location: 15 Church Lane, Whitefield, Manchester, M45 7NE
Proposal: Replacement of conservatory/plinth with Two storey rear extension; Addition of new doors at rear basement level and Raised terrace at rear with glazed balustrade

Application No.: 68859 **App. Type:** FUL 16/11/2022 Approve with Conditions
Location: 5 The Meadows, Old Hall Lane, Whitefield, Manchester, M45 7RZ
Proposal: First floor balcony extension at front

Application No.: 68882 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 7 Spencer Avenue, Whitefield, Manchester, M45 7RE
Proposal: Hip to gable roof extension; Loft conversion with rear dormer

Application No.: 68886 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 3 Spencer Avenue, Whitefield, Manchester, M45 7RE
Proposal: Two storey side extension; Loft conversion with rear dormer

Ward: **Whitefield + Unsworth - Unsworth**

Application No.: 68698 **App. Type:** FUL 02/11/2022 Approve with Conditions
Location: Nightfreight, Roach Bank Road, Bury, BL9 8RQ
Proposal: Remove brickwork covering the portal frame on the east and west elevations; Over cladding on all elevations where required; Replace existing single width roller doors with double width roller doors; Remove canopy on the south elevation and install 5 No. additional roller doors

Application No.: 68744 **App. Type:** FUL 24/11/2022 Approve with Conditions
Location: Land adjacent to junction of Pilsworth Road and Pilsworth Way, Pilsworth Industrial Estate, Bury, BL9 8RE
Proposal: Extensions to 13 existing units; Erection of 3 new build units; External alterations (including comprehensive recladding) and associated access, parking, servicing, security treatment and landscaping arrangements

Application No.: 68803 **App. Type:** FUL 02/11/2022 Approve with Conditions
Location: 118 Sunny Bank Road, Bury, BL9 8LJ
Proposal: Single storey front extension; Single storey rear extension

Application No.: 68805 **App. Type:** FUL 08/11/2022 Approve with Conditions
Location: 3 Statter Street, Bury, BL9 8AZ
Proposal: Installation of 90mm external wall EPS insulation system with silicon render finish to external elevations

Application No.: 68807 **App. Type:** FUL 31/10/2022 Approve with Conditions
Location: 3 Anfield Close, Bury, BL9 8JZ
Proposal: Replace existing conservatory with single storey rear extension; Replace flat roof to front porch with a pitched roof

Application No.: 68850 **App. Type:** FUL 16/11/2022 Approve with Conditions
Location: 30 Cartmel Close, Bury, BL9 8JA
Proposal: Single storey side/rear extension and Pitched roof to existing front extension

Application No.: 68891 **App. Type:** FUL 28/11/2022 Approve with Conditions
Location: 396 Parr Lane, Bury, BL9 8LZ
Proposal: First floor side extension

Application No.: 68902 **App. Type:** CON 04/11/2022 Raise No Objection
Location: Units B And G Birch Industrial Estate, Whittle Lane, Heywood, OL10 2SX
Proposal: Article 18 Consultation from Rochdale Council (ref: 22/01148/FUL): Demolition of Units B and G and erection of a warehouse unit (Class B2/B8) with ancillary office space (Class E) including parking, servicing areas, associated works and infrastructure - Resubmission of 21/01489/FUL

Total Number of Applications Decided: 111

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REPORT FOR NOTING

Agenda Item 6

Bury
COUNCIL

**Agenda
Item**

6

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	13 December 2022
SUBJECT:	PLANNING APPEALS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	<p>Planning Appeals:</p> <ul style="list-style-type: none"> - Lodged - Determined <p>Enforcement Appeals</p> <ul style="list-style-type: none"> - Lodged - Determined
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A

Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS

DIRECTOR:

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Committee of the Planning Appeals lodged against decisions of the authority and against Enforcement Notices served and those that have been subsequently determined by the Planning Inspectorate.

Attached to the report are the Inspectors Decisions and a verbal report will be presented to the Committee on the implications of the decisions on the Appeals that were upheld.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-

Contact Details:-

David Marno, Head of Development Management
Planning Services, Department for Resources and Regulation,
3 Knowsley Place ,Bury BL9 0EJ

Tel: 0161 253 5291

Email: d.marno@bury.gov.uk

**Planning Appeals Lodged
between 31/10/2022 and 04/12/2022**



Application No.: 68245/PMBPA

Appeal lodged: 24/11/2022

Decision level: DEL

Appeal Type: Written Representations

Recommended Decision: Prior Approval Required

Applicant: Mr Tony Rostron

Location Tottington Manor Farm, Turton Road, Bury, BL8 3QQ

Proposal Prior approval for the change of use from agricultural building to dwellinghouse

Application No.: 68723/FUL

Appeal lodged: 04/11/2022

Decision level: DEL

Appeal Type: Written Representations

Recommended Decision: Refuse

Applicant: Cardtronics UK Ltd, trading as CASHZONE

Location 237 Wash Lane, Bury, BL9 7DH

Proposal Retention of ATM (Automated teller machine)

Total Number of Appeals Lodged: 2

<p align="center">Planning Appeals Decided between 31/10/2022 and 04/12/2022</p>



Application No.: 67318/FUL

Decision level: DEL

Recommended Decision: Refuse

Applicant: Mrs Janice McNally

Location: 273 Parr Lane, Bury, BL9 8PJ

Proposal: Retrospective planning permission for proposed change of use of land to the rear of property to be within the residential curtilage

Appeal Decision: No further action

Date: 19/11/2022

Appeal received too late

Application No.: 67554/FUL

Decision level: DEL

Recommended Decision: Refuse

Applicant: Mr Abdullah Naveed

Location: 73 Brierley Street, Bury, BL9 9HW

Proposal: Change of use from dwelling (Class C3) to residential institution (Class C2) for proposed children's care home (for up to 4 children with a minimum of 2 full time carers overnight, 1 sleeping, 3 full time carers during the day and living together as a single household)

Appeal Decision: Dismissed

Date: 04/11/2022

Appeal type: Written Representations

Application No.: 68263/FUL

Decision level: DEL

Recommended Decision: Refuse

Applicant: harryjacksonsurveyorsltd

Location: 7 Burndale Drive, Bury, BL9 8EN

Proposal: Two storey side extension with gable roof plus new parking layout

Appeal Decision: Allowed

Date: 17/11/2022

Appeal type: Written Representations



Appeal Decision

Site visit made on 4 October 2022

by David Jones BSc (Hons) MPlan MRTPI

an Inspector appointed by the Secretary of State

Decision date: 04 November 2022

Appeal Ref: APP/T4210/W/22/3297379

73 Brierley Street, Bury BL9 9HW

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Mr Abdullah Naveed against the decision of Bury Metropolitan Borough Council.
 - The application Ref 67554, dated 17 September 2021, was refused by notice dated 31 January 2022.
 - The development proposed is a change of use from C3 dwelling to C2 residential institution for proposed children's care home, for up to 4 children with a minimum of 2 full time carers overnight [1 sleeping], 3 full time carers during the day & living together as a single household.
-

Decision

1. The appeal is dismissed.

Main Issues

2. The main issues are:

- the effect of the proposed development on highway safety, with particular reference to parking; and
- whether the living conditions for future occupiers would be acceptable, with particular regard to the availability of suitable outdoor amenity space.

Reasons

Highway Safety

3. The appeal property is a five bedroomed end-terraced dwelling located on a corner plot at the junction of Brierley Street and Sultan Street. The property is situated within a densely populated residential area, characterised by predominately terraced housing with small back lanes running in between the rows of terraces. The property includes a small enclosed rear yard, however as with most properties in the surrounding area there is no provision for off-street parking.
4. The proposed development would change the use of the existing dwelling to a children's care home. The proposed use would see the property occupied by up to 4 children who would be supported by 3 full-time residential carers during the day and 2 full-time residential carers overnight.
5. Saved Policy HT2/4 of the Bury Unitary Development Plan (adopted August 1997) (UDP) requires developments to make adequate provision for their car

parking and servicing requirements in accordance with the Council's car parking standards. The Council's current car parking standards are detailed within the Development Control Policy Guidance Note 11: Parking Standards in Bury (May 2007) (SPD11).

6. SPD11 includes a table listing different types of developments and the maximum standards for car parking provision applicable to each development type. This includes residential institutions where it is stated that 1 car parking space per 4 beds should be provided, although SPD11 also makes it clear that parking standards for care and nursing homes should be considered on their own merits depending upon the nature and location of the proposal.
7. The proposed development does not provide any car parking spaces, and the lack of any off-street parking at the property means that staff and any other visitors attending the site by car would have to park on the street. The carers would work shifts resulting in a changeover of staff and an increase in vehicle movements at the end of each shift period, although no information as to the time or frequency of shift patterns has been provided.
8. Whilst unrestricted on-street parking is available in the vicinity of the appeal site, the Council observed during their site visit that there were difficulties locating a parking space even during the day. At the time of my early afternoon site visit I also noted that there were a very limited number of on-street parking spaces available in the immediate vicinity of the appeal site, with vehicles parked along both sides of Brierley Street and Sultan Street. Although my site visit was only a snapshot in time, it is an indication that demand for on-street parking in the area is relatively high particularly given that I would expect demand to be at its greatest in the evening when residents would for example be likely to return from work.
9. Although the existing carriageway widths on Brierley Street and Sultan Street mean that on-street parking can be accommodated without jeopardising access to emergency vehicles, the on-street parking of vehicles, often along large stretches of both sides of the surrounding roads, makes manoeuvrability on the highway difficult. As shown in image 1 of the appellant's appeal statement, and from my observations, whilst there is sufficient room for a single vehicle to drive along the surrounding roads, should two vehicles meet then they would be unable to pass when there is on-street parking. I also observed that the number of vehicles parked along the highway often restricted visibility when entering or exiting one of the many connected side streets, as well as making parking itself more onerous.
10. I acknowledge that on-street parking is an existing situation for the majority of properties in the vicinity of the appeal site, and that the existing use of the appeal property as a five bedroomed dwelling would likely result in vehicles belonging to any occupiers being parked on the street. However, the proposed use would result in up to 3 carers being on site 24 hours a day with shift patterns resulting in a turnover of staff, in addition to any other visitors and professionals who may attend the property.
11. I recognise the location of the site is near to various facilities and services and that its proximity to public transport links mean that it would be possible for carers and other visitors to travel to and from the property without their car. In such circumstances, SPD11 states that residential related developments may be able to provide less than the stipulated maximum number of parking

spaces. However, I find that given the nature of the proposed use including the need for 24-hour care, shift patterns, and the fact that children housed at the property would be expected to attend local schools and other after school groups and sports clubs, it would be highly unlikely that staff would not travel to the site by car.

12. Therefore, notwithstanding SPD11 which sets out that 4 bed dwellings should provide 3 car parking spaces whereas residential institutions should provide only 1 space per 4 beds, from the information before me I find that the proposed use would likely result in an increase in both vehicle movements and demand for on-street parking spaces in comparison to the existing use of the property as a dwelling. Whilst this increase may be moderate, in the absence of any substantive evidence to the contrary, I find that the proposed use would only serve to exacerbate an existing issue which would not be conducive to securing good levels of highway safety.
13. Furthermore, despite the sites corner plot location providing a potentially better parking context than some other properties in the locality, the available evidence indicates that there is an existing relatively high demand for on-street parking spaces in the area. In addition, the presence of a small number of houses opposite the appeal site on Brierley Street which have off-street parking, does little to reduce pressure for on-street parking given that the presence of dropped kerbs prevents any such parking.
14. I also note that the Council's Highways department objected to the proposal on the basis that the proposed servicing and car parking provision is inadequate and would likely lead to vehicles parking and carrying out manoeuvres on the highway, to the detriment of the free flow of traffic and road safety. Although there is little substantive evidence before me which indicates that the appeal proposal would require any specific servicing provision, for the reasons given above I find no compelling reason to disagree with this conclusion in respect of the lack of adequate car parking provision. The lack of third-party objections on this matter does not lead me to a different conclusion.
15. The proposed development would therefore not provide adequate car parking provision, resulting in vehicles parking and manoeuvring on the highway to the detriment of highway safety. As such, the proposal would be contrary to Saved Policy HT2/4 of the UDP and guidance contained within SPD11. These require, among other things, that developments make adequate provision for their car parking requirements.
16. The Council also referred to Saved Policies CF3, CF3/1 and CF5 of the UDP its decision notice, which relate to Social Services, Residential Care Homes and Nursing Homes, and Childcare Facilities respectively. However, in respect of this main issue regarding highway safety, the provisions of these policies are not applicable.

Living Conditions

17. The appeal property includes a modest amount of external private amenity space. This space is predominately located to the rear of the property in the form of an enclosed yard which is hard surfaced and measures approximately 32 square metres in area.

18. The National Planning Policy Framework (the Framework) is a material consideration in the determination of the application. One of the overall aims and objectives of the Framework is to achieve well-designed places. Paragraph 130 requires, among other matters, that planning decisions should ensure that developments create places which promote health and well-being, with a high standard of amenity for existing and future users.
19. The Council's Children's Centre & Early Years team have objected to the proposal, including concerns that the lack of outdoor space would be an issue as "children need outdoor space to play". Whilst the external amenity space is comparable in size to other properties in the surrounding area, the proposed use would see the appeal property occupied by up to 4 children or young people at any one time. This would likely intensify the need for sufficient outdoor amenity space for use over a prolonged period in comparison to a residential dwelling.
20. The appellant has referred me to various national guidance and standards for residential and children's care, including the 'Children's Homes Regulations and Quality Standards (2015)', and I acknowledge that none of these stipulate a specific or minimum area of external amenity space to be provided. However, given its size and layout I find that the proposed external amenity space would be inadequate for regular use by 4 children or young people and would not provide opportunity for a variety of different activities to take place within it. I therefore consider that the useability of this space would likely be limited and insufficient for the proposed use of the site.
21. Although there are parks and sports fields within walking distance of the appeal site which could be used by occupants of the property, these would most likely assist in older children who participate in larger or team sports which do not commonly take place within domestic gardens. I do not consider therefore that this is an appropriate alternative, particularly for younger children, nor would it overcome the poor standard of external amenity space provided at the appeal property.
22. I therefore conclude that the proposed development would result in unacceptable living conditions for future occupiers, with particular regard to the provision of suitable outdoor amenity space. The development would therefore be contrary to the Framework which, although not detailing any specific requirement or size for outdoor amenity space, seeks that development provides a high standard of amenity for future users.
23. The Council has referred to Saved Policies H2/2, EN1/2, CF3, CF3/1 and CF5 of the UDP in its decision notice. However, as none of these policies are relevant to this main issue concerning the living conditions of future occupiers, they have not been determinative in relation to this matter.

Other Matters

24. I have had regard to other various matters raised including concerns relating to anti-social behaviour, and the potential for noise and disturbance to be generated by the proposed use to the detriment of the living conditions of occupiers of nearby residential properties. However, as I am dismissing the appeal on other grounds, I have not pursued these matters further.

Conclusion

25. The proposed development would provide a valuable service in the community by providing accommodation for children and young people who have been placed in social care. There would be benefits to the local economy through employment opportunities for local people. I acknowledge that a number of policies contained in the UDP also support the proposal in principle, including policies H4/2, CF3 and CF3/1. I find however that these benefits are outweighed by the adverse impacts that would arise from granting planning permission, by reason of unacceptable harm being caused to highway safety and to the living conditions of future occupiers.
26. The proposal would conflict with the development plan, when taken as a whole, and there are no material considerations, including the approach of the Framework, which would indicate a decision other than in accordance with it. The appeal should therefore be dismissed.

David Jones

INSPECTOR



Appeal Decision

Site visit made on 15 November 2022

by J Williamson BSc (Hons) MPlan MRTPI

an Inspector appointed by the Secretary of State

Decision date: 17 November 2022

Appeal Ref: APP/T4210/D/22/3306388

7 Burndale Drive, Bury BL9 8EN

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Mr and Mrs Katy and Christian Chivers against the decision of Bury Metropolitan Borough Council.
 - The application Ref 68263, dated 27 March 2022, was refused by notice dated 22 June 2022.
 - The development proposed is described as two storey side extension.
-

Decision

1. The appeal is allowed, and planning permission is granted for two storey side extension at 7 Burndale Drive, Bury BL9 8EN, in accordance with the terms of the application Ref 68263, dated 27 March 2022, subject to the following conditions:
 - 1) The development hereby permitted shall begin not later than 3 years from the date of this decision.
 - 2) The development hereby permitted shall be carried out in accordance with the following approved plans: Location Plan, Site Plan Existing, Site Plan Proposed, Drawing No. 1 Existing Elevation, Drawing No. 2 Existing Ground Floor Plan, Drawing No. 3 Existing First Floor Plan, Drawing No. 4 Rev A Proposed Elevations, Drawing No. 5 Proposed Ground Floor Plan, Drawing No. 6 Proposed First Floor Plan, Drawing No. 7 Existing and Proposed Street Scene, Car Parking (Existing) (submitted to the Council on 13/04/22), and Car Parking (Proposed) (submitted to the Council on 13/06/22).
 - 3) The materials to be used in the construction of the external surfaces of the development hereby permitted shall match those used in the existing building.
 - 4) Prior to the development hereby approved being brought into use, the 2 No. car parking spaces shown on the approved car parking plan (proposed) shall be made available for use. These spaces shall be retained for the lifetime of the development, without impediment to their designated use.

- 5) Prior to the development hereby approved being used, details of the drop kerb denoted on the approved car parking plan (proposed) shall have been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented prior to the approved development being brought into use.

Preliminary Matters

2. During the planning application process modified plans were submitted to the Council, basically setting the gable elevation in around 5 cm from the boundary and showing 2 No. on-site car parking spaces. I have made my Decision, as did the Council, based on the amended plans.
3. The planning application form gives the name of the agent as the applicant. However, the appeal form notes Mr & Mrs Chivers as the appellants. The Council's Decision Notice makes it clear that the Decision is for Mr & Mrs Chivers. I therefore consider Mr & Mrs Chivers to have been the applicants of the planning application and therefore the rightful appellants.
4. I have used the description of proposed development provided on the application form as this adequately describes the proposal. The approved plans show that the proposal would have a gable end and that there would be 2 No. on-site parking spaces. The conditions attached require development to be carried out in accordance with the approved plans and for the on-site parking spaces to be provided before the approved extension is brought into use.

Main Issue

5. The main issue is the effect of the proposed development on the character and appearance of the area.

Reasons

6. The appeal property is a two-storey dwelling with a dual-pitched roof and gable ends. It is sited towards the centre of a row of 6 No. detached dwellings which form part of a residential area, primarily consisting of 2-storey detached and semi-detached dwellings.
7. The 2 No. dwellings located at either end of the row are sited forward of the front elevations of the other 4 No. dwellings. Number 11 has a gable frontage. Number 9 has a hipped roof and a single-storey side with wrap-around front porch extension. Number 5 has a gable frontage and a 2-storey side extension. The 2-storey side extension is set-back from the front elevation around 1.5 m at first-floor level and is designed with a side gable. Number 3 has a hipped roof and a single-storey side/front porch extension like the extension at number 9. Number 1 has a 2-storey side extension which is set-back at first-floor level around 1 m with a flat roof design. There is some slight variation in the roof ridge heights of each of the 6 dwellings; and gaps of varying sizes are retained between each of the properties. These details need to be borne in mind in assessing whether the proposed extension would create a terracing effect.
8. The Council Officer's Report and its Decision Notice refer to what **could** happen **if** some of the other dwellings within the row made alterations to their properties, either under permitted development rights or via a planning

application. However, the appraisal of the current proposal must be based on the site-specific circumstances as they are, and not on speculation about what occupiers of other properties within the row may or may not do in the future.

9. The proposal is for a 2-storey side extension located on the north-eastern side of the existing dwelling, towards the side boundary with number 9. The extension would be the full depth of the existing dwelling; hence, its front and rear elevations would be in line with the front and rear elevations of the existing, original property. The heights of the eaves and roof ridge would follow those of the existing dwelling.
10. A gap of around 1 m would be retained between the gable end elevation of the proposed extension and the south-western facing side elevation of number 9, which, as noted above, has a hipped roof. Additionally, as noted above, the property the other side of the appeal site, number 5, has a 2-storey side extension which is set-back at first-floor level; I also note that there is a gap of around 1 m between the respective side elevations of the appeal property and number 5.
11. Section 5 of the Council's Supplementary Planning Document 6: Alterations and Extensions to Residential Properties (adopted 2004 and updated 2010), (SPD), provides guidance regarding 2-storey side extensions. It recommends that the front elevation at first-floor level should be set-back at least 1.5 m from the main frontage of the original house. The objective of the guidance is to avoid the appearance of uncharacteristic terracing.
12. The SPD notes that the requirement for a set-back may be relaxed in certain instances. Although the appeal site may not strictly fall within either of the scenarios listed, bearing in mind the existing characteristics of the site outlined above, eg a short row of 6 dwellings, some irregularity of building line in the row, the differing design details of properties within the row and the gaps that would remain between the appeal dwelling and the 2 dwellings either side, despite not being set back, within the specific circumstances of the site I consider that the proposal would not create a terracing effect.
13. I therefore conclude that the proposal would not be out of keeping with the street scene and consequently would not harm the character or appearance of the area. As such, the proposal accords with saved policies EN1/2 and H2/3 of the Bury Unitary Development Plan, (1997), (policies which are broadly consistent with policies in the Framework¹ regarding protecting the character and appearance of areas). Collectively, and among other things, these policies require new development to not have an unacceptable adverse impact on the character or appearance of the area. The proposal also satisfies the relevant aims and objectives of the SPD.
14. I have not assessed the proposal against saved policy EN1/1. Although this is entitled 'visual amenity', it seeks to not permit development that would have a detrimental effect on "*public views of prominent or important buildings, especially those in areas of architectural or historic interest*" or "*the visual amenity both within, or viewed from, areas of environmental interest such as the Green Belt, Special Landscape Areas or the river valleys*", neither of which are relevant to the appeal proposal.

¹ The National Planning Policy Framework

Other Matters

15. Concern has been raised regarding the potential effect of the proposal on living conditions of occupiers of existing neighbouring properties. However, for the reasons outlined in the Council Officer's Report, I, like the Council, consider that the proposal would not adversely affect the living conditions of occupiers of existing neighbouring properties. A concern has been raised regarding refuse bin storage. I note that on the approved plans (drawing No. 5) a combined bin store/cycle store is included. As such, I consider the proposal would not create any issues regarding refuse bin storage.

Conditions

16. I have taken account of the conditions suggested by the Council, the comments provided by the appellants, and the requirements of the Framework and the Planning Practice Guidance.
17. A condition specifying the approved plans has been attached to clearly identify what has been approved, as amended plans were submitted during the planning application. A condition specifying materials is attached to protect the character and appearance of the area. Conditions related to parking spaces and a drop kerb have been attached in the interest of highway safety.

Conclusion

18. For the reasons outlined, I conclude that the appeal is allowed.

J Williamson

INSPECTOR

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